

Agreement in Principle for Highway-related Departures

DEPARTURES FORM

The Project Sponsor makes the following departure submission for consent / approval by the Principal Project Sponsor and the Assistant Director for Highways and Transport:

General Details	
Project Name	DIDCOT TO CULHAM RIVER CROSSING, DIDCOT GARDEN TOWN, HIF1
OCC Project Sponsor (<i>name & team</i>)	Aron Wisdom, Major Projects Development
Lead Designer details (<i>name & job title</i>)	David Riach, Associate, AECOM
Highway authority name	Oxfordshire County Council
Date of submission and version number	19 January 2021, RIV-DS-07 version P02

Project Details	
Road number (<i>where applicable</i>)	N/A
Road name(s) (<i>where applicable</i>)	This route is new. Road name to be decided.
Location (<i>nearest town/ city</i>)	Didcot
Road category and type (<i>where applicable</i>)	A road, S2 all-purpose single carriageway
National Speed limit (<i>where applicable</i>)	Proposed to be 50mph (85kph design speed)
Summary description of existing conditions and key design issues	<p>No existing conditions as this is a proposed road which will be newly constructed.</p> <p>The key design issue is the requirement of an overtaking value of 30% for rural S2 roads. Taking the rural extent of the link road as 3.3km (out of its overall 3.6km length), 30% equates to approximately 1.0km per direction.</p> <p>Due to the constraints along the proposed road, the required 30% overtaking value cannot be achieved. These constraints include the number and proximity of junctions and other features along the route, two bridge structures and flood plain. The proposed design provides an overtaking value of 26%.</p> <p>As a result, 1 no. Departure from Standard is required.</p>

