

The Network Rail (Leeds to Micklefield Enhancements) Order**TRANSPORT AND WORKS ACT 1992****Transport and Works (Applications and Objections
Procedure) (England and Wales) Rules 2006****THE NETWORK RAIL (LEEDS TO MICKLEFIELD
ENHANCEMENTS) ORDER****DOCUMENT NR19: STATEMENT OF CASE FOR THE
APPLICATION****Rule 10(2)(d)**

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**The Network Rail (Leeds to Micklefield Enhancements) Order
Statement of Case NR19
17 November 2023**

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1. GENERAL INTRODUCTION

1.1 Structure of Statement of Case

- 1.1.1 On 17 July 2023, Network Rail Infrastructure Limited ("**Network Rail**") applied to the Secretary of State for Transport ("**SoS**") pursuant to section 6 of the Transport and Works Act 1992 ("**the TWA**") for an order known as the Network Rail (Leeds to Micklefield Enhancements) Order ("**the Order**") (**NR02**) under sections 1 and 5 of the TWA.
- 1.1.2 The Order, if made, would enable Network Rail to carry out works necessary to ensure that the upgrade and electrification of this part of the North Transpennine Route ("**NTPR**") can be delivered, and is required to ensure that the full benefits of the wider Transpennine Route Upgrade ("**TRU**") programme can be achieved.
- 1.1.3 The TRU comprises a series of projects between Manchester to Leeds and Leeds to York along the NTPR. The projects are shown in Figure 1 (Transpennine Route Upgrade Project Areas) below and are at different stages of development and delivery. For example, Manchester Victoria to Stalybridge (W1) and York to Church Fenton (part of Project E1) are currently being delivered with extensive works already undertaken. The Huddersfield to Westtown Project (W3) Transport & Works Act Order was approved and made in 2022 and is now in delivery. In approving the TWAO, the Secretary of State (SoS) acknowledged in their decision letter that train services regularly encounter congestion and delays on the Transpennine route, resulting in performance and reliability issues for those services. The SoS agreed with the Inspector's view that NTPR does not currently meet the needs of passengers and like the Inspector, the SoS agrees with Network Rail that NTPR is in urgent need of improvement.

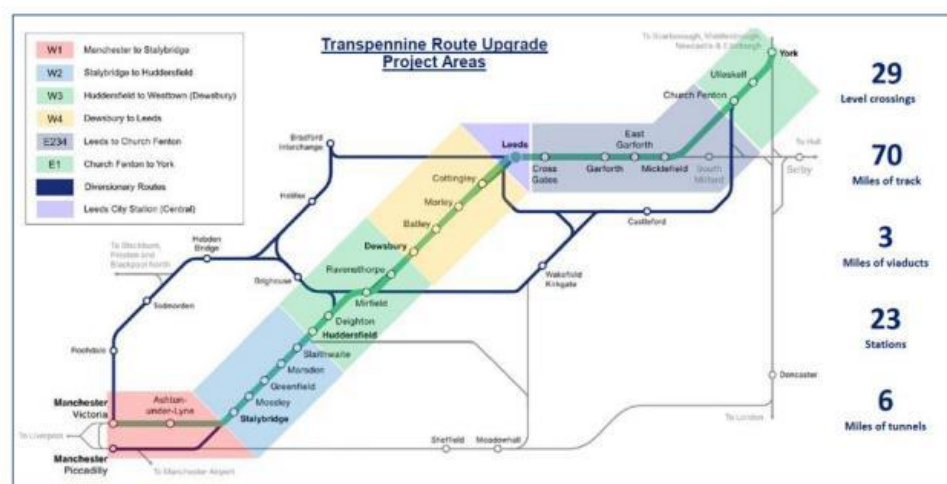


Figure 1 - Transpennine Route Upgrade Project Areas

- 1.1.4 Each project contributes to the overall upgrade of the TRU programme and together deliver the key outputs of the programme which are detailed in Section 3.
- 1.1.5 The Order (if authorised) will facilitate significant part of the works within the project areas E2-E4, including further works on the Leeds to York section of the NTPR. The works that are not consented or facilitated by the Order but form part of the wider TRU programme within the Leeds to Church Fenton section of the TRU programme will be referred to as the "**E2 to E4 Project**". The Order however only includes part of the works required for E2-E4 Project.
- 1.1.6 The wider programme of works East of Leeds (including the E1 – E4 project) is being delivered through a variety of planning mechanisms, including using Network Rail's permitted development rights, applications under the Town and Country Planning Act (TCPA) and promotion of three Transport and Works Act Orders (including the present Order).
- 1.1.7 In broad terms, the Order (if made) would authorise:
- a) the acquisition of land and rights in land for the construction of works;
 - b) the extinguishment of private rights of way;
 - c) the demolition, reconstruction, and construction of overbridges and highways;
 - d) the power to execute street works, temporarily and permanently stop up and/or divert, footpaths and highways, alter the layout of streets, provide access to works, make traffic regulations and provisions relating to streets; and
 - e) the closure of 5 level crossings.
- 1.1.8 The Order list 6 specific works in Schedule 1 of the Order ("**the Scheduled Works**") which would be authorised under article 7(1) of the draft Order.
- 1.1.9 The Scheduled Works, other works and land uses which would be authorised under the Order or other consents applied for alongside the Order will be referred to as "**the Order Scheme**".
- 1.1.10 The wider programme of works and the land uses which would be facilitated or enabled by the Order, which are authorised either pursuant to the Order and request for deemed planning permission or under permitted development rights including prior approvals and/or separate planning permissions, are referred to as "**the Scheme**".

- 1.1.11 The application for the Order is accompanied by a request for deemed planning permission under s.90 of the Town and Country Planning Act 1990 (**NR12**) for the works authorised under the Order. Separately, 4 applications for listed building consents were submitted to Leeds City Council ("**LCC**") in connection with proposed works to 4 listed structures: Austhorpe Road Bridge, Brady Farm Bridge, Crawshaw Woods Bridge and Ridge Road Bridge. These applications have now been referred to the Secretary of State for Levelling Up, Housing and Communities pursuant to Section 12 of the Planning (Listed Buildings and Conservation Areas) Act 1990, it is anticipated these will be considered by the Inspector as part of the public inquiry for this Order. The Scheme would be delivered through a combination of authorisations granted by the Order, deemed planning permission, permitted development rights under the Town and Country Planning (General Permitted Development) (England) Order 2015 ("**GDPO**"), separate planning permissions and the listed building consents.
- 1.1.12 The level crossings within the Order limits are a significant constraint on the proposed upgrades to this part of the NTPR and will introduce a higher level of risk to those that would use them that must be addressed if those improvements are to come forward. The closure of the level crossings under the Order is necessary if level crossing risk is to remain as low as reasonable practicable (ALARP) and, in turn, help deliver an increase in capacity and improvement in journey times and performance reliability of railway services on the NTPR. The closures and any replacement of public rights of way will also remove the risk posed to users of the level crossing, and those using the railway services, in line with Network Rail and Office of Rail and Road (ORR) objectives.
- 1.1.13 A table setting out each element of the Order Scheme is appended to this Statement of Case at Error! Reference source not found..
- 1.1.14 Some of the elements of the Order Scheme are associated with a proposed interim timetable, with improved journey times and service frequency, which is proposed for December 2025. All must be delivered if the proposed electrification between Neville Hill West and Church Fenton and the 2028 timetable change are to go ahead. Further details are provided in Section 9 of this Statement of Case.
- 1.1.15 The Scheme is therefore an important contributor towards the delivery of the objectives for the TRU programme.
- 1.1.16 The period during which objections, representations and letters of support could be made to the Order application ended on 24 August 2023. The

Department for Transport (**DfT**) received 30 objections, 7 representations and 3 letters of support during this period.

- 1.1.17 As a consequence of the objections and representations, and in accordance with the Transport and Works (Inquiries Procedure) Rules 2004 SI No. 2018 (**“the TWA Inquiries Rules”**), the SoS announced on 6 October 2023 their intention to hold a Public Inquiry (**“the Inquiry”**) into the application.
- 1.1.18 Rule 7 of the TWA Inquiries Rules requires Network Rail to provide a Statement of Case. This Statement of Case contains the full particulars of the case Network Rail intends to make at the Inquiry in support of the Order application.
- 1.1.19 Appendix 1 to this Statement of Case provides a list of the documents which Network Rail currently intends to refer to or submit in evidence at the Inquiry. These documents will be available for public inspection at the locations and times set out in Section 16 of this Statement of Case. Network Rail reserves the right to add to the core document list.
- 1.1.20 In this Statement of Case, references to the core supporting documents listed in Appendix 1 are shown in bold type.

1.2 Structure of the Statement of Case

- 1.2.1 This Statement of Case is structured as follows:
- Section 1: General Introduction
 - Section 2: Background
 - Section 3: The Applicant and the Application
 - Section 4: Consents & Authorisations Sought
 - Section 5: Level Crossings
 - Section 6: The Case for the Scheme
 - Section 7: Policy Framework
 - Section 8: Scheme Development
 - Section 9: Scheme Description and Construction
 - Section 10: Environmental Effects

- Section 11: Historic Environment
- Section 12: Open Space
- Section 13: Land and Property
- Section 14: Consultation and Engagement
- Section 15: Objections, representations and statement of support
- Section 16: Notice as regards core documents

2. BACKGROUND

2.1 Transpennine Route Upgrade

- 2.1.1 The TRU is a multi-billion-pound, transformative, long-term railway infrastructure programme that will improve connectivity in the North, principally West-East, between the major economic centres of the northwest, Yorkshire-Humber and the northeast. It will support economic growth in the North and deliver benefits for passengers and communities along the route and beyond.
- 2.1.2 Due to the size, differences, and complicated nature of the works required to upgrade the existing rail infrastructure between Manchester and York, TRU is being delivered as a phased programme of interventions with those interventions being delivered as separate projects, alongside each other. As indicated in chapter 1, the Order Scheme and Scheme are part of the E2 to E4 Project. The E2 to E4 Project comprises, in broad terms, the electrification of the line, Journey Time Improvements (JTIs) through line speed increases, increased capacity as a result of the JTI, electrification and signalling and enhanced resilience and reliability of the line between West of Cross Gates Station in Leeds and a point just to the southwest of Church Fenton.
- 2.1.3 The location of all the TRU projects are shown in Figure 1(Transpennine Route Upgrade Project Areas 1) and Figure 2 (Maturity of the Projects) below and are at varying stages of maturity and delivery.

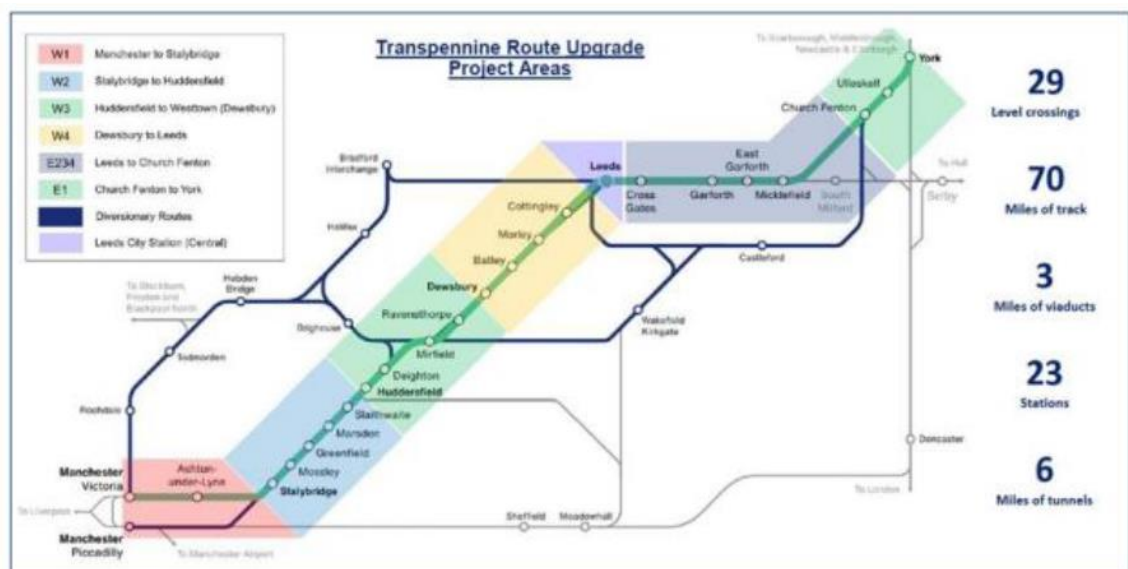


Figure 2 - Transpennine Route Upgrade Project Areas

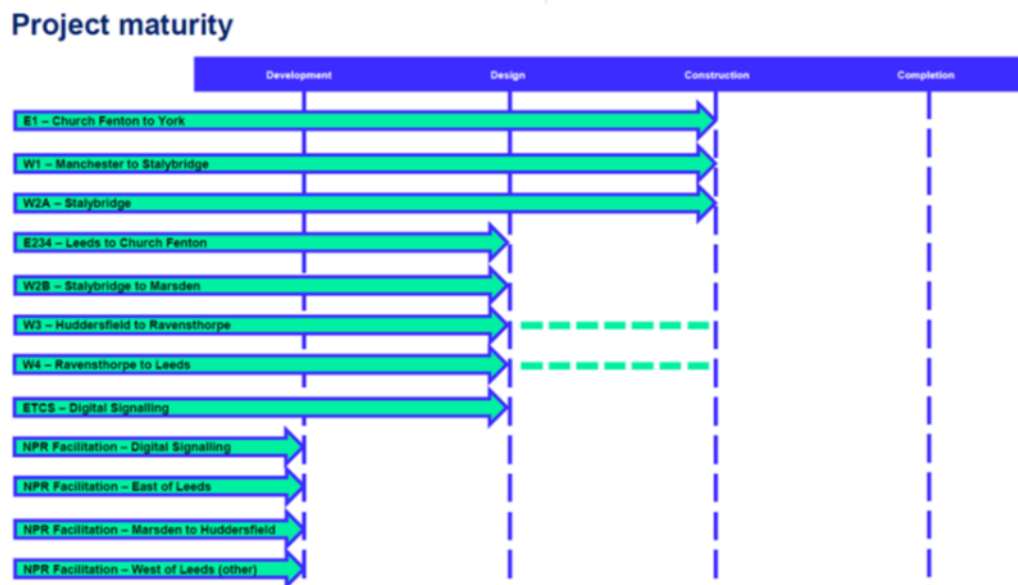


Figure 3 - Maturity of Projects by Project

2.1.4 Each project contributes to the overall upgrade of the Transpennine Route and together deliver the key outputs of the TRU Programme which are:

- An improved journey time for Leeds – Manchester Victoria of 43-44mins;
- An improved journey time for York to Manchester Victoria of 67-69mins;
- Capability to operate 8 ‘express services’ an hour on the route. (This scheme delivers on this capacity improvement aim by installation of modern signalling, electrification of the lines, and line speed increases);
- Capability to operate 6 ‘local services’ an hour on the route. (This scheme delivers on this capacity improvement aim by installation of modern signalling, electrification of the line, and line speed increases);
- Performance of the Transpennine Route to be 92.5%¹ (Public Performance Measure) or higher each period. (The Scheme delivers on this reliability aim by removal of the level crossings and providing upgraded modern railway equipment throughout the Scheme Route);
- Freight paths/rights to be retained as existing. (This scheme delivers on this aim by delivering improved capacity and resilience on the railway); and

¹ PPM is a measurement of train punctuality and arrival into destination stations within 5 minutes of timetabled time.