Departure Details																																																	
Discipline	Highways																																																
Type	Geometric																																																
Relevant technical standards	DMRB CD 109 Revision 1 - March 2020, Highway Link Design																																																
Underlying standard <i>(if applicable)</i>	DMRB																																																
Difference between standard(s) and proposed design	RIV-DS-07 CD 109 Paragraph 9.2 states "The minimum overtaking value for rural S2 roads shall be 30%." The proposed design provides an overtaking value of 26% northbound and 27% southbound.																																																
Reason for departure	<p>The required 30% overtaking value cannot be achieved due to the constraints to the proposed road geometry. Considering the rural element of the road as being the 3.27km extent from immediately north of the raised parallel crossing at Ch. 355 to Ch. 3625 at Abingdon Roundabout, the following constraints are present:</p> <ul style="list-style-type: none"> - Two roundabout junctions (one incorporating a segregated left turn lane (SLTL)) - Two ghost island priority junctions - Three direct accesses - Two bus laybys (one northbound and one southbound) - Two controlled pedestrian/cyclist crossing points (one with associated equestrian crossing) - One uncontrolled pedestrian/cyclist crossing point - Two non-overtaking horizontal curves - Four non-overtaking crest curves (vertical alignment) - Two bridge structures, one over railway sidings and one over the river and associated flood plain. <p>The proposed horizontal alignment has been kept as straight as possible to maximise the overtaking opportunity with level overtaking sections. The constraints noted above also limit the provision of alternative overtaking options such as climbing lanes, dual carriageway overtaking sections or wide single 2+1 overtaking lanes.</p> <p>The proposed design (version P04) provides an overtaking value of 26% northbound and 27% southbound. The sections are listed in Table 1 below.</p> <p>Table 1 Overtaking Sections</p> <table border="1" style="width: 100%; border-collapse: collapse; text-align: center;"> <thead> <tr> <th colspan="6">Overtaking Sections</th> </tr> <tr> <th colspan="3">Northbound</th> <th colspan="3">Southbound</th> </tr> <tr> <th>Start Chainage</th> <th>End Chainage</th> <th>Length (m)</th> <th>Start Chainage</th> <th>End Chainage</th> <th>Length (m)</th> </tr> </thead> <tbody> <tr> <td>355</td> <td>510</td> <td>155</td> <td>575</td> <td>430</td> <td>145</td> </tr> <tr> <td>2340</td> <td>2485</td> <td>145</td> <td>2740</td> <td>2420</td> <td>320</td> </tr> <tr> <td>2620</td> <td>3170</td> <td>550</td> <td>2840</td> <td>3260</td> <td>420</td> </tr> <tr> <td colspan="2">Total</td> <td>850</td> <td colspan="2">Total</td> <td>885</td> </tr> <tr> <td colspan="2">%age of 3270 rural link</td> <td>26.0%</td> <td colspan="2">%age of 3270 rural link</td> <td>27.1%</td> </tr> </tbody> </table>	Overtaking Sections						Northbound			Southbound			Start Chainage	End Chainage	Length (m)	Start Chainage	End Chainage	Length (m)	355	510	155	575	430	145	2340	2485	145	2740	2420	320	2620	3170	550	2840	3260	420	Total		850	Total		885	%age of 3270 rural link		26.0%	%age of 3270 rural link		27.1%
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Associated departures	None
Justification	
Justification for the departure	<p>A design option was considered to increase the length of overtaking section between Sutton Courtenay Roundabout and Abingdon Roundabout, however this option was rejected given the associated construction implications of providing a Full Overtaking Sight Distance (FOSD) vertical curve and the identified benefits of the proposed design in comparison. The implications of increasing the overtaking section on this link include a higher river crossing structure, a large northern embankment, structure drainage issues, and the need for flood plain culverts with attendant maintenance issues. The proposed design will still provide two overtaking sections in each direction between the two roundabouts.</p> <p>A FOSD vertical crest curve with K value of 285 would be necessary to increase the overtaking section between the two roundabouts such that the vertical alignment would rise continuously from Sutton Courtenay Roundabout at the south to Abingdon Roundabout at the north. By comparison, the proposed design incorporates a non-overtaking crest located south of the main river structure. As shown in long section drawings 0027 to 0029 in Annex A, the vertical alignment rises from Sutton Courtenay Roundabout across the flood plain, then lowers over the river to the north side, then rises again to Abingdon Roundabout.</p> <p>The design at this location will incorporate an approach viaduct above the flood plain on the south side of the river plus a main crossing structure over the river itself, with an overall structure length of 500m. For details, refer to general arrangement drawings in Annex A.</p> <p>In terms of benefits, the proposed design:</p> <ul style="list-style-type: none"> • raises the height of the approach viaduct across the flood plain which benefits flood conveyancing capacity and bridge pier arrangements • creates more open bridge spans and removes the need for culverts through embankment which would have attendant flood conveyance and maintenance issues • lowers the height of the bridge piers at the main river crossing structure • reduces the embankment height on the north side of the river • introduces a cutting north of the river which creates fill material and reduces imported fill requirements • improves the drainage design on the 500m long structure such that the deck can be drained both to the north and south, reducing the required drainage system capacity and negating the need to shed runoff directly to the flood plain below • brings cost savings associated with benefits noted above • still provides overtaking sections in both directions, to the south between Sutton Courtenay Roundabout and the non-overtaking crest, and between the non-overtaking crest and Abingdon Roundabout to the north. <p>Given the benefits of the proposed design compared to the implications of altering the road alignment and structures design to increase the overtaking value, plus the associated cost implications, it is recommended that a Departure from Standard is accepted by OCC.</p>

Mitigation measures included <i>(if any)</i>	Centreline road markings to be provided to clearly define overtaking sections, with hazard warning lines to indicate where overtaking may not be safe.
Mitigation measures rejected <i>(if any)</i>	None

Response and Sign-Off

Requested departure is agreed for inclusion in the design

If agreement is subject to any condition(s), provide details and rationale:

If not agreed, state ground(s):

Principal Project Sponsor Signature: **Aron Wisdom** Digitally signed by Aron Wisdom
Date: 2021.01.22 12:11:38 Z Date: _____

Assistant Director for Highways and Transport Signature:  Date: 22nd March 2021

I can confirm I approve and happy with proposed departure, in particular as any post scheme issue could be mitigated through reduced speed limited or no overtaking if identified as required.

Annex A – Schedule of submitted drawings, reports and supporting documentation

Title	Attachments	Version
<i>Drawings (relating to the departure)</i>		
Preliminary Design, Departures, Sheets 1 to 3 of 3 RIV_PD-ACM-HGN-SW_ZZ_ZZ_ZZ-DR-CH-0011 RIV_PD-ACM-HGN-SW_ZZ_ZZ_ZZ-DR-CH-0012 RIV_PD-ACM-HGN-SW_ZZ_ZZ_ZZ-DR-CH-0014	Attached	P02
Preliminary Design, Long sections, Sheets 5 to 7 of 13 RIV_PD-ACM-HGA-SW_ZZ_ZZ_ZZ-DR-CH-0027 RIV_PD-ACM-HGA-SW_ZZ_ZZ_ZZ-DR-CH-0028 RIV_PD-ACM-HGA-SW_ZZ_ZZ_ZZ-DR-CH-0029	Attached	P02
River Thames Crossing, Option Study, General Arrangement, Sheets 1 to 3 of 3 RIV_PD-ACM-SBR-DGT_STR_ZZ_ZZ-DR-CB-0021 RIV_PD-ACM-SBR-DGT_STR_ZZ_ZZ-DR-CB-0022 RIV_PD-ACM-SBR-DGT_STR_ZZ_ZZ-DR-CB-0023	Attached	P01.2 P01.1 P01.1
<i>Reports (relating to the departure)</i>		
<i>Supporting documentation (for information)</i>		
DMRB CD 109 Revision 1 - March 2020	Link to CD 109	Rev 1



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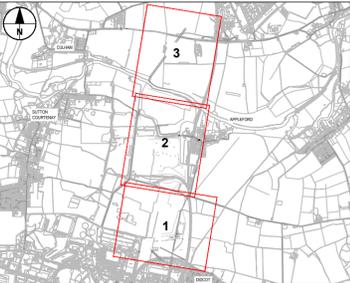
PRELIMINARY DESIGN DRAWING
NO CONSTRUCTION RISKS TO NOTE IN CURRENT REVISION

FOR MAINTENANCE / OPERATION / DECOMMISSIONING AND DEMOLITION RISKS SEE PROJECT HEALTH AND SAFETY FILE.

KEY

- REDLINE BOUNDARY
- PROPOSED CARRIAGEWAY FULL DEPTH CONSTRUCTION
- PROPOSED CARRIAGEWAY TRANSITION CONSTRUCTION FROM EXISTING TO PROPOSED CARRIAGEWAY
- PROPOSED ASPHALT FOOTWAY
- PROPOSED CYCLEWAY
- PROPOSED SHARED USE FOOTWAY / CYCLEWAY
- PROPOSED GRASS VERGE/LANDSCAPING
- PROPOSED ROAD MARKINGS

DRAFT



DEPARTURE RIV-DS-07:
FULL OVERTAKING VALUE OF
30% CANNOT BE ACHIEVED

DEPARTURE RIV-DS-05:
SEPARATION BETWEEN LAY-BY
AND JUNCTION

DEPARTURE RIV-DS-04:
SEPARATION BETWEEN
LAY-BYS ON BOTH SIDES OF
THE ROAD

REVISION DETAILS		By	Date	Suffix
FIRST ISSUE	AZ	KC	27/10/20	P01
DEP 07 ADDED	AZ	KC	03/12/20	P02
		Check		

Purpose of issue
SUITABLE FOR INFORMATION



Client
OXFORDSHIRE COUNTY COUNCIL

Project Title
**RIVER CROSSING
DIDCOT GARDEN TOWN
HIF 1 SCHEMES**

Drawing Title
**HIGHWAYS
PRELIMINARY DESIGN
DEPARTURES
SHEET 1 OF 3**

Designed AZ	Drawn AZ	Checked HP	Approved KC	Date 03/12/20
Internal Project No. 60632497	Suitability S2	Discipline Civil - Highways		