- A contribution to Network Rail's Decarbonisation Strategy and climate policy. (This scheme contributes to Network Rail's strategy and policy by delivering the electrification of the railway throughout the Scheme Route).

2.1.5 Transport plays a vital role in enabling a modern trading economy, and rail has a particular advantage in connecting cities, both to each other and to their markets. The NTPR – the subject of the TRU – is the key East-West artery across the Northern economy. As shown in Figure 3 (Transpennine Route) below, it forms the most direct existing rail link between Manchester, Leeds and York, but is also used as a “spine” to link a wider set of economic centres such as Newcastle and Hull in the East, and Liverpool in the West. As well as linking city centres, the line also joins these to smaller towns and commuting areas, and key sites such as Manchester Airport and universities and research centres (including in smaller hubs like York and Huddersfield). The route also serves an important economic function in terms of supporting freight flows (typically 5-10 trains over the Transpennine section per day) as one of the most direct East-West corridors across the North.

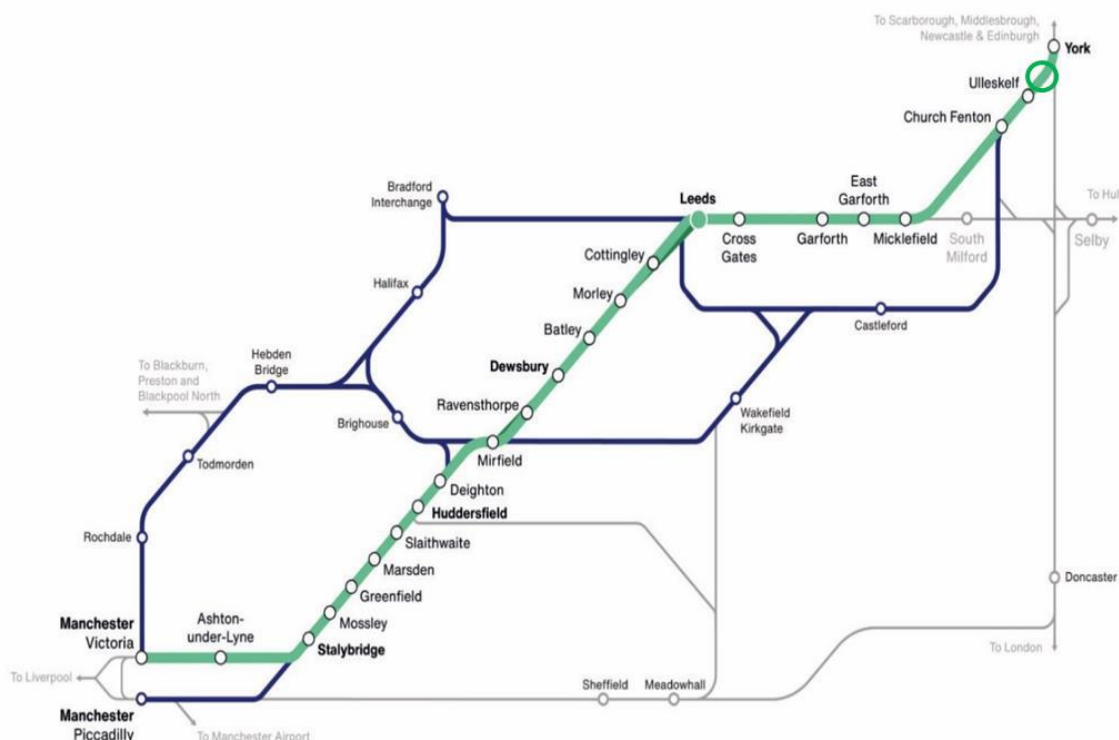


Figure 4 - Transpennine Route

2.1.6 Stretching across the North of England between York and Manchester, via Leeds and Huddersfield, the 76-mile NTPR serves 23 stations, crosses over and dips under 285 bridges and viaducts, passes through six miles of tunnels, and crosses over 29 level crossings.

- 2.1.7 The Leeds to Micklefield section of the NTPR ("**Scheme Route**") is a two-track railway. The route diverges at Micklefield with trains heading East to Hull and North to York, Newcastle and Scotland. The section of line from Micklefield to Church Fenton is also a two-track railway.
- 2.1.8 The NTPR is not currently well-placed to deliver a key enabling role in levelling up the Northern conurbations and making them a more coherent and productive economic entity. Up to the outbreak of the COVID pandemic, demand on the route had doubled to 50 million journeys per year since the mid-1990s (see Figure 4 (Passenger Journeys by Train Operating Companies) below), but the historic reduction in the carrying capacity of the infrastructure meant the route has reached the practical limit of its capacity. Pre-COVID, the route performed poorly, and it will form a constraint, rather than an enabler, to building back the Northern economy.
- 2.1.9 Following discussions between DfT and Network Rail, it is anticipated, and to be planned for, that rail passenger numbers and demand will return to pre COVID-19 levels under the medium-demand scenario and continue their original growth trajectory once the pandemic-related impacts have passed.
- 2.1.10 As a result, it is highly likely that use of the NTPR will come to surpass pre COVID levels before the TRU programme, including the E2 to E4 Project, is completed and operational by the end of 2028.
- 2.1.11 Overall productivity in the North trails behind the UK average. For the last 30 years, the North's economic value per person, measured as Gross Value Added (GVA) has been consistently around 15% below the average for the rest of the UK. Pre-COVID data showed that the gap had widened further with the GVA per person in the North 18% below the UK average. The widening gap can be attributed to the North generally experiencing slower GVA growth rates over the last decade compared to the UK average, with evidence showing low levels of transport investment being a contributing factor to the low levels of growth².

²<https://www.ons.gov.uk/employmentandlabourmarket/peopleinwork/labourproductivity/articles/regionalandsubregionalproductivityintheuk/february2019>

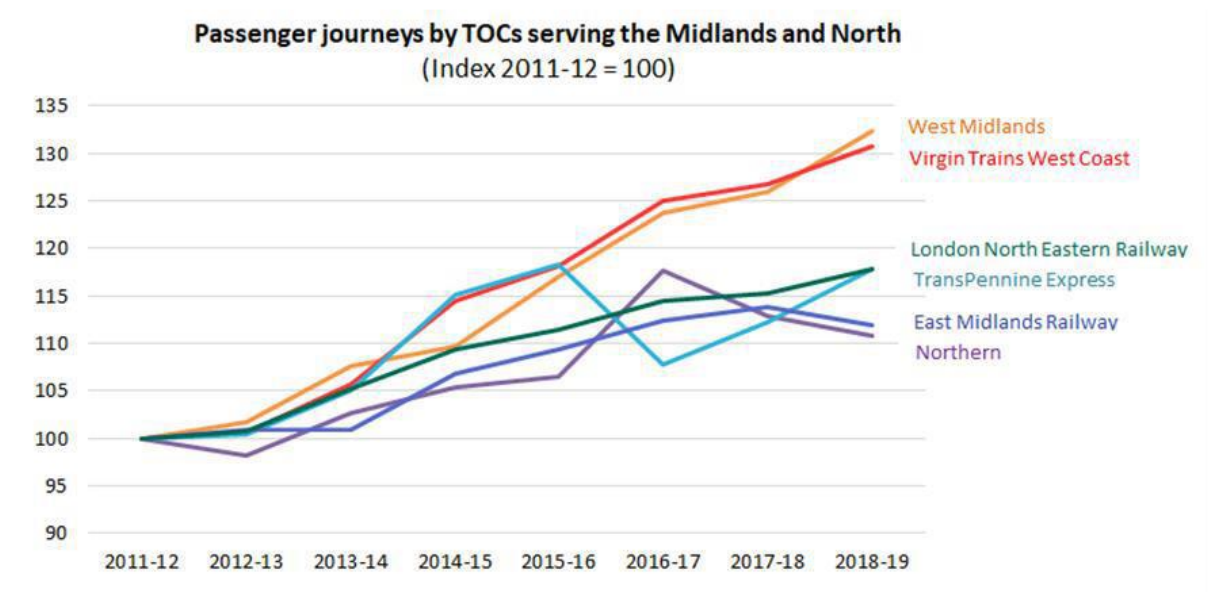


Figure 5 - Passenger Journeys by Train Operating Companies

2.1.12 The amount of investment historically of the NTPR has not matched that of the East Coast Main Line or West Coast Main Line. Nevertheless, in offering much improved rail services and enhancing their daily lives, rail investment on the Manchester -Leeds- York corridor can contribute strongly to the “levelling up” agenda – both in terms of facilitating post- COVID economic recovery and growth but in offering citizens in the North a brighter economic future.

3. THE APPLICANT AND THE APPLICATION

3.1 The Applicant

- 3.1.1 Network Rail is the applicant for the Order. Network Rail owns and operates the rail infrastructure network of Great Britain ("**the Network**"). Network Rail's purpose is to provide a safe, reliable, economic and efficient railway in Great Britain.
- 3.1.2 Network Rail is primarily responsible for the operation, maintenance, repair and renewal of track, stations, signalling and electrical control equipment. Train services on the Network are operated by Train and Freight Operating Companies ("**TOCs**" and "**FOCs**"). Network Rail, as owner of the Network, grants rights to use the track, stations, depots and access through contacts approved by the Office of Rail and Road ("**ORR**").
- 3.1.3 The activities of Network Rail as the network operator are regulated by the ORR through a network licence granted under Section 8 of the Railways Act 1993. The Network licence requires Network Rail to secure (a) the operation and maintenance of the Network; (b) the renewal and replacement of the Network; and (c) the improvement, enhancement and development of the Network, in each case in accordance with best practice and in a timely, efficient and economical manner so as to satisfy the reasonable requirements of funders and rail-related service providers.

3.2 The Application Documents

- 3.2.1 The application for the Order and associated deemed planning permission comprise the following documents as required by the Transport and Works (Applications and Objections Procedure) (England and Wales) Rules 2006 ("**2006 Rules**").
- Copy of the Application letter (**NR01**)
 - Draft Order (**NR02**)
 - Explanatory Memorandum (**NR03**)
 - Statement of Aims (**NR04**)
 - Funding Statement (**NR05**)
 - Estimate of Costs (**NR06**)
 - Consultation Report (**NR07**)

- Book of Reference (**NR08**)
- Land Plans (**NR09**)
- EIA Screening Decision (**NR10**)
- Rule 18 Waiver (**NR11**)
- Request for Deemed Planning Permission (**NR12**)
- Planning Statement (**NR13**)
- Planning Drawings (**NR14**)
- Design and Access Statement (**NR15**)
- Environmental Report (**NR16**)
- Code of Construction Practice (**NR17**)
- List of Consents, Permission or licences under other enactments (**NR18**)

3.3 The Order Scheme

- 3.3.1 As indicated above, whilst the Order would authorise a number of works authorised under the Order and/or its associated consents, it is also required to facilitate other works which are being undertaken within the E2 to E4 Project pursuant to permitted development rights (or other consents). The Order Scheme will be constructed on land which consists of both operational railway land and land outside of Network Rail's ownership, and in respect of which it seeks power pursuant to the Order to either compulsorily acquire (land or rights) or temporarily use land, together with the ability to carry out highway alterations, stopping up and diversion of roads, footpaths and bridleways (temporary and permanent) and close level crossings.
- 3.3.2 In its consultation material preceding the application, Network Rail broke down the Order Scheme into 17 elements by geographical location running west to east (Leeds to Micklefield). Figure 6 (Elements in their geographical location) below shows each of these elements in their geographical locations.
- 3.3.3 Those 17 elements are summarised below, with further detail provided within Section 9 of this Statement of Case.

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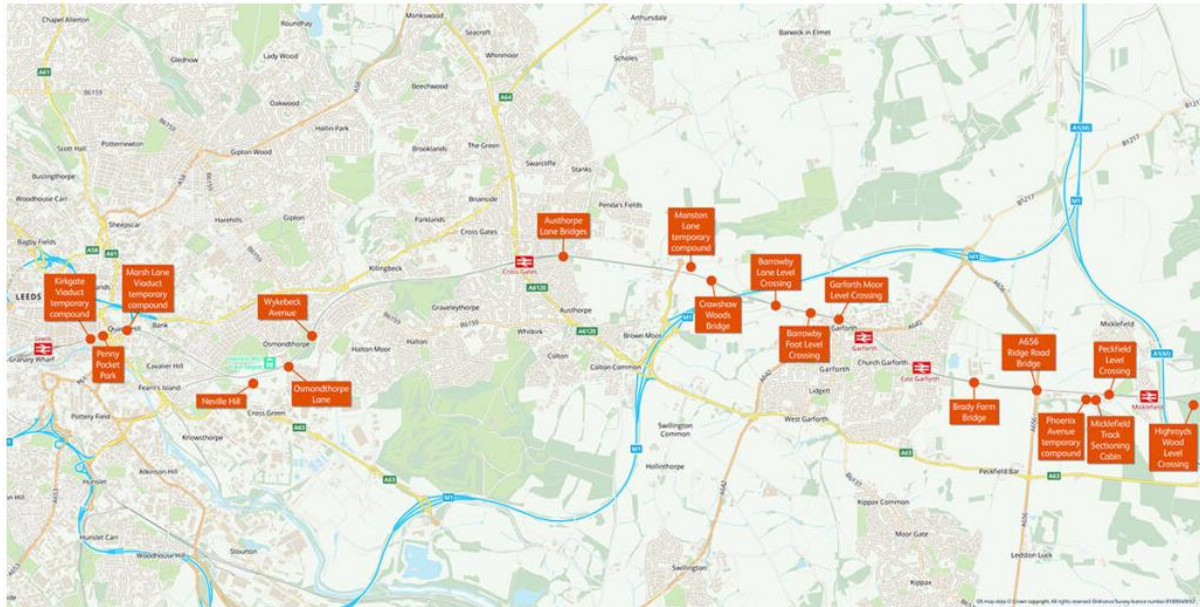


Figure 6 – Elements in their geographical location

Kirkgate Viaduct Temporary Compound

- 3.3.4 The temporary compound is required in connection with works to an existing bridge structure at Kirkgate. This is a single span bridge structure sitting within the larger elevated structure carrying the railway from Leeds Station East junction to approximately 80m west of Shannon Street footbridge. This bridge is to be replaced with a new modern bridge to accommodate the increased dynamic loads of the proposed line speed increase. The replacement of this bridge will be carried out under permitted development rights and would not be authorised under the draft Order or deemed request for planning permission.
- 3.3.5 Network Rail has secured prior approval (23/03890/DPD) pursuant to Part 18 of Schedule 2 of the GDPO for the alterations to Kirkgate Viaduct. Prior approval was granted on 16 August 2023 (**NR20**).
- 3.3.6 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Kirkgate to Marsh Land (also known as Penny Pocket Park)

- 3.3.7 Penny Pocket Park is referred to in the supporting TWAO documents as the Kirkgate to Marsh Land. This is located on a railway embankment in central Leeds, approximately 1.6km east of Leeds Railway Station. The railway embankment forms part of an approximately 1ha public park known as Penny Pocket Park.