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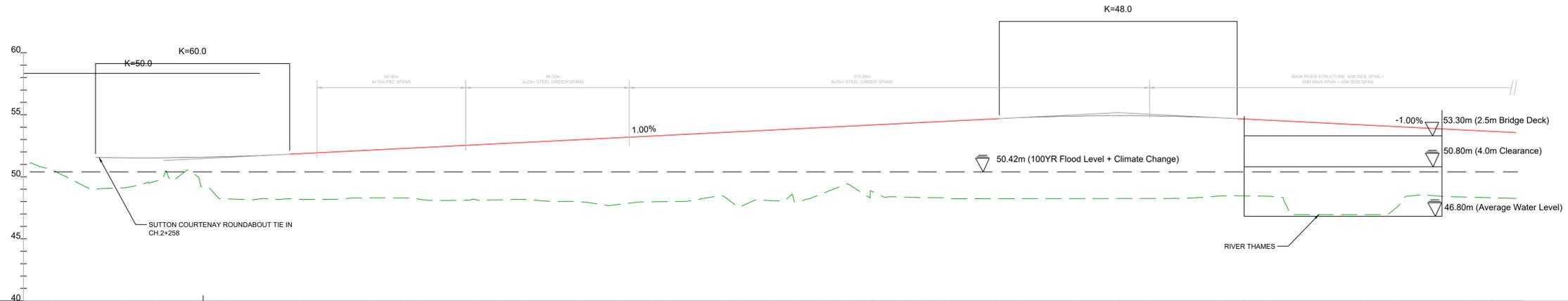
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Location SW_ZZ_ZZ_ZZ	Location DR-CH-0011	Location 1 Type	Location 1 Role	Location 1 Number

Rev: 03 December 2020 11:05:25
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CHAINAGE	2230.001	2300.000	2400.000	2500.000	2600.000	2700.000	2800.000	2830.000
EXISTING LEVELS	51.106	49.168	48.091	48.183	48.281	48.297	48.423	48.247
PROPOSED LEVELS		51.577	52.475	53.475	54.475	54.831	53.863	53.563



SUPERELEVATION	
LEFT OUTSIDE LANE	
RIGHT OUTSIDE LANE	-2.5%

DIDCOT TO CULHAM RIVER CROSSING MAINLINE-2 LONGSECTION
 SCALE: H 1:1000 ; V 1:200
 CHAINAGE: 2230.001m - 2830.000m

First Issue	AZ	KC	23/10/20	P01
REVISED ALIGNMENT	AZ	KC	17/12/20	P02
REVISION DETAILS	By	Check	Date	Suffix

SUITABLE FOR INFORMATION

Client
 County Hall
 New Road
 Oxford
 OX1 1ND

Project Title
**RIVER CROSSING
 DIDCOT GARDEN TOWN
 HIF 1 SCHEMES**

Drawing Title
**HIGHWAYS
 PRELIMINARY DESIGN
 LONG - SECTIONS
 SHEET 5 OF 13**

Designed AZ	Drawn AZ	Checked DR	Approved KC	Date 17/12/20
Internal Project No. 60632497	Suitability S2	Discipline Civil - Highways		

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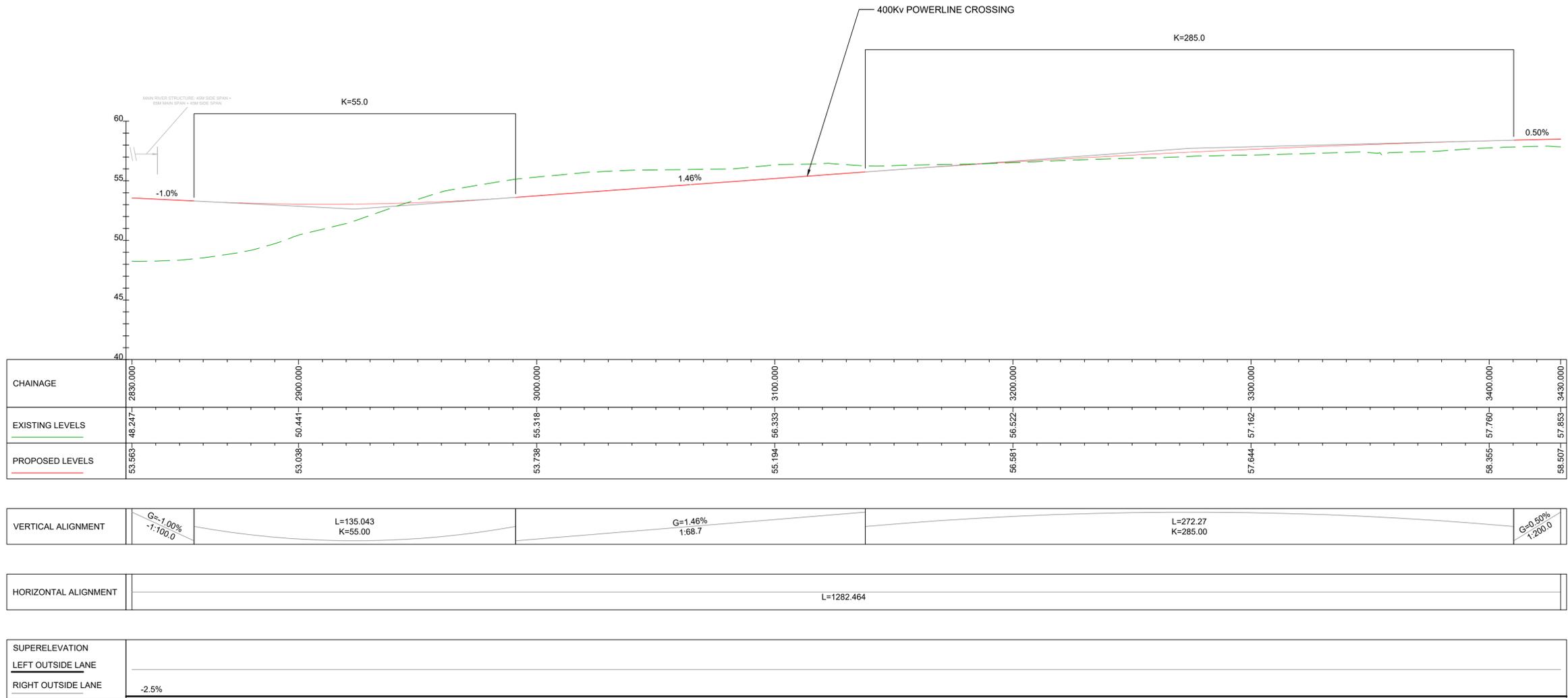
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Location SW_ZZ_ZZ	I Type 1 Role 1 Number			

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DIDCOT TO CULHAM RIVER CROSSING MAINLINE-2 LONGSECTION
 SCALE: H 1:1000 ; V 1:200
 CHAINAGE: 2830.000m - 3430.000m

First Issue	AZ	KC	23/10/20	P01
REVISED ALIGNMENT	AZ	KC	17/12/20	P02
REVISION DETAILS	By	Check	Date	Suffix

Purpose of issue
SUITABLE FOR INFORMATION

Client
 County Hall
 New Road
 Oxford
 OX1 1ND

Project Title
**RIVER CROSSING
 DIDCOT GARDEN TOWN
 HIF 1 SCHEMES**

Drawing Title
**HIGHWAY
 PRELIMINARY DESIGN
 LONG - SECTIONS
 SHEET 6 OF 13**

Designed AZ	Drawn AZ	Checked DR	Approved KC	Date 17/12/20
Internal Project No. 60632497	Suitability S2	Discipline Civil - Highways		

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Drawing Number RIV_PD	Work Package ID -ACM	1 Originator -HGA	1 Volume -CH-0028	Rev P02
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Plot Date: 18 December 2020 15:29:46
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 - 1 IN 100 YEAR FLOOD LEVEL INCLUDES +70% CLIMATE CHANGE.
 - FOR FINISHED ROAD LEVELS AND CHAINAGES REFER TO THE HIGHWAYS SERIES DRAWINGS.

SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION

THE FOLLOWING NOTES HIGHLIGHT SIGNIFICANT RESIDUAL HAZARDS IDENTIFIED BY THE DESIGNER. TYPICAL HAZARDS THAT SHOULD BE IDENTIFIED BY A COMPETENT CONTRACTOR ARE NOT INCLUDED. THE CONTRACTOR SHALL CARRY OUT THE WORKS USING AN APPROVED SAFE SYSTEM OF WORK. FURTHER INFORMATION ON RESIDUAL HAZARDS CAN BE FOUND ON THE SID WORKBOOK RESIDUAL HAZARD LOG.

USE OF WARNING TRIANGLES ON DRAWINGS

REF	HAZARD

SIGNIFICANT HAZARDS PRESENT THROUGHOUT THE EXTENT OF THE WORKS WHICH ARE DEEMED BY THE DESIGNER TO BE UNAVOIDABLE AND CANNOT BE ELIMINATED OR REDUCED ARE LISTED IN THE TABLE ABOVE. WHERE PRACTICAL THE LOCATION OF INDIVIDUAL HAZARDS ARE INDICATED ON THE DRAWING. FOR FURTHER DETAILS, SEE RESIDUAL DESIGN HAZARD REGISTER IN PRE-CONSTRUCTION INFORMATION.