- 3.3.8 The Order would authorise the installation of electrification and signalling infrastructure works to be carried out alongside the existing railway track at Penny Pocket Park, together with the permanent acquisition of land needed for these works (4 small parcels of land comprising approximately 190 square metres).
- 3.3.9 The land to be permanently acquired is public open space, which in total will not exceed 209 square metres; replacement land is therefore not required. A separate application pursuant to Section 19 of the Acquisition of Land Act has been made to the Department of Levelling Up, Housing and Communities for the requisite certificate to be granted.

Marsh Lane Viaduct Temporary Compound and Construction Land

- 3.3.10 Marsh Lane Viaduct Temporary Compound and Construction Land is adjacent to Marsh Lane Viaduct in Leeds City centre. The land identified in the land and works plans is required for a temporary compound and construction site required to carry out the replacement of the bridge over Marsh Lane which, is to be replaced with a new modern bridge. The replacement of this bridge (which carries the railway over the highway known as Marsh Lane) will be carried out under permitted development rights (Part 18 of Schedule 2 of the GDPO) and would not be authorised under the draft Order or deemed request for planning permission.
- 3.3.11 The Order will authorise Network Rail to temporarily take possession of the land required for the compound together with construction access with the ability to temporarily stop up streets in order to facilitate the Marsh Lane bridge works.

Neville Hill Access Land

- 3.3.12 Neville Hill Access Land comprises a parcel of land (approximately 0.195ha) at the end of Newmarket Approach which is an adopted highway in Leeds. The land consists of areas of grass verge and a footpath connecting Newmarket Approach to a non-definitive bridleway which travels east to west between Halton Moor Road and Pontefract Lane
- 3.3.13 The Order would authorise Network Rail to acquire a strip of land off Newmarket Approach in order to facilitate the provision of a new access from the highway to the Neville Hill railway sidings. The Order would also permit the temporary use of land as a work site in order to facilitate the construction of the new access road.
- 3.3.14 Network Rail has applied separately to Leeds City Council for planning permission (23/03522/FU) for the construction of a new access road and

shared-use footpath/cycleway off the existing Newmarket Approach to enable this new vehicular access to Neville Hill South Sidings to be provided. The application is yet to be determined by the Local Planning Authority. The consultation period ended on 19 September 2023 and the statutory period for determination has now expired.

Osmondthorpe Lane

- 3.3.15 Osmondthorpe Lane is located to the east of the Neville Hill train maintenance depot. Land at Osmondthorpe Lane will be required as a temporary compound in order to facilitate works required for the construction of a replacement underbridge at Osmondthorpe Lane.
- 3.3.16 The construction of the replacement bridge itself would be carried out under permitted rights (Part 18 of Schedule 2 of the GDPO). Prior approval was granted by a decision ref. 23/00903/DPD issued on 2 May 2023 (**NR21**).
- 3.3.17 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Wykebeck Avenue Temporary Compound

- 3.3.18 Wykebeck Avenue is located in the Osmondthorpe area of Leeds. The land at Wykebeck Avenue is required as a temporary compound (including storage) to enable track renewal and overhead line clearance works and to enable upgrade of tracks and electrification of the line in that area. The Order will authorise the temporary use of the land for this compound. The works supported by the compound would be carried out under permitted development rights, within Network Rail's operational land.
- 3.3.19 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Replacement of Austhorpe Lane Bridge & Austhorpe Lane Gas Main Diversion

- 3.3.20 Austhorpe Lane Bridge is located 0.6km east of Cross Gates Railway Station. It is a Grade II Listed structure with a single carriageway public highway crossing over it. Austhorpe Lane Footbridge runs immediately adjacent and parallel to the road bridge on the western side of the bridge and is a PRow.
- 3.3.21 Austhorpe Lane Bridge is to be reconstructed to enable the installation of overhead electrical wires for electrification of the line. A new two-lane

carriageway and 2-metre footway on the western side is to be constructed. This is Scheduled Work No 1 in the draft Order.

- 3.3.22 The Gas Main Pipe Bridge is a high-pressure gas main attached to Austhorpe Lane Overbridge on the eastern side of the bridge, connecting into the buried gas main system either side of the bridge. The gas main will be diverted under the railway line via a bored tunnel. This is Scheduled Work No 2 in the draft Order.
- 3.3.23 Listed building consent has been applied for under application number 23/04387/LI, for the demolition of the existing Austhorpe Lane bridge (including the adjacent footbridge and gas pipeline) and a new bridge will be erected in its place.
- 3.3.24 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound areas north-west and south-east of Austhorpe Lane Overbridge would be permitted under Class A of Part 4 of Schedule 2 of the GDPO.
- 3.3.25 The Order will authorise the temporary possession for the access for construction and working site and the right to temporarily stop up streets, whilst those works are being carried out.

Manston Lane Temporary Compound

- 3.3.26 Manston Lane is located between the A6120 and the M1 at the eastern edge of Leeds. Manston Lane temporary compound will be used to enable track renewal and overhead line clearance works and to enable upgrade of tracks and electrification of the line.
- 3.3.27 The Order will authorise the temporary possession of the land for construction and access.
- 3.3.28 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Crawshaw Woods Bridge Raising

- 3.3.29 Crawshaw Woods Overbridge is located approximately 2.5km east of Cross Gates Railway Station and 0.3km to the north of the M1 motorway. It is a Grade II Listed structure with private vehicle rights and a public right of way running over it.
- 3.3.30 Works to Crawshaw Woods Bridge form part of the application for deemed planning permission and comprise Scheduled Work No.3. The works

comprise the repair and restoration of the original cast iron framework and reinstatement at a higher level, which is required in order to enable the installation of OLE. The bridge abutments will be increased in height through the re-use of original stone from Crawshaw Woods Bridge and from the demolition of Brady Farm, Austhorpe Lane and Ridge Road bridges. The existing bridge parapet is a modern addition and will be replaced in the new design with a more historically in-keeping design. The Order includes the bridge works and an access point from the public highway.

- 3.3.31 Temporary use of land will be required as a construction access and construction compound north and south of the railway in order to facilitate the carrying out of the works to Crawshaw Woods Bridge. The Order will authorise the temporary use of land for this purpose.
- 3.3.32 Network Rail has also applied for listed building consent (23/04388/LI) for the dismantling of the superstructure of the bridge, the raising of the abutments by 1.4 metres and the replacement of the refurbished cast iron superstructure at the higher level.
- 3.3.33 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

New Barrowby Lane Bridge & Closure of Barrowby Lane Level Crossing & Barrowby Foot Level Crossing

- 3.3.34 Barrowby Lane and Barrowby Foot Level Crossings are located to the west of Garforth.
- 3.3.35 Barrowby Lane Level Crossing is a bridleway crossing that connects Barrowby Lane to the south of the railway, leading to Nanny Goat Lane to the north of the railway. The crossing is located 1.2m west of Garforth Railway Station.
- 3.3.36 Barrowby Foot Level Crossing is a public footpath crossing, accessed by steps located approximately 430m to the east of Barrowby Lane Level Crossing. It connects Barrowby Lane to the south of the railway to Nanny Goat Lane to the north of the railway.
- 3.3.37 Barrowby Lane and Barrowby Foot Level Crossings will be closed. Pedestrians, cyclists and horse riders will be diverted across the railway via a new accessible ramped bridleway bridge close to the location of the existing Barrowby Lane Level Crossing.

- 3.3.38 The works to construct the new bridge form part of the application for deemed planning permission and comprise Scheduled Work No 4 in the draft Order.
- 3.3.39 Land is also required for construction access and as a temporary compound. The Order will authorise the temporary possession of land for these purposes.
- 3.3.40 The Order will also authorise the extinguishment and diversion of public footpaths and bridleways in this area (associated with the closure of the level crossings) together with the creation of a new right of way over the western section of Nanny Goat Lane in Garforth (where a PROW already exists over the eastern section), together with the temporary use of land to facilitate its provision.

Garforth Moor Level Crossing

- 3.3.41 Garforth Moor Level Crossing is located in Garforth. Garforth Moor Level Crossing is currently temporarily closed, but the Order will enable it to be closed permanently and for any private and public rights over the level crossing to be extinguished.
- 3.3.42 No physical works are required to close the crossing, as following its temporary closures, all crossing equipment has been removed and a boundary fence has been installed either side.
- 3.3.43 Powers are also sought under the Order to formalise the current access arrangements being used by the Garforth Bank Row Allotment holders to access the allotments from Barwick Road. The Order will authorise the grant of new rights over an existing track for users of the allotments for the purposes of accessing the allotments, together with the right to upgrade the existing access track to maintain it and for vehicle parking.
- 3.3.44 Network Rail has applied for (retrospective) planning permission (22/03133/FU) for a permanent access track and parking area/turning head to service the adjacent allotments. No other infrastructure changes are proposed.

Brady Farm Bridge Temporary Compound

- 3.3.45 Brady Farm Overbridge is located at Sturton Grange in Leeds and is a Grade II listed overbridge. It will be demolished pursuant to permitted development rights under Class B, Part 11, Schedule 2 of the GDPO and works to the bridge are not therefore included within the draft Order or the application for deemed planning permission. Network Rail has applied for listed building consent application for the demolition of the bridge (23/04389/LI) which

Network Rail understands will be considered at the Inquiry into the draft Order.

- 3.3.46 A temporary compound is required to facilitate and support the works associated with the demolition of Brady Farm Bridge (which is required for installation of overhead wires). The Order will authorise the temporary use of land adjoining the bridge for this purpose and for construction access.
- 3.3.47 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Replacement Ridge Road Bridge and Ridge Road Gas Main Diversion

- 3.3.48 Ridge Road bridge is located in Micklefield, Leeds and is a Grade II Listed bridge. The bridge currently carries the A656 public highway across the railway. There is also a high-pressure gas main that runs via a pipe bridge next to the road bridge. The bridge is to be demolished and reconstructed at a higher level to enable installation of overhead wires for electrification of the line. Those works will also necessitate the diversion of the high-pressure gas main.
- 3.3.49 The works to construct the bridge form part of the application for deemed planning permission and are Scheduled Work No. 5 with the works to divert the high-pressured gas main comprising Schedule Work No.6.
- 3.3.50 Network Rail has applied for listed building consent (23/04390/LI) for the demolition of the existing Ridge Road bridge and the new bridge to be erected in its place.
- 3.3.51 The Order will authorise Network Rail to temporarily possess land for construction and access in order to facilitate the carrying out of these Scheduled Works.
- 3.3.52 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Phoenix Avenue Temporary Compound

- 3.3.53 Phoenix Avenue is located in Micklefield, Leeds. Phoenix Avenue Temporary Compound will be used to enable track renewal and overhead line clearance

works and to enable upgrade of tracks and electrification of the line, along with the works associated with the closure of Peckfield Level Crossing and the Micklefield Track Sectioning Cabin.

- 3.3.54 The Order will authorise the temporary use of land as a compound.
- 3.3.55 Any buildings, works and plant or machinery required in connection with the use of the required temporary compound will be authorised under Class A of Part 4 Schedule 2, of the GDPO.

Micklefield Track Sectioning Cabin

- 3.3.56 A new track sectioning cabin is needed to safely regulate the power supply to the overhead electric wires in the local area and thus to enable electrification of the line. It will be located 50 metres to the west of the existing Peckfield Level Crossing.
- 3.3.57 The Order would also authorise the permanent acquisition of land required for the construction of the TSC. The TSC forms part of the application for deemed planning permission.

Peckfield Level Crossing

- 3.3.58 Peckfield Level Crossing is located to the south end of Micklefield. It is a bridleway crossing that connects Pit Lane (south of the railway) to Lower Peckfield Lane (north of the railway).
- 3.3.59 The Order will authorise the closure of Peckfield Level Crossing and diversion of the existing PROW. Two alternative options for diversion are proposed, with the works required for both diversions being the subject of the application for deemed planning permission.
- 3.3.60 The options proposed are
- a) Option 1:
- The creation of a new public right of way footpath between the Great North Road and Pit Lane to the north of the Railway to provide pedestrian access to residential properties and a link to the (existing) PROW on Lower Peckfield Lane. From the residential properties, the footpath runs parallel with the railway, along the boundary of the recreation ground, to where it joins Great North Road.
 - The Lower Peckfield Lane Highway Works to upgrade Lower Peckfield Lane to the north of the railway (including three passing

points) and to provide a small car parking area for residents of railway cottages.

b) Option 2:

- Is almost identical to Option 1, but the footpath in Option 1 would instead be a bridleway.

3.3.61 In addition to extinguishing current PRow and authorising the creation of the new PRow, the Order will authorise the permanent acquisition of the land to enable the improvements to Lower Peckfield Lane to be undertaken and to temporarily use land for carrying out the works and access for construction.

Highroyds Wood Level Crossing

3.3.62 Highroyds Wood Level Crossing is located in Micklefield and is currently temporarily closed under a TTRO on safety grounds, so it is not presently accessible for public use.

3.3.63 The Order will authorise Highroyds Level Crossing to be closed permanently, and the PRow Micklefield 7 to be extinguished over the level crossing.

3.3.64 The footpath will be diverted east via a new path to an existing railway underpass. The proposed new footpath crosses the local authority boundary into North Yorkshire and links with public footpath Huddlestone with Newthorpe No 35.39/2/1.

3.3.65 The closure of Highroyds Wood Level Crossing will not require any planning permission as any fencing/removal of deck would be carried out permitted developments rights.

3.3.66 This level crossing closure and PRow diversion proposal is also the subject of a current application under S119A of the Highways Act 1980.

3.3.67 The Order will authorise Network Rail to temporarily possess land for access for construction in order to facilitate the diversion works to be undertaken.

3.4 Funding

3.4.1 The funding position in respect of the Project is detailed in the Funding Statement accompanying the Order application (**NR05**). The project cost as set out in the Funding Statement is £28,514,172 which Network Rail and the Government have provided public commitments to fund, subject to continued ongoing Value for Money (**VfM**) tests to ensure it remains the right thing to do for the UK taxpayer.

- 3.4.2 The TRU (and the Project) will span two railway investment periods: Control Period 6 (2019 to 2024) and Control Period 7 (2024 to 2029), where a total of over £8bn will be invested in the TRU.
- 3.4.3 In April 2021, a further £317m was committed to TRU by the Government to commence delivery and continue design development on TRU. Further funding announcements will occur as the programme progresses.
- 3.4.4 In addition to the funds already available to Network Rail in Control Period 6 to progress the Project, further funding will be made available for the delivery of the Project from the identified funding allocation for TRU in accordance with the Rail Network Enhancements Pipeline (**RNEP**) governance process. This provision will be sufficient to deliver the Project as part of TRU, provided it has the required consents and continues to represent VfM.
- 3.4.5 In March 2021, the DFT has confirmed the Government's commitment to TRU and the Project along with allocation of funding subject to ongoing consideration to ensure the Project delivers the best results for both rail users and taxpayers.
- 3.4.6 The Project is therefore considered to be fully funded. The authorised funds will meet the capital cost of implementing the Order inclusive of compensation and acquisition of blighted land as identified within Section 149 of The Town and Country Planning Act 1990 and undertaking associated work such as environmental mitigation as necessary.

4. CONSENTS AND AUTHORISATIONS SOUGHT

The draft Order will authorise both the Order Scheme and facilitate the wider Scheme (with powers for the acquisition and use of land, extinguishment and diversion of highway and footpaths (both temporary and stopping up) and works required to be undertaken within the highway.

4.1 EIA Screening

4.1.1 The entire E2 to E4 Project (including the Order Scheme elements), was originally screened for an Environmental Impact Assessment (EIA) and submitted to LCC for a screening decision in February 2022 under the Town and Country Planning (Environmental Impact Assessment) Regulations 2017. Network Rail received a screening opinion from LCC in April 2022, confirming that an EIA was not required for the for the E2 to E4. **(NR22)**.

4.1.2 An Environmental Impact Assessment (EIA) screening decision was also sought from the Secretary of State in April 2023 for the Order Scheme under the Transport and Works (Applications and Objections Procedure) (England and Wales) Rules 2006. The Screening Decision letter dated 17 May 2023 **(NR10)** confirmed that the Order Scheme was not EIA development, and on that basis, a formal Environmental Statement was not submitted with the Order application. Network Rail has nevertheless submitted an Environmental Report with the application to examine the environmental effects of the Order Scheme. Section 10 provides more detail on the Environmental Report **(NR16)**.

4.2 Deemed Planning Permission

4.2.1 Network Rail has applied to the SoS, for a direction under section 90(2A) of the Town and Country Planning Act 1990 that planning permission be deemed to be granted for the Scheduled Works (see **NR12**).

4.2.2 In making the request for deemed planning permission it is proposed that the planning permission would be deemed to be granted subject to the conditions which are set out in Appendix 1 of Deemed Planning Permission and Statement of Proposed Conditions **(NR12)**. The draft planning conditions were prepared by Network Rail in consultation with Leeds City Council.

4.2.3 Deemed planning permission is sought for the following elements of the Order Scheme:

- Penny Pocket Park

- Austhorpe Lane Bridge Replacement and Austhorpe Lane Gas Diversion works
- Crawshaw Woods Bridge Raising works
- New Barrowby Lane Bridge
- A656 Ridge Road Bridge replacement and Gas Main Diversion
- Micklefield Track Sectioning Cabin
- Peckfield Level Crossing Closure – associated highway works

4.3 Permitted Development

4.3.1 The temporary use of land for compounds, work site and construction access purposes will be authorised by the Order to facilitate the carrying out of the Scheme. Any temporary buildings plant or machinery required for the setting up of the compounds will be authorised under permitted development rights as summarised in **Table 1** below.

| Scheme Element | Permitted Development & Other |
|---|---|
| Kirkgate Viaduct compound and Kirgate Construction Land | Class A of Part 4 Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Marsh Lane Compound and Marsh Lane Construction Land | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Osmandthorpe Lane compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Wykebeck Avenue Compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Austhorpe Lane Northwest and Southeast compounds | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Manston Lane Compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |

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| Scheme Element | Permitted Development & Other |
|--|---|
| Crawshaw Woods Bridge North and South compounds | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Barrowby Lane Bridge compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Ridge Road compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Brady Farm compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |
| Phoenix Avenue compound | Class A of Part 4 of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015 |

Table 1 - Table of elements of Order Scheme authorised under Permitted Development

4.4 Listed Building Consent

- 4.4.1 The Scheme includes works to four Grade II listed bridges. Three of these include works are the subject of the application for deemed planning permission: Crawshaw Woods Bridge, Austhorpe Lane Bridge; and Ridge Road Bridge. The fourth, Brady Farm Bridge will be demolished within Network Rail operational land which does not require planning permission, (it is exempted demolition pursuant to Part 11 to Schedule 2 to GDPO) but a separate listed building consent is required.
- 4.4.2 Network Rail has applied for listed building consents for the works to the Grade II listed bridges as set out in the **Table 2** below.

| | Scheme Element | Reference No. |
|-----|-----------------------|---|
| I. | Crawshaw Woods Bridge | 23/04388/LI - Listed Building Consent for the careful dismantling of the superstructure of the bridge, the raising of the abutments by 1.4 metres and the replacement of the refurbished cast iron superstructure at the higher level |
| II. | Austhorpe Lane Bridge | 23/04387/LI - Listed Building Consent for the demolition of the existing Austhorpe Lane bridge (including the adjacent footbridge and gas pipeline) and a new bridge erected in its place |

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| | Scheme Element | Reference No. |
|------|-------------------|--|
| III. | Ridge Road Bridge | 23/04390/LI - Listed Building Consent for the demolition of the existing Ridge Road bridge and adjacent gas pipeline and a new bridge erected in its place |
| IV. | Brady Farm Bridge | 23/04389/LI - Listed Building Consent for the demolition of Brady Farm overbridge |

Table 2 - Elements of Order Scheme where listed building consent is sought

4.5 Scheme Planning Permissions and Prior Approvals

4.5.1 Other works associated with the Scheme are or will be subject to standalone planning permissions or separate prior approvals pursuant to Part 18 (Class A) of Schedule 2 of the Town & Country Planning (General Permitted Development) (England) Order 2015.

4.5.2 The Planning Permissions are:

| Scheme Element | Reference No. | Status |
|------------------------------|---|---|
| Neville Hill | 23/03522/FU - For the Construction of a new access road and shared-use footpath/cycleway off the existing Newmarket Approach to enable a new vehicular access to Neville Hill South Sidings | Determination period expired. Decision yet to be issued. |
| Garforth Moor Level Crossing | 22/03144/FU - Retrospective planning application for access track and turning head to service adjacent allotments | Determination period expired. Decision yet to be issued. |

4.5.3 Those parts of the Scheme comprising works which are authorised (or will be authorised by) prior approvals are:

| Scheme Element | Reference No. | Status |
|-------------------|---|---------------------------|
| Kirkgate Viaduct | 23/03890/DPD – Prior approval for the alterations to existing underbridge | Granted on 16 August 2023 |
| Osmandthorpe Lane | 23/00903/DPD – Prior approval for the proposed removal and replacement of the existing deck with a new bespoke steel orthotropic weathering steel deck type underbridge, with reinforced concrete | Granted on 2 May 2023 |

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| Scheme Element | Reference No. | Status |
|-----------------------|---|---|
| | edge beams to provide robust kerbs and collision protection to the lightweight deck | |
| Marsh Lane Viaduct | | Prior approval required under Part 18 Sch 2 – application not yet submitted |

5. LEVEL CROSSING RISKS

5.1 Network Rail Level Crossing Safety Strategy

- 5.1.1 Level Crossings represent the highest single risk on the railway network, being 30% of all risk areas (excluding suicide and trespass). Network Rail is committed to eliminating unacceptable safety risk and accidental fatalities at its level crossings. This is Government-promoted and endorsed policy.
- 5.1.2 Section 117 of the Railways Act 1993, incorporates sections 2 to 4 of the Health and Safety at Work etc. Act 1974. Network Rail's duty under the Health and Safety at Work etc. Act 1974 Section 3 (duty to non-employees to conduct and undertake in such a way as to ensure so far as is reasonably practicable they are not exposed to risks to their health or safety) includes a responsibility for users of level crossings. Network Rail's strategic objectives for meeting its long-term level crossing vision of no accidents at level crossings on Britain's main line network include maximising risk reduction. Closure of level crossings is the most effective way of removing risk from the network.
- 5.1.3 Network Rail has established a long-term strategy of reducing level crossing risk. Its long-term strategy targeting improved safety on Great Britain's railway is set out in its strategy document: "Enhancing Level Crossing Safety 2019-2029, A long term strategy targeting improved safety on Great Britain's railway" **(NR23)**. Designed around ALARP (as low as reasonably practical) the policy builds on Network Rail's core safety vision of 'Everyone home safe everyday', which sits behind its long-term safety objective of no accidents at level crossings, and its commitment to improving level crossing safety, doing all that is reasonably practicable to close crossings and improve safety at those which remain open (pg14).
- 5.1.4 A key element of Network Rail's policy on level crossings, to close level crossings where possible, is reflected in the Office of Rail and Road (ORR) "Principles for managing level crossing safety" published on 15th July 2021 **(NR24)** which presents principles set around a risk-based approach focusing on the interface of users, railway and highway. The ORR's "Strategy for regulation of health and safety risks – 4: level crossings" **(NR25)** goes on to stipulate that closure of level crossings should be the first consideration in a risk control strategy by the duty holder in line with the general principles of prevention under Management of Health and Safety at Work Regulations 1999.
- 5.1.5 It is widely acknowledged that the closure of level crossings is the most effective way to remove the risk. This is consistent with the General Principles

of Prevention, set out in Schedule 1 of the Management of Health and Safety at Work Regulations 1999, in particular the following: (a) avoiding risks; (b) combating the risks at source; (f) replacing the dangerous by the non-dangerous or the less dangerous.

5.1.6 Level crossings are an open interface between the railway and the level crossing user, giving rise to an increased potential for user behaviour to affect train operations. They have differing levels of protection and are broadly split into two groups:

- Active crossings: where the road vehicle or user is warned of the approach of a train through the closure of gates or barriers and/or by warning lights and/or alarms; and
- Passive crossings: where no warning of train approach is given other than by the train driver who may use the train horn. The onus is on the user to determine whether it is safe to cross the line. Instruction for proper use must be provided at each location, along with other appropriate signage.

5.1.7 Barrowby Lane Level Crossing (which carries a bridleway) is an "active crossing" and is protected by Miniature Stop Lights ("**MSL**"). The other four level crossings in the Order are passive level crossings.

5.1.8 Most of the risk at level crossings is to those crossing the railway. The biggest risk is users being struck by a train.

5.2 Assessing Risk: Quantitative and Qualitative Assessments

5.2.1 Network Rail Work Instruction NR/L3/XNG/308 'Risk assessing level crossings' (**NR26**) requires risk assessment of level crossings to be undertaken at least once every three years and three months and as soon as possible where there are network changes which will directly affect level crossing risk. Assessors use both quantitative and qualitative judgement to ensure a balanced approach is adopted in the risk management of level crossings. The 'All Level Crossing Risk Model' ("**ALCRM**"), is a quantitative decision support tool for identifying and mitigating risk at level crossings.

5.2.2 The calculated levels of risk are used as one part of Network Rail's overall risk management process, informing Network Rail of the relative risks of different level crossings and guiding business decisions on crossing upgrades and closures. ALCRM is a proven and robust working tool.

5.2.3 ALCRM risk modelling includes consequences associated with level crossing accidents. It can look specifically at the causes of accidents that could occur at different types of crossings. For example, in relation to footpath crossings,

where users are responsible for complying with signage and for making their own decision on when it is safe to cross, accidents may be caused by their failure to correctly stop, look and listen for trains.

- 5.2.4 To calculate the level of risk for each level crossing, ALCRM requires specific information about each asset. Information is gathered from existing records held by Network Rail on the crossing, using intelligent sources, stakeholder engagement and most importantly from a site inspection; during which the presence of a defined set of observable crossing features is recorded, including aspects such as crossing orientation, census, type of users, and the visibility of the crossing when approaching.
- 5.2.5 ALCRM determines level crossing risk using the same basic principles as for any risk assessment; namely hazard identification, frequency, and consequence assessment leading to a calculation of risk. These calculations are fundamental to the way in which crossing risk is calculated, as risk varies according to the particular characteristics of each crossing; the people using it, the number of lines it crosses, the number of trains and frequency, train speed and variance of speed.
- 5.2.6 ALCRM reports two measures of risk; collective risk and individual risk per traverse.
- 5.2.7 Collective risk is a measure of the total harm, or safety loss, and is expressed in terms of Fatalities and Weighted Injuries (FWI) per year, where FWI is a measure that accounts for fatalities and injuries. Collective risk is reported in a simplified numeric form ranked from '1 to 13' where '1' represents the highest risk and '13' represents nil risk. It applies cumulatively to all users, train staff and passengers.
- 5.2.8 The risk per traverse is the risk to the individual user making a single traverse of the crossing. ALCRM also calculates this risk as an FWI and like collective risk, it too is reported in a simplified form expressed as a letter, ranked from "A" to "M" ("A" representing the highest risk and "M" representing nil risk).
- 5.2.9 To widen the scope of quantitative assessment of risk, the qualitative professional judgement or "structured expert judgement" of the risk assessor, the Level Crossing Manager (LCM) is applied throughout the risk assessment process to ensure a balanced approach is adopted in the risk management of each level crossing. The qualitative professional judgement of the LCMs enable a broader and holistic safety perspective to form part of decision making and risk control selection.
- 5.2.10 Information to support structured judgement is derived through the collation of evidence during the site inspection, by applying local knowledge, using

smart intelligent sources such as the internet and mapping services, through external stakeholder engagement, and analysis of previous assessments and accident/incident data. LCMs will take cognisance of all factors including user demographic, accident history, and foreseeability of an event when evaluating available options to manage safety and/or the requirement to pursue closure.

- 5.2.11 In August 2014 Network Rail introduced the more comprehensive Narrative Risk Assessment ("**NRA**"). The NRA is the documented means of presenting the combined output of all routine level crossing risk assessments. It adopts both the quantitative calculated risk and the qualitative commentary and observations recorded by the LCM in the ALCRM, and feeds this into a structured judgement to be considered alongside other analysis.
- 5.2.12 The NRA is the document that summarises the risk at the level crossing. Details are gathered during site visits at the level crossing and are entered into the ALCRM which provides a quantifiable risk score/ranking and this allows comparison with other crossings.