REVISION DETAILS	By	Date	Suffix

INITIAL STATUS OR WIP



Client: OXFORDSHIRE COUNTY COUNCIL
 Project Title: DIDCOT GARDEN TOWN HIF 1 SCHEMES PRELIMINARY DESIGN

Drawing Title: RIVER THAMES CROSSING OPTION STUDY GENERAL ARRANGEMENT SHEET 1

Designed	Drawn	Checked	Approved	Date
GT	---	---	---	---

Internal Project No: 60632497
 Suitability: S0
 Scale @ A1: AS SHOWN
 Discipline: Civil - Bridge and Structures

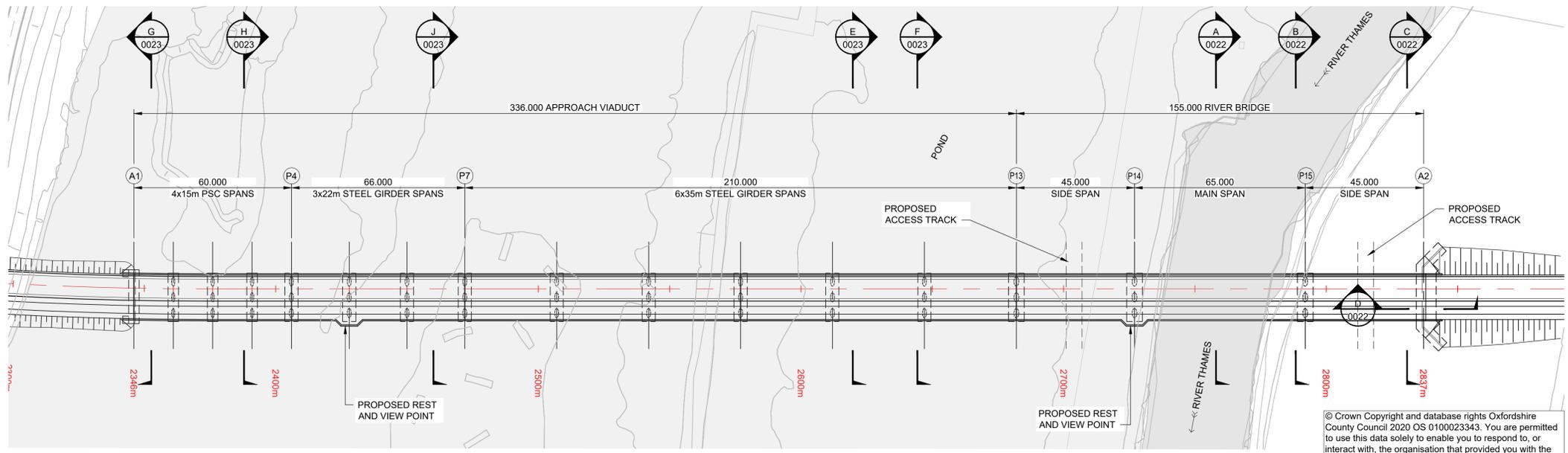
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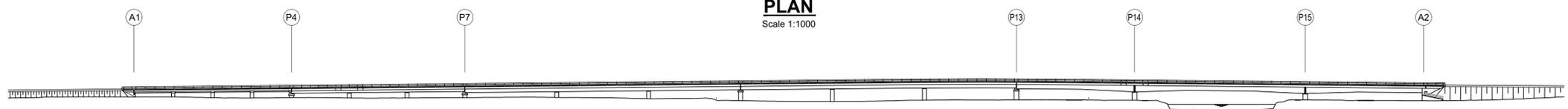
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Drawing Number	Work Package ID	1 Originator	1 Volume	Rev
RIV_PD	-ACM	-SBR	-	P01.2

Location: DGT_STR_ZZ_ZZ-DR-CB-0021
 I Type: 1 Role: 1 Number: 1



PLAN
 Scale 1:1000



ELEVATION OF VIADUCT
 Scale 1:250

CHECK PRINT		
CHECK	DATE	INITIAL
DRAFTING		
ENGINEERING		
CORRECTIONS COMPLETED		

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