5.3 The Level Crossings in the Order

- 5.3.1 Barrowby Lane Level Crossing facilitates a public bridleway over the railway. On each side of the railway there is an MSL, telephone and self-closing bridleway gate. Horse riders and cyclists are instructed to dismount by signage. The MSL signage contains the crossing instructions for pedestrian users (and horse riders).



Figure 7 - An image of the current arrangements at Barrow by Lane Level Crossing

- 5.3.2 Barrowby Lane Level Crossing currently has an ALCRM score of C3.
- 5.3.3 The train service over Barrowby Lane Level Crossing consists of passenger and freight trains. There are approximately 322 trains per day and trains are timetabled to run for 24 hours per day. The crossing is used by pedestrians, horse riders and cyclists.
- 5.3.4 A Level Crossing Risk Assessment produced by Sotera Risk Solutions in 2018 states that the crossing, in its existing arrangement, has had 25 incidents over 10 years of analysed Safety Management Intelligence System (SMIS) data. These consisted of 4 instances of misuse (including one where the train driver had to apply the emergency brake), 1 instance of a horse being struck and killed by a train, 1 instance of a dog on the crossing and 19 instances of telephone failure. A Narrative Risk Assessment (NRA) for this level crossing produced by Network Rail in June 2019. The NRA states that a level crossing safety event has not been known to occur at Barrowby Lane level crossing in the previous twelve months and the crossing has not had a safety event/user misuse reported within the last 36 months. However, as part of the consultation process there was anecdotal evidence that not all

equestrian users were using the telephones provided when using the crossing.

- 5.3.5 Barrowby Foot Level Crossing facilitates a public footpath over the railway. On each side of the railway there is an existing steel pedestrian gate at the bottom of the railway embankment. Within the railway boundary, there are steps leading to each area – ‘Stop Look Listen’ signage is located at the decision point on each side of the railway.



Figure 8 - An image of the current arrangements at Barrowby Foot Level Crossing

- 5.3.6 Barrowby Foot Level Crossing currently has an ALCRM score of C10.
- 5.3.7 The train service over Barrowby Foot Level Crossing consists of passenger and freight trains. There are approximately 322 trains per day and trains are timetabled to run for 24 hours per day. The crossing is used by pedestrians only, although there is very little usage.
- 5.3.8 A Level Crossing Risk Assessment produced by Sotera Risk Solutions in 2018 states that there was no SMIS data for this crossing but a near miss was recorded in Network Rail data for 22nd May 2016 when a male aged approximately 20 years crossed in front of a train. A female then started to

cross, at which point the train driver applied his brake. The girl then stepped back. An NRA for this level crossing produced by Network Rail in May 2019. The NRA states that a level crossing safety event has not been known to occur at Barrowby Foot Level Crossing in the previous twelve months and there is no recorded history of known or reported incidents at this crossing recorded in Network Rail's incident files.

- 5.3.9 Garforth Moor Level Crossing facilitated a public footpath over the railway. The level crossing was temporarily closed in 2018 by Network Rail on safety grounds. Pedestrian traffic from the south of the railway was diverted via Barwick Road to a new car park on the north of the allotments, accessed via a new track.



Figure 9 - An image of the temporarily closed Garforth Moor Level Crossing (which has been removed)

- 5.3.10 Peckfield Level Crossing facilitates a public bridleway over the railway. On each side of the railway there is a telephone, horse dismount block, manually operated steel gate and crossing instruction signage for both pedestrian users and persons in charge of animals.



Figure 10 - An image of the current arrangements at Peckfield Level Crossing

- 5.3.11 Peckfield Level Crossing currently has an ALCRM score of B2.
- 5.3.12 The train service over Peckfield Level Crossing consists of passenger and freight trains. There are approximately 322 trains per day and trains are timetabled to run for 24 hours per day. The crossing is used by pedestrians and cyclists. Note that there has been extensive user census data gathered at this level crossing and there is no evidence of equestrian use.
- 5.3.13 A Level Crossing Risk Assessment produced by Sotera Risk Solutions in 2018 states that the crossing, in its existing arrangement, has had 11 incidents over 10 years of analysed SMIS data. These consisted of 2 instances of misuse, 1 instance of something striking the train and 8 instances of telephone failure. 4 near misses were recorded (including 3 from Network Rail data not included above) including one where the train came within 2 feet of striking a user, one 15 yards from striking a user and one where vandals almost derailed the train by leaving ballast on the rail. An NRA for this level

crossing produced by Network Rail in March 2020. The NRA states that a review of the incident data files from April 2014 to the date of the NRA (2020) shows that there have been 6 reported cases of a “near miss” with pedestrian users at the crossing recorded in Network Rail’s files. One incident on 19 September 2019 involved a dog walker crossing in front of the approaching train, the train driver sounded the train horn and applied the emergency brake. During the consultation process there was anecdotal evidence that there were instances of refuse collectors using the crossing with bins from the residences to the north of the railway.

- 5.3.14 Highroyds Wood Level Crossing facilitated a public footpath over the railway. The level crossing was temporarily closed in 2021 by Network Rail under Section 3 of the Health and Safety at Work Act 1974 due to trains stopping on the crossing and a serious rise in incidents of misuse resulting in multiple near misses being reported by train drivers.



Figure 11 - An image of the temporarily closed Highroyds Wood Level Crossing

6. THE CASE FOR THE SCHEME

6.1 Network and operational constraints – the need for the Scheme

- 6.1.1 The Transpennine Route is a key strategic rail route for passengers and freight across the North of England, with the core route linking Manchester and York, via Huddersfield and Leeds.

- 6.1.2 Demand for passenger and freight services is high and is expected to rise significantly in the future and the Transpennine Route is one of the busiest lengths of rail at peak times on the national rail network.
- 6.1.3 The Transpennine Route currently handles a mix of fast express, local stopping services and freight, but has not seen significant infrastructure investment in enhancements to increase capacity for many years. Therefore, the Transpennine Route network is increasingly becoming crowded and congested, journeys are slow and unreliable and due to the current infrastructure provision being relatively dated there is limited existing capacity to accommodate predicted growth.
- 6.1.4 The Leeds to Micklefield section of TRU currently experiences a number of constraints relating to the capacity, reliability and modernisation that are common to the whole Transpennine Route. The key constraints can be summarised as follows.
- Track Capacity: The Scheme route is almost entirely two tracks, with one track in each direction from Leeds to Micklefield. This can adversely affect train performance and resilience whereby slower 'local' services can be caught by fast 'express' services unable to pass, or train malfunctions can lead to delays to all) services and the resultant capacity issues.
 - Line speed: Currently limited to 70-80mph and needs to be increased up to 100 mph in some sections to deliver required benefits.
 - Existing bridge structures are too low to allow overhead line electrification equipment (OLE) to be installed, limiting train rolling stock to diesel power.
 - Existing railway infrastructure requires renewal in order cope with planned greater number of trains at increased line speed.
 - Private & Public Rights of Way exist across the existing level crossings, providing at-grade access across the railway. As well as the safety implications, this also impacts on the efficiency and reliability of the railway where incidents at the grade crossings take longer for the rail network to recover from, affecting JTI, capacity and the PPM.
- 6.1.5 As currently operated, the Scheme route is a non-electrified section of the railway network. Switching from diesel trains to electric rolling stock has benefits for the environment and will assist Network Rail in achieving its decarbonisation objectives. Electrification also benefits train performance, with faster acceleration and more efficient braking being made possible, assisting with the JTI, capacity, and Public Performance Measures ("**PPM**") objectives.

6.2 Aims of TRU

- 6.2.1 TRU is a series of projects between Manchester, Huddersfield, Leeds and York with the objective of improving journey times and capacity, improving reliability and resilience and delivering environmental benefits from modal shift of transportation from road to rail and reduction of diesel-powered trains.
- 6.2.2 TRU seeks to address the current constraints on the Transpennine Route and support aspirations to deliver economic growth and “levelling up” opportunities (see section 6) across the North of England. Without the implementation of the Scheme, these wider TRU aims will not be met in full.
- 6.2.3 Due to the scale and complexity of the works required to upgrade the existing rail infrastructure, TRU is designed as a phased programme of interventions which will be delivered as separate projects. In combination, these TRU Projects will deliver the overarching TRU benefits detailed in section 6.3.
- 6.2.4 As set out above, the specific aims of TRU are listed below.
- An improved journey time for Leeds – Manchester Victoria of 43-44mins.
 - An improved journey time for York to Manchester Victoria of 67-69mins.
 - Capability to operate 8 ‘express services’ an hour on the Transpennine Route.
 - Capability to operate 6 ‘local services’ an hour on the Transpennine Route.
 - Performance of the Transpennine Route to be 92.5% of higher each period.
 - Freight paths/rights to be retained as existing.
 - A contribution to Network Rail’s Decarbonisation Strategy and climate policy.

6.3 Scheme benefits

- 6.3.1 The component elements of the Scheme are described in section 3 of this Statement of Case.
- 6.3.2 The Scheme is a major part of the wider TRU programme and delivery of the Scheme will help achieve the TRU aims, which in turn will unlock the wide-ranging benefits of TRU. Of the TRU Programme benefits, the Order Scheme directly contributes to all but one of the six objectives:

- An improved journey time for York to Manchester Victoria of 67-69mins. The Scheme delivers on this journey time improvement aim through increased fast line running speeds of up to 90mph and electrification throughout the extent of the Scheme Route;
- Capability to operate 8 'express services' an hour on the route. The Scheme delivers on this capacity improvement aim by installation of modern signalling, electrification of the lines, and line speed increases;
- Capability to operate 6 'local services' an hour on the route. The Scheme delivers on this capacity improvement aim by installation of modern signalling, electrification of the lines, and line speed increases;
- Performance of the Transpennine Route to be 92.5% (Public Performance Measure) or higher each period. The Scheme delivers on this reliability aim by removal of the level crossings and providing upgraded modern railway equipment throughout the Scheme Route;
- Freight paths/rights to be retained as existing. The Scheme delivers on this aim by delivering improved capacity and resilience on the railway; and
- A contribution to Network Rail's Decarbonisation Strategy and climate policy. The Scheme contributes to Network Rail's strategy and policy by delivering the electrification of the railway throughout the Scheme Route.

6.3.3 The Scheme benefits are discussed further below.

Increased Linespeed

6.3.4 One of the key infrastructure benefits of the works proposed, including the level crossing closures, will be to allow for the introduction of modernised signalling across the whole of the Transpennine route, and for an increase in line speed up to to 100mph. This work will then further allow for an increase in train capacity and frequency, critical on this section of route as it is only two track, meaning more, and faster journeys for passengers.

6.3.5 The TRU enhancements that the Scheme will help to deliver include track realignments which will allow trains to run faster and journey times to be reduced, delivering the associated economic benefits that will flow from those track enhancements.

Improved efficiency and reliability of the railway, and reduction of delays to trains and other users

- 6.3.6 The closure of the level crossings, installation of OLE and electrification, and general rail infrastructure improvements, including track realignment, will help to reduce conflicts between slow (including freight) and fast trains and increase railway capacity.
- 6.3.7 The Installation of modern infrastructure, including new signalling, will result in fewer equipment and signalling failures which cause service disruption. Overall, improved efficiency and reliability of the railway will provide a service that meets the transportation needs of rail users, with consequent benefits to the wider economy of the North of England.
- 6.3.8 The closure of the level crossings will reduce the number of incidents on the railway which cause delays. Incidents at level crossings have the potential for severe impacts on train performance, where at-grade incidents occur, significant train performance impacts are common. Incidents at level crossings are typically more complex to resolve and recover from than traditional railway infrastructure issues e.g. wires failing or signals broken.

Safety

- 6.3.9 The delivery of safe passage across the railway for Public Right of Way users will remove the existing risks associated with conflicts between trains and public and avoids any increases in these risks to unacceptable levels due to faster and more frequent trains and the presence of O(H)LE. As part of its statutory duty to minimise risk to railway users, workforce and the public, Network Rail has adopted a long-term strategy to improve level crossing safety (Enhancing Level Crossing Safety, Network Rail 2019). This strategy notes that level crossings are one of the main public safety risks on the railway. The only way to remove the risk is by removing the at grade interface.

Reduced operating and maintenance costs

- 6.3.10 New track and electrification equipment delivered by TRU will reduce maintenance costs compared to the existing railway, with associated benefits for taxpayers, the travelling public, freight operators and freight end-users. The closure of the Level crossings will also reduce the number of crossings on the network which need to be inspected, maintained and operated, with corresponding reduction in the associated costs of the same.

Environmental benefits

- 6.3.11 The electrification of the railway allows diesel-powered trains to be replaced by electric trains. In addition to faster acceleration and more efficient breaking of electric trains, this will deliver climate change and local air quality benefits.

Modern Signalling

- 6.3.12 As set out earlier in section 2, the Order scheme forms part of the wider programme of works, including further works on the Leeds to York section of the NTPR. It is currently proposed that all necessary civil and signalling works for the modern signalling will be completed, installed, and available for use no later than December 2024. For the Order scheme, this means all works associated to the closure of level crossings need to be completed by no later than December 2024, in order for testing to take place and be signed off by the ORR ready for the December 2025 timetable change. Other works in the Order are required for the full electrification of the route, and need to be complete by the end of 2027, for the energising of the route in 2028.

6.4 Strategic alternatives

- 6.4.1 The remit of TRU is to address the performance issues related to the existing service, increase the capacity on the route, and to decrease journey times. As a result, there are very few high-level strategic alternatives that would deliver these benefits without investing in the infrastructure.
- 6.4.2 The NTPR is a key West-East artery, used by over 50million passengers each year. The only feasible modal shift to reduce the pressure on the route would be to increase capacity on the surrounding strategic highway network. Projects have been ongoing on the only other arterial West-East route in the vicinity of the NTPR, the M62 motorway, since 2012 in order to address its own capacity and performance issues, with further work scheduled up to 2024. Other Transpennine road schemes, away from the M62, would not be of sufficient scale to provide a feasible alternative to rail travel.
- 6.4.3 Other rail schemes that have been reviewed include the South Transpennine Route (**STPR**) that links Manchester to Sheffield, but does not link into the centres such as Leeds, York, Hull or stations north to Scotland, rather to the Midland Mainline and southern destinations. As such this STPR route was discounted, and is undertaking significant capacity and improvement works on its own to address issues.
- 6.4.4 Works are happening on other routes close to the NTPR, such as the Calder Valley line, but these are works to upgrade them sufficient that they can be used as diversionary routes whilst the TRU Programme is constructed, and

again, work on them in isolation would not deliver the benefits the TRU programme is remitted to deliver.

- 6.4.5 During the development phases of TRU, strategic alternatives were considered that could deliver the TRU aims (see section 6.2). The main strategic alternatives considered were partial electrification and full electrification of the TRU route. It was concluded that partial electrification would not meet the aims of TRU and a full electrification scheme was chosen as the preferred scheme. This decision is confirmed in government policy set out in the Integrated Rail Plan (IRP) for the North and Midlands (November 2021).
- 6.4.6 Once a full electrification scheme was selected, alternative options to manage operational railway implications were identified and assessed (e.g. underbridge deck strengthening, signalling and electrification plant) structural implications (e.g. overbridges too low to accommodate overhead line equipment beneath them) and level crossing closures. An overview of the alternative options considered for each element of the TWAO Scheme is set out in section 9.

6.5 Future Rail Schemes

- 6.5.1 The Scheme and the TRU programme of works are the first major rail infrastructure projects in the North of England in the last twenty years (since the West Coast Mainline upgrade). Both are being delivered by Network Rail, within the funding and governance mechanisms already set out, with necessary funding approved to date. There are a number of other rail schemes that may potentially be delivered through NPR but these are yet to be authorised and are some time off being realised.

6.6 Non-infrastructure alternatives

- 6.6.1 The rail industry has been responding to recent problems referred to above, through a range of initiatives, especially to reduce crowding and improve passenger experience. On the NTPR specifically, TPE has invested heavily in new rolling stock: the “Nova” trains. As well as providing an enhanced passenger environment and the ability for some “bi-mode” stock to use electric traction where available (beyond Manchester and York), the key benefit of this investment is a significant increase in seating capacity, with the typical train lengthened from 3 to 5 cars, and those cars having a higher seating density.
- 6.6.2 The introduction of new rolling stock and an improved timetable in December 2019 have increased the nominal seats per hour across the Pennines on fast trains from 900 to circa 1600, an improvement of 80%. These improvements

are a positive step towards relieving recent overcrowding on the route, at least for the faster trains. However, challenges remain around performance, reliability, and capacity for both local and express services, which can only be resolved through infrastructure investment.

7. POLICY FRAMEWORK

7.1 Introduction

- 7.1.1 In this section, a summary of the main national, regional and local transportation and planning policy is set out, followed by an assessment of compliance of the Order Scheme with these policies.

7.2 National Transport and Railway Policy

- 7.2.1 There is a suite of transport and railway policy and guidance documentation of relevance to the Scheme.

The Growth Plan 2022 – Chancellor of the Exchequer September 2022

- 7.2.2 The Growth Plan outlines various measures proposed to accelerate the delivery of priority major infrastructure, which it classifies as a vital means of driving the UK's economic growth and delivering Net Zero. The Plan also lists infrastructure projects that will be prioritised for acceleration, including the TRU as a specifically named priority major infrastructure project.

Levelling Up the United Kingdom – White Paper 2022

- 7.2.3 The White Paper sets out the next stages in the Government's programme to level up the UK, including:
- By 2030, local public transport connectivity across the country will be significantly closer to the standards of London, with improved services (page xvii).
 - With reference to the Integrated Rail Plan (2021), the improvements could see improved services in terms of destinations served, electrified trains, higher frequencies, more seats and/or faster services.

Integrated Rail Plan for the North and Midlands – 2021

- 7.2.4 The Integrated Rail Plan (IRP) (**NR27**) for the North and Midlands was published in November 2021 and sets out core railway projects that the Government intends to invest in over the next 30 years.
- 7.2.5 Recognising that the TRU will now be significantly expanded to enable Northern Powerhouse Rail (NPR), it will be managed as the first phase of NPR. The IRP states that the whole TRU route will be electrified, installing full digital signalling, and add longer sections of three and four-tracking to allow fast trains to overtake stopping services, leading to an initial increase in

through passenger services of 20% compared with the pre-COVID-19 situation (page 14).

- 7.2.6 In the core pipeline, the IRP states that between Liverpool and York, to build NPR in line with the 2019 Option 1, which was developed by Transport for the North (TfN) and resulted in electrification of Leeds – York (the TRU Project) with some sections of four-tracking (page 100).

Rail Network Enhancement Pipeline (RNEP) – Department for Transport (2019) (NR28)

- 7.2.7 In March 2018, the Government published its new approach to rail enhancements. This document set out the rationale behind creating a rolling programme of investment which focusses on delivering real benefits for passengers, freight users and the economy. The Pipeline was updated in the Autumn of 2019.
- 7.2.8 The Rail Network Enhancement Pipeline (RNEP) is the approach applied to all rail enhancements within England and Wales which are in receipt of funding from central government (i.e. from the Department for Transport). This was set out in the Statement of Funds Available (SoFA), which was published in October 2017.
- 7.2.9 The Transpennine Route Upgrade was listed as a Scheme in Stage 3 - Decision to Design with the stated planned railway output of delivering improved performance, capacity and journey time between Manchester and York.
- 7.2.10 Projects listed in this table have progressed through to the final development stage and will be either working towards or have completed an Outline Business Case. In April 2020, the Transpennine Route Upgrade was awarded Outline Business Case (£3bn) and provided funding to take TRU programme projects through design and delivery.
- 7.2.11 The Scheme is consistent with, and aligned with the outcomes identified in the RNEP, and at each governance stage this is confirmed.

National Infrastructure Delivery Plan (2016)

- 7.2.12 National Infrastructure Delivery Plan ("NIDP") (NR29) published in March 2016 sets out how the Government intends to support the delivery of key infrastructure projects and programmes. It brings together the Government's plans for economic infrastructure over a 5-year period from 2016 – 2021.

- 7.2.13 The NIDP sets out context to the national strategy for delivering infrastructure, including a vision “provide world class train services that drive economic growth and exceed passenger expectations”. It states that the Government is committed to carrying out the largest rail modernisation programme since Victorian Times (paragraph 4.6).
- 7.2.14 The NIDP sets out infrastructure priorities 2020/21 which includes the Network Rail enhancement programme. This programme is designed to provide necessary extra capacity, more services and better journeys. The TRU is highlighted as being one of the key projects and programmes (page 36), with the NIDP noting that continued Transpennine electrification works form part of the Network Rail enhancement programme.

Williams-Shapps Plan for Rail – HM Government (2021) (NR30)

- 7.2.15 The Plan sets out how the Government intends to deliver wider changes to the railway network. There are seven key areas of the plan. These include bringing the railways back together, delivering more punctual and reliable services (under the umbrella of a new public body, Great British Railways; making the railways easier to use; re-building public transport use after the pandemic; maintaining a safe and secure railway for all; keeping the best elements of the private sector that have helped drive growth; making the railways more efficient; and growing, not shrinking the network.
- 7.2.16 The Scheme directly and indirectly supports many of the ambitions of the Plan in terms of how it will grow the rail network, supporting economic growth across the North following the COVID-19 pandemic.
- 7.2.17 The NIDP sets out a vision to “provide world class train services that drive economic growth and exceed passenger expectations”. Within the strategy to achieve this, the NIDP sets out the Government rail infrastructure priorities to 2020/21. This includes the Network Rail enhancement programme, which is designed to provide necessary extra capacity, more services and better journeys.

National Policy Statement for National Networks 2015 (NR43)

- 7.2.18 The National Policy Statement for National Networks (‘NPS-NN’) sets out the need for, and Government’s policies to deliver, development of nationally significant infrastructure projects (NSIPs) on the national road and rail networks in England. Whilst the Scheme does not meet the NSIP threshold set out in the Planning Act 2008, as amended by the Highway and Railway (Nationally) Significant Infrastructure Project) Order 2013, the NPS-NN states:

“In England, this NPS may also be a material consideration in decision making on applications that fall under the Town and Country Planning Act 1990 or any successor legislation. Whether, and to what extent, this NPS is a material consideration, will be judged on a case by case basis.”

7.2.19 The NPS-NN is therefore material to a decision on the application for deemed planning permission for the Order Scheme.

7.2.20 Section 2 of the NPS-NN states that:

“The Government will deliver national networks that meet the country’s long-term needs; supporting a prosperous and competitive economy and improving overall quality of life, as part of a wider transport system. This means:

- Networks with the capacity and connectivity and resilience to support national and local economic activity and facilitate growth and create jobs.*
- Networks which support and improve journey quality, reliability, and safety.*
- Networks which support the delivery of environmental goals and the move to a low carbon economy.*
- Networks which join up our communities and link effectively to each other.”*

7.2.21 Paragraph 2.2 states that "there is a critical need to improve the national networks to address road congestion and crowding on the railways to provide safe, expeditious and resilient networks that better support social and economic activity; and to provide a transport network that is capable of stimulating and supporting economic growth".

7.2.22 Paragraph 2.4 goes on to set out the pressures the national networks are under, including a projected increase of 40% of journeys undertaken by rail and rail freight having the capacity to double by 2030. It is important to note that although demand for journeys fell away during the 2021/22 period due to the impacts of the COVID-19 epidemic, expectations are for long term increases in demand to continue to grow.

7.2.23 Paragraph 2.6 of the NPS-NN states that improved transport links help to rebalance the economy and at paragraph 2.10 Government concludes at a strategic level that there is a compelling need for the development of national networks.

- 7.2.24 The need for development of the national rail network is set out from paragraphs 2.28 – these identify the importance of the rail network as a vital part of the national transport infrastructure and for the growing demand for rail travel.
- 7.2.25 Paragraph 2.29 sets out the Government’s vision for the transport system, where railways deliver reliable and safe routes to work and connect communities with services and provide for freight transport.
- 7.2.26 Paragraph 2.37 highlights the need to improve the capacity, capability, reliability and resilience of the rail network, meaning reduced overcrowding, improved journey times, robust operational performance and a modal shift from road to rail.
- 7.2.27 Paragraph 2.40 promotes the environmental benefits of rail improvements stating that modal transport shift can help reduce transport’s carbon emissions.
- 7.2.28 The Scheme overall is entirely consistent with the aims of the NPS-NN. It will enhance connectivity, quality, safety and reliability on the Transpennine route and in particular between Leeds and York. It will foster economic growth and opportunity and contribute towards the target of Net Zero.
- 7.2.29 In the context of Net Zero in particular, the improvements delivered by the TRU as a whole, are expected to deliver a reduction in annual emissions of approximately 1,500 tonnes of carbon dioxide equivalents, due to electrification of the line and a shift to bi-modal trains. The TRU programme, of which the Scheme is a part of, is a key component of the delivery of the Net Zero element of the 2022 Growth Plan and sustainable transportation advocated in the 2021 IRP.
- 7.2.30 The Scheme also aligns with the NPPF key objective of moving to a low carbon economy, and the NPS-NN emphasis on how rail improvements can help de-carbonise transportation, due to the electrification of the railway facilitating a move away from diesel powered trains, and the policy encouragement for solutions which support reductions in greenhouse gas emissions and congestion.

National regional transport policy - summary

- 7.2.31 The whole thrust of the transport policies outlined here is to seek improvements to the region’s transport systems and the rail network, recognising the benefits of rail in the movement of large numbers of people and container/bulk goods over large distances or into congested city centres. The Scheme is consistent with national transport policy in that it provides an

improvement in capacity and line speed for the Leeds to York line, as well as helping to reduce at grade conflicts with rail traffic. This improves the reliability of the network and this in turn contributes to encouraging a more sustainable mode of transport as well as fostering economic growth. The need for the Scheme is therefore driven by a body of government policy, both at national and regional level, culminating in the Government's IRP for the North and Midlands (November 2021) where full support was given to the development of the Transpennine Route as the first step in delivering the wider package of improvements known as Northern Powerhouse Rail.

7.3 Regional and Local Rail Policy

Strategic Transport Plan, Transport for the North 2019

- 7.3.1 TfN comprises the North's local transport authorities and business leaders and Network Rail, National Highways, and HS2 Ltd. TfN has created a Northern transport strategy, setting out priorities for significant investment in the north's inter-city road and rail network. The TfN Strategic Transport Plan (**NR31**) notes that the strategic outputs of TRU are fully delivered so the TRU programme can deliver maximum benefits to the Northern Powerhouse Rail Programme which will deliver long term benefits across the North (section 98-103).

West Yorkshire Transport Strategy 2040

- 7.3.2 Published by the West Yorkshire Combined Authority, which includes Leeds City Council and covering the Leeds to York route and the whole Transpennine Route, the West Yorkshire Transport Strategy (**NR32**) lists TRU as one of the major opportunities that must be seized, noting that funding of TRU is crucial for improvements to the transport network connecting the major towns and cities of the North in the coming years. It also notes the urgency of TRU, stating that it will release capacity needed now, in advance of future NPR projects.

Leeds City Region Strategic Economic Plan 2016-2032

- 7.3.3 Published in 2016 by the West Yorkshire Combined Authority and the Leeds City Region Enterprise Partnership and covering West Yorkshire and York and Selby Districts, the Plan (**NR33**) identifies investment in transport infrastructure as one of four priorities, noting that investment will include new stations on the Transpennine Route.

North Yorkshire County Council Local Transport Plan 2016 – 2045

- 7.3.4 The County Council's Local Transport Plan (adopted April 2016) (**NR34**) considers a 30-year time period, stating that improving road and rail connections into these City Regions remains an important element of the County Council's strategy to encourage economic growth in North Yorkshire.
- 7.3.5 It is pertinent to note that all of the regional transport initiatives (TfN, Northern Powerhouse Rail and the North and West Yorkshire Transport Plans) make reference to, and will build upon, the Transpennine upgrade and thus the Scheme, and the benefits it is expected to realise.
- 7.3.6 The Scheme therefore clearly aligns with the objectives set out in relation to Transport for the North and Northern Powerhouse Rail by providing significant investment and improvement to the North's railway infrastructure and increasing the capacity, speed and reliance of the Network.

7.4 Local Planning Policy

- 7.4.1 The majority of the Order area lies within the administrative area of Leeds, though one element (Highroyds Wood Crossing) is within the administrative area of North Yorkshire (formerly Selby DC).
- 7.4.2 For Leeds, the Order Scheme is in accordance with relevant planning policy set out in the Core Strategy (with Core Strategy Selective Review Policies) 2019 (**NR36**), Saved Policies of the Unitary Development Plan (**NR37**), the Site Allocations Plan (**NR38**), the Natural Resources & Waste Local Plan (**NR39**), the and the Garforth Neighbourhood Plan (**NR40**). The specific policies are given below, with a short commentary on how the Scheme meets the policy's objective. Fuller explanations are given in section 7 of the Planning Statement (**NR13**).
- 7.4.3 Elements of the Scheme (Marsh Lane Compound and Marsh Lane Construction Land; Neville Hill Access Land; Osmondthorpe Lane Compound) are within the boundary of the Aire Valley Area Action Plan (AVAAP), adopted 2017, but these elements do not form part of the TWAOP application for deemed planning permission. It should also be noted that the AAVP itself deals with purely local issues pertinent to the area and does not include policies to shape and control the strategic rail corridors though the Plan Area (save for HS2), Leeds Core Strategy (as amended by the Core Strategy Selective Review 2019) (adopted 2019).
- 7.4.4 The Core Strategy is the main strategic document within the Local Plan for Leeds and sets out the strategic policy framework for the district to 2028 and a housing requirement to 2033. The key policy is Spatial Policy 11 - Transport Infrastructure Investment Priorities, as it supports the development of

strategic transport infrastructure and specifically encourages those proposals which will bring forward such infrastructure. In the reasoned justification specific mention is made of the planned electrification of the Transpennine route. The Order Scheme would deliver on this aspiration through provision of an improved and more reliable rail network.

7.4.5 The other relevant policies of the Core Strategy are mentioned below.

- **General Policy:** This commits to taking a sustainable development policy position that reflects the aims of the NPPF in this regard. The Scheme in itself is delivering improvements to sustainable transport, and thus supports the policy.
- **Spatial Policy 1: Location of Development.** All development should respect and enhance local character and identity. Proposals have been developed with this in mind, particularly the design of the new bridges.
- **Spatial Policy 3: Role of Leeds City Centre.** This policy includes provision to improve public transport links between the City Centre and the rest of the District. Again, the Scheme will support this policy through improving links to the east including the suburban stations of Cross Gates, Garforth, East Garforth and Micklefield as well as wider afield towards York.
- **Spatial Policy 8: Economic Development Priorities.** This seeks to support a competitive local economy by, amongst other things, improving accessibility to employment opportunities by public transport. The Scheme would help to achieve this target by providing an improved rail service into Leeds centre, to the east of Leeds and beyond.
- **Policy P10: Design.** This seeks to ensure new development is based on a thorough contextual analysis and provide good design that is appropriate to its location, scale and function. Supplemental to this the Accessible Leeds SPD (**NR42**) outlines six 'Implementation Points' which applicants for planning permission should take into account. Implementation Points (IP) 2, 3, 4 and 5 are relevant.
- *IP 2, 3 and 4 – Seek to provide an all-inclusive design based on access for all principles; IP5 seeks an access statement which will identify the approach taken to achieve such inclusive access.*

7.4.6 How the Order Scheme complies with these policies is covered in more detail in the Design and Access Statement (**NR15**) (DAS). The DAS includes a design appraisal, showing that the Order Scheme complies with the relevant national and local planning policies. It provides safe, well-functioning railway infrastructure, taking account of the important issues of access for

maintenance and inspection, and materials durability. The DAS concludes that the Scheme is sympathetic to the local landscape through good design whilst making efficient use of land, minimising impacts on agricultural land and flood zones and adopting sustainable construction principles.

- 7.4.7 For example, the replacement Austhorpe Lane and Ridge Road Bridges will show a new bespoke design incorporating original stone material a steel arch feature that conserves the original basket arch design principal. At Crawshaw Woods Bridge the original cast iron structure will be refurbished and retained, alongside the reuse of original stone material in the raised bridge abutments.
- 7.4.8 As such the design of the Order Scheme's components are considered to be of a scale and appearance which complies with Policy P10.
- 7.4.9 Green Space (Policy G6: Protection and Redevelopment of Existing Green Space) will be protected from development unless one of the following criteria is met:
- There is an adequate supply of accessible green space / open space within the analysis area and the development site offers no potential for use as an alternative deficient open space type;
 - The green space/ open space is replaced by an area of at least equal size, accessibility and quality in the same locality; or
 - Proposals demonstrate a clear relationship to improvements of existing green space quality in the same locality.
- 7.4.10 The Order Scheme affects green space development plan allocations in several locations.
- G1913 – Austhorpe Lane Southeast Compound
 - G84 – Kirkgate to Marsh Lane Land (Penny Pocket Park)
 - G523(1) - Neville Hill Access Land
 - G523(2) – Osmondthorpe Lane Compound
 - G1213 - Peckfield Level Crossing Closure Scheme
 - G114 – Highroyds Wood Level Crossing Closure Scheme
- 7.4.11 Green space allocation references G523(1), G523(2) and G114 relate to TWAO elements that do not require deemed planning permission and therefore Policy G6 is not relevant to these elements.

- 7.4.12 At green space allocation reference G1213, the proposals for a new formal Public Right of Way footpath or bridleway deliver wider planning benefits and improve access to and quality of the green space and are therefore in accordance with policy G6. Further, LCC has commented as part of its holding objection letter (18/08/23) (OBJ 07) "footpaths are compatible with designation as green space".
- 7.4.13 A specific objection has been made by the Council in respect of the loss of green space land (green space allocation reference G84) at Penny Pocket park (Kirkgate to Marsh Lane Land). However, the area of green space land that will be acquired to accommodate the electrification and signalling equipment required is very small, limited to an area less than 209 square metres. It is also at the top of a steep railway embankment, that is in part covered by gravestones and not generally used for recreation purposes. On this basis it is considered that the proposed use of this land does not have a negative impact on usable greenspace and, as it is not lost, no alternative space is required elsewhere and therefore the proposals do not conflict with Policy N1. LCC has commented as part of its holding objection letter (18/08/23) appendix 1 Index row 239, that it agrees with the conclusion in the Planning Statement that the proposals at Penny Pocket Park will not have an unduly negative impact on designated green space which supports the Network Rail position that these works do not conflict with Policy G6.
- 7.4.14 As part of its holding objection letter (18/08/23), a further objection has been made by the LCC regarding impacts on land designated as Austhorpe Lane Neighbourhood Park (green space allocation reference G1913) which is affected by the Austhorpe Lane Southeast Compound. Network Rail has provided details of the timing and duration of the temporary use of land southeast of Austhorpe Lane bridge in the Consents and Works Register. Network Rail has also provided a consideration of impacts of the temporary and permanent works associated with this element of the Order Scheme in the Environmental Report (**NR16**), and has provided a 'Outline Landscape and Ecological Mitigation Proposals' included in Figure 8.5.2 in Volume 2 Environmental Report (**NR16**) which illustrates the proposed restoration scheme for the site, taking account of the Austhorpe Lane Neighbourhood Park proposals and an extant land restoration scheme associated with the Thorpe Park development planning permission, and taking account of detailed conversations held with the LCC landscape and nature conservation team over several meetings, where the need for a compound in this location to deliver the works, permanent access requirements to a gas main tunnel shaft and detailed proposals to minimise the impact of the works were explained and discussed.

- Policy P11: Conservation. The historic environment and its setting will be conserved and enhanced. This is demonstrated clearly through the retention and enhancement of the Crawshaw Woods bridge. Whilst it is accepted that three other listed structures will be lost, it is necessary to deliver the clearances required for electrification. The design of the replacement structures shows a commitment to compensate for the necessary loss.
- Policy P12: Landscape. The character, quality and biodiversity of the area including the historical and cultural significance will be conserved and enhanced.
- Policy G1: Enhancing and Extending Green Infrastructure. Development in, or adjacent to designated Green Infrastructure will have to consider the impact it would have on these sites.
- Policy G2: Creation of New Tree Cover states “Development which would result in harm to, or the loss of, Ancient Woodland and Veteran Trees will be resisted”
- Policy G8: protection of Important Species and Habitats. Development will not be permitted which would seriously harm, directly or indirectly, designated sites of biodiversity or geological importance.
- Policy G9: Biodiversity Improvements. Development is required to demonstrate biodiversity net gain.

7.4.15 The above group of policies are concerned with the protection and enhancement of the natural environment. An appraisal of potential effects on the landscape, existing ecological features and opportunities for promoting biodiversity has been undertaken and is reported in the Environmental Report **(NR16)**. The Scheme has been designed to avoid areas of higher ecological value and specific measures are to be implemented to mitigate in those areas where there is unavoidable impact.

7.4.16 The Environmental Report **(NR16)** also confirms that there will be no adverse residual construction or operational effects on the natural environment. Examples of the type of mitigation are quoted in the Planning Statement **(NR13)**, para 7.8.10 onwards). This also includes detail on the impact on existing trees in conjunction with the Environmental Report **(NR16)**: Volume 3: Appendix 9: Technical Note: Arboriculture. This confirms that there will be limited impact on trees. This is underpinned by adherence to the draft planning condition 4 **(NR12)** (plans of trees to be removed, retained and protected during preliminary works) and the compensatory loss of trees

through implementation of the Landscape and Ecological Management Plan (LEMP).

7.4.17 In terms of biodiversity net gain (BNG), Network Rail is committed to deliver 10% BNG over the whole route as enshrined in the Transpennine Route Upgrade – Programme wide Net Positive Biodiversity Strategy (Revision P03.1), and a BNG assessment will be carried out to meet the Strategy objectives. BNG will be delivered locally wherever practicable. It will be more diverse and species-rich and connectivity between habitats will be enhanced. These improvements will be secured by means of planning conditions relating to BNG itself and a Landscape and Ecology Management Plan (see **(NR12)**).

- Policy EN5: this seeks to manage flood risk. As none of the Scheme sites are within Flood Zones 2 or 3 and are not at risk from fluvial flooding. There is a degree of surface water flooding risk affecting some of the Scheme sites, but this will be controlled through the production of construction phase surface water management plans where required under the auspices of the proposed Deemed Planning Permission condition No. 6 and Chapter 7 of the CoCP Part A **(NR17)**, providing appropriate surface water flooding prevention / mitigation. In its operational phase the only additional mitigation is required at the Micklefield TSC, where it will be built with raised finished floor levels and a drainage system will be installed. As such the Scheme complies with the policy.
- Policy T1: Transport Management. To compliment the provision of new infrastructure the Council will support the following management priorities:
 - a) develop and provide tailored, interactive, readily available information and support that encourages and incentivises more sustainable travel choices on a regular basis;
 - b) sustainable travel proposals including travel planning measures for employers and schools.

7.4.18 The Order Scheme will address this policy through the careful assessment of traffic and transport related impacts and consequential effects during construction and operation. This is included in the Environmental Report **(NR16)**: Volume 3: Appendix 11: Technical Note: Transport. There will be a TRU logistics construction strategy concentrating on a central construction hub with a number of satellites to provide interim staging posts and minimising as far as possible use of HGV's and promoting the delivery of materials by rail if possible. However, there are no predicted significant effects due to increases in traffic movements.

- 7.4.19 Mitigation measures will be adopted including those specified in a Construction Traffic Management Plan (CTMP), again the subject of a specific condition in the request for Deemed Planning Consent (**NR12**). This will outline the range of measures to minimise construction traffic impacts as outlined in the Environmental Report Appendix 11. Of particular relevance to Policy T1 is the preparation of a Travel Plan for project staff will be produced as required to encourage sustainable travel.

Leeds Unitary Development Plan Review ‘Saved’ Policies (adopted 2006) (the ‘UDP’) (NR37)

- 7.4.20 The Leeds UDP was adopted in 2001 and reviewed in 2006. When the Core Strategy was adopted in 2019, ‘Saved’ Policies from the UDP were retained. The following policies are considered relevant considerations for the Scheme, though the way in which the Order Scheme meets these policies is very similar to the Core Strategy policies as outlined previously.
- 7.4.21 Policy N14 Listed Buildings and Preservation. There will be a presumption in favour of the preservation of listed buildings. Consent for the demolition or substantial demolition of a listed building will be permitted only in exceptional circumstances and with the strongest justification. Given that TRU forms an important part of Northern Powerhouse Rail, which is a key driver for economic growth across the whole of the north of England, and that TRU on its own will deliver substantial benefits associated with faster, more reliable rail connections and a decarbonised railway, it is considered that the public benefits of TRU outweigh the total loss of the listed bridges, justifying total loss, as required by the policy.
- 7.4.22 Policy N17 Listed Buildings Character and Appearance. This policy seeks to preserve the character of existing listed buildings wherever possible. This is exemplified through the retention of bridge Crawshaw Woods bridge, where the most important element of the bridge (the cast iron superstructure) is to be retained and repaired.
- 7.4.23 Policy N24 Development Proposals Next to Green Belt / Corridors states that where development proposals about the Green Belt, green corridors or other open land, their assimilation into the landscape must be achieved as part of the scheme. If existing landscape features would not achieve this, a landscaping scheme will be required to be implemented which deals positively with the transition between development and open land.
- 7.4.24 The Order Scheme incorporates indicative landscaping proposals involving native species hedgerow and tree planting, designed to be sympathetic to the surrounding landscape character whilst offering screening and softening of new structures as appropriate, along with compensatory planting for the loss

of vegetation necessary as part of the construction works. This is proposed to be secured by way of planning condition 4 ecology and landscape management plan, in the Request for Deemed Planning Permission and Statement of Proposed Conditions **(NR12)**.

- 7.4.25 Policy N29 Sites of Archaeological Importance seeks to preserve such sites and instigate appropriate investigation as required.
- 7.4.26 Predicted archaeology related impacts and consequential effects of the Order Scheme are assessed in the Environmental Report **(NR16)**: Volume 3: Appendix 6: Technical Note: Archaeology. This appraisal considered a zone 250m around the sites, identifying archaeological assets with the potential to be affected by the Scheme.
- 7.4.27 These include the former Barnbow World War I National Filling Factory (NHLE 1415057), a loopholed gatehouse (MWY5326) to the former Vickers tank factory, part of the Barnbow Royal Ordnance Factory, and impacts on the site of St Peter's graveyard at Penny Pocket Park, where the railway boundary sits adjacent to an early-medieval period graveyard. In addition, around 20 non-designated West Yorkshire Historic Environment Record below-ground assets have been identified within the 250m study area. They include features such as medieval villages, crop marks, bellpits and other features indicative of Iron Age farming, most of which will not be affected by the Order Scheme.
- 7.4.28 Archaeological recording of assets affected will be secured through the implementation of section 5 of the Code of Construction Practice Part A **(NR17)**. The proposed deemed planning permission condition No. 9 provides for the preparation and approval by the local planning authority of an agreed construction methodology and, if necessary, a Written Scheme of Investigation (WSI). Therefore, overall the Order Scheme complies with Core Strategy policy P11 on conservation and saved UDP Policy N29 that requires investigation of archaeological assets of the type affected.
- 7.4.29 Policy N33 refers to development in the Green Belt. The elements of the Order Scheme located within the Green Belt are the re-constructed bridge at Crawshaw Woods, the new Barrowby Lane bridleway bridge, the new Ridge Road bridge, and highway works associated with the closure of Peckfield Crossing.
- 7.4.30 The works at Barrowby Lane and Peckfield are considered to be local transport infrastructure which by reason of location must be in the Green Belt (see below).

- 7.4.31 However, the raising of the bridges at Crawshaw and Ridge Road are due to electrification, which is not considered to be of simply local importance but has an important regional dimension. As such it would also be necessary to consider the special circumstances which exists to justify the development.
- 7.4.32 It is considered that the changes to the bridges at Crawshaw Woods and Ridge Road will not reduce the openness of the Green Belt, since the only difference will be to raise the parapets of each bridge by a small amount that will only be noticeable at very close quarters. With no reduction in Green Belt openness and no conflict with the purpose of the Green Belt, the level of harm to the Green Belt is minimal. Given the importance of the Transpennine line as a key component of national transport infrastructure, the Scheme is essential in maintaining and improving the performance and capacity of the line. This in turn supports Government policy on sustainable transport and achieving reductions in greenhouse gas emissions, reducing congestion and delivering economic benefits. These needs and benefits clearly outweigh any minimal harm to the Green Belt and provide the very special circumstances to justify development in the Green Belt.
- 7.4.33 The Peckfield Level Crossing Closure works involve local at-grade access improvements on Pit Lane / Lower Peckfield Lane, with no substantial above-ground built structures, on the boundary of the Green Belt. These works must be inside the Green Belt because alternative land outside and adjacent to the boundary of the Green Belt, is in use as a public recreation ground. These works will therefore not affect Green Belt openness.
- 7.4.34 The New Barrowby Lane Bridge works will provide a local transport solution to allow the removal of two level crossings and to provide a safe means of crossing the railway.
- 7.4.35 Both level crossings are entirely within the Green Belt and there is no alternative viable location outside the Green Belt to site the new structure. Landscape analysis (Appendix 8 of the Environmental Report **(NR16)**) concludes that the introduction of the New Barrowby Lane Bridge will not affect Green Belt openness, save for short-distance views in the immediate vicinity. In this setting, any impact on Green Belt openness will be very small and the overall openness of the Green Belt will be preserved. Even if it is considered that the New Barrowby Lane Bridge would reduce the openness of the Green Belt, it is considered that very special circumstances exist that justify its approval, (similar to the justification for the Crawshaw Wood and Ridge Road bridge works).
- 7.4.36 An extract of the UDP proposals map showing the extent of the Green Belt in these areas can be found in figure 13 on page 68 of the Planning Statement **(NR13)**.

- 7.4.37 Policy N35 Development and Agricultural Land. This states that development will not be permitted if it seriously conflicts with the interests of protecting areas of best and most versatile agricultural land.
- 7.4.38 An assessment of Agricultural Land Classification shows small elements of the Scheme covering both Grade 2 and Grade 3 land, with Grade 3 in the west, between Austhorpe and Crawshaw Woods and Grade 2 in the east, between Barrowby and Peckfield. During construction, a total of about 7.6ha of Grade 2 and Grade 3 agricultural land will be temporarily affected. A Soil Resource Management Plan (part of the CoCP Part B) will be implemented during construction, to ensure the reinstatement of the land to its pre-construction condition and quality. Very small amounts of agricultural land will be lost permanently at certain locations, (0.3Ha Grade III and 1.2Ha Grade II) but it is considered that the overall loss of agricultural land does not conflict with the interests of protecting the best and most versatile agricultural land therefore the proposals are consistent with UDP policy N35 (and paragraph 174 of the NPPF).
- 7.4.39 Policy N37A Development in the Countryside. This states that all new development or change in land use within the countryside should. have regard to the character of the landscape in which it is set and maintain particular features which contribute to this; and, where appropriate, contribute positively to restoration or enhancement objectives by incorporation of suitable landscape works.
- 7.4.40 Again, as stated previously, the Order Scheme proposals seek to assimilate new features into the overall railway corridor as unobtrusively as possible. The effect of the Scheme on landscape character and visual amenity is set out in the Environmental Report **(NR16)**.
- 7.4.41 Volume 3: Appendix 10: Technical Note: Landscape and Visual. It concludes that the Scheme will not introduce any new uncharacteristic landscape elements and that changes to landscape character are expected to be small, with the greatest small impacts occurring temporarily during construction. Again, the LEMP will be approved under planning condition by LCC, covering details of landscape, visual and biodiversity impact mitigation schemes for the Order Scheme. it is therefore consistent with Policy N37A.

Site Allocations Plan (adopted 2019) (the ‘SAP’) (NR38)

- 7.4.42 The SAP was adopted on 10th July 2019. Following a successful high court challenge by the Aireborough Neighbourhood Development Forum the SAP was reviewed in August 2020. The review led to all housing sites (including mixed-use sites) that, immediately before the adoption of the SAP were in the

green belt, (37 sites) were to be remitted back to the SoS and the Planning Inspectorate for further examination.

- 7.4.43 One of the sites relevant to the review of the SAP is land to the south of Crawshaw Woods bridge; originally allocated for housing and employment as a mixed use (allocation MX2-38), this was deleted in the first set of proposed modifications to the SAP in March 2021. It was replaced with an employment-only allocation (EG2-37) on the same footprint.
- 7.4.44 Following the closure of the consultation on the main modifications, a half-day hearing was held in May 2022 to discuss both the integrated rail plan and the proposed allocation EG2-37. Following the hearing, the Inspector responded to the council in December 2022 concluding that the removal of the site EG2-37 from the Green Belt was not justified by way of exceptional circumstances. A consultation took place until 27th January 2023 to respond to this conclusion. The outcome of this consultation is yet to be published. In any event, given that the compound required to deliver the Crawshaw Woods bridge works that is coincident with EG2-37 is needed for around 6 months in 2025, it is not expected that these works would impact adversely on the delivery of this employment land allocation or the yet to be determined planning application affecting this land (reference 22/08491/OT).
- 7.4.45 The other allocations relevant to the Order Scheme are discussed fully in the Planning Statement (**NR13**, paragraph 6.3.31 onwards). None have any significant effect on the Order Scheme.

Natural Resources and Waste Local Plan (adopted 2013) (the ‘NRWLP’) (NR39)

- 7.4.46 Relevant policies include Water 2: Protection of Water Quality. This seeks to protect sensitive water bodies through the control of surface water run-off. Water 6 addresses flood risk. Land 1 addresses contaminated land, including thorough site analysis and remediation as necessary. Land 2 considers development and trees, seeking to protect the latter and where necessary for removal, replacement trees should be on the basis of 3 new for every one tree lost, preferably on site.
- 7.4.47 In accordance with NRWLP Policy Land 1, the Order Scheme designs have been carefully developed to avoid the loss of trees as far as practicable. This has involved carrying out an arboriculture survey of all the development sites and using survey information to work with the design team to consider alternative locations for works and to amend designs and compound land requirements and methods of working, to reduce tree loss to the absolute minimum required. Planting proposals will comply with the LCC planning

policy requiring 3:1 replacement tree planting, in accordance with the NRWLP Policy Land 1 and Land 2 Emerging development plan.

The Leeds Local Plan Update Publication Draft Consultation 2022, Your Neighbourhood, Your City, Your Planet (NR41)

- 7.4.48 The policies set out in this document would effectively update the 2019 Core Strategy. A further period of consultation on proposed changes has been concluded in December 2023. Examination of the Plan is expected around Spring 2024, with adoption expected around Autumn 2024. Those relevant to the Order Scheme include the overall approach, in pursuing objectives of carbon reduction and sustainable infrastructure – *“including in the latter that:*

Leeds will ensure the delivery of an accessible and integrated transport system which focuses on public transport and active travel and is worthy of its role at the heart of Leeds City Region, supporting communities and inclusive growth.”

- 7.4.49 In addition, policy SP11A of the Draft Consultation (Mass Transit & Rail Infrastructure) states that projects that enable the delivery of a West Yorkshire Mass Transit network in Leeds will be supported to deliver improved connections, East Leeds is specifically mentioned as an opportunity. Rail infrastructure improvement schemes will be supported where they are designed to ensure that any potential adverse environmental, social and economic impacts are minimised and mitigated, and that any potential benefits or opportunities are maximised.

The Garforth Neighbourhood Plan (GNP) (NR40)

- 7.4.50 The GNP was approved on 1st September 2023.
- 7.4.51 Policies from the GNP which are relevant to the Order Scheme proposed include Policy HBE9, where it is recommended that hedges and boundary treatments in the vicinity of Nanny Goat Lane and Barrowby Lane are retained. Policy GSRE5 seeks to enhance existing local green corridors, retaining existing trees and hedgerows with emphasis on soft rather than hard landscaping. GSRE 6 seeks measures appropriate to the scale of the development to link and enhance the green infrastructure network. Policy GSRE7 supports improvements to the right of way network and GSRE9 seeks to protect and enhance the area's biodiversity, with GSRE10 seeking a commitment to secure a net gain in biodiversity. Finally, GSRE 11 seeks to protect the special landscape character of Garforth. The development proposals for the new Barrowby crossing comply with (in particular) the policy GSRE7 as they will facilitate the safe and convenient crossing of the railway

allowing for the elimination of two level crossings whilst seeking to preserve existing hedgerows as far as possible.

North Yorkshire

7.4.52 It should be noted that there is no deemed planning consent request attached to any works in the North Yorkshire area. However, there is support for the Scheme in the North Yorkshire Local Transport Plan (as discussed in paragraphs 7.3.4 -7.3.6) and in the Selby District Core Strategy Local Plan 2013 (still the extant development plan document for this part of the North Yorkshire administrative area). Policy SP15 (Sustainable Development & Climate Change), states that: *Schemes should, where appropriate:*

- *“improve energy efficiency and minimise energy consumption;*
- *protect, enhance and create habitats to both improve biodiversity resilience to climate change and utilise biodiversity to contribute to climate change mitigation and adaptation; include tree planting; and*
- *make provision for cycle lanes and cycling facilities, safe pedestrian routes and improved public transport facilities.”*

7.5 National Planning Policy

National Planning Policy Framework (NPPF), DCLG (2012 & updated September 2023) (NR35)

7.5.1 The NPPF is the overarching national planning policy document. Paragraph 11 of the NPPF states that, at the heart of the Framework, is a presumption in favour of sustainable development. For decision-taking this means approving development proposals that accord with the development plan without delay. In instances where there are no relevant development plan policies, permission should be granted unless there are clear NPPF policy reasons for refusal or any adverse impacts would significantly and demonstrably outweigh the benefits, when assessed against the NPPF.

7.5.2 **Paragraph 8** sets out the three key objectives of the planning system - economic, social and environmental objectives, including minimising pollution, identifying and co-ordinating the provision of infrastructure and moving to a low carbon economy.

7.5.3 **Chapter 6 (Building a strong, competitive economy)** states that planning should operate to encourage and not act as an impediment to sustainable growth and significant weight should be placed on the need to support economic growth and productivity.

- 7.5.4 **Chapter 9 (Promoting sustainable transport)** states that the transport system needs to be balanced in favour of sustainable transport, giving people a real choice about how they travel (paragraph 104). It further states that encouragement should be given to solutions which support reductions in greenhouse gas emissions and reduce congestion.
- 7.5.5 **Chapter 12 (achieving well-designed places)**.states that planning policies and decisions should aim to ensure that inter alia, developments function well over their lifetime are visually attractive as a result of good layout and appropriate and effective landscaping and are sympathetic to landscape setting.
- 7.5.6 **Chapter 13 (Protecting Green Belt land)** states that certain forms of development are not inappropriate in the Green Belt provided they preserve its openness and do not conflict with the purposes of including land within it. This includes proposals for local transport infrastructure which can demonstrate a requirement for a Green Belt location (paragraph 150).
- 7.5.7 **Chapter 14 (Meeting the challenge of climate change, flooding and coastal change)** paragraphs 161-164 state that local planning authorities should ensure flood risk is not increased elsewhere and only consider development appropriate in areas at risk of flooding where, informed by a site-specific flood risk assessment following the Sequential Test and, if required, the Exception Test provided it can be demonstrated that:
- the development would provide wider sustainability benefits to the community that outweigh the flood risk; and
 - the development will be safe for its lifetime taking account of the vulnerability of its users, without increasing flood risk elsewhere, and, where possible, will reduce flood risk overall.
- 7.5.8 **Chapter 15 (Conserving and enhancing the natural environment)** paragraph 174 states that planning decisions should contribute to and enhance the natural and local environment by “recognising the intrinsic character and beauty of the countryside, and the wider benefits from natural capital and ecosystem services – including the economic and other benefits of the best and most versatile agricultural land, and of trees and woodland”.
- 7.5.9 **Paragraph 180** sets out the importance of recognising the wider benefits of ecosystems, minimising impacts on biodiversity and providing net gains in biodiversity where possible (paragraph 180).
- 7.5.10 **Paragraph 186** states that planning policies and decisions should sustain and contribute towards compliance with relevant limit values or national

objectives for pollutants, and opportunities to improve air quality or mitigate impacts should be identified, such as green infrastructure provision and enhancement.

- 7.5.11 **Chapter 16 (protecting the Historic Environment)** stresses in paragraph 194 the need “where a site on which development is proposed includes, or has the potential to include, heritage assets with archaeological interest, local planning authorities should require developers to submit an appropriate desk-based assessment and, where necessary, a field evaluation.”

How the Order Scheme meets the requirements of the NPPF

- 7.5.12 The need for and benefits of TRU and the Scheme itself are set out in sections 2 and 5 of the Planning Statement (**NR13**) as well as in the Statement of Aims (**NR04**).
- 7.5.13 The Order Scheme comprises a series of improvements to the Transpennine Route. The development in its own right provides additional capacity on the Route, thereby strengthening and reinforcing the opportunity for rail to be a viable alternative to road-based traffic, with its commensurate benefits on climate change and sustainability. It also improves connectivity along the Route, which in turn acts as an enabler for economic improvements. This clearly accords with the Government’s desire for the planning system to facilitate the building of a strong and competitive economy as set out in chapter 1 of the NPPF.
- 7.5.14 Railways are generally recognised as being a more sustainable transport system than the private car. To achieve a modal shift from the private car to trains, the railway system needs to be made more attractive to users in terms of its reliability and capacity. The Scheme therefore supports and fulfils the core land use planning principles of the NPPF by contributing to low-carbon economic growth and promoting sustainable travel in the region.
- 7.5.15 This is entirely consistent with chapter 9 of the NPPF (Promoting sustainable transport) and the environmental benefits of the Scheme in relation to carbon footprint are detailed fully in the Environmental Report (**NR16**).
- 7.5.16 Further benefits of the TRU and the Order Scheme will be improved efficiency and reliability of the railway, bringing benefits of increased capacity (train capacity and frequency), reduced incidents and delays and reduced journey times, improve safety through level crossing closures and reduce operation and maintenance costs.
- 7.5.17 It will also deliver wider environmental and socio-economic benefits, including a contribution to decarbonisation of the railway and improved local air quality, direct economic growth through employment and supply chain benefits from

construction, and the indirect regional economic benefits through connectivity of improved rail services.

- 7.5.18 As such the Order Scheme contributes directly to the aims of the NPPF as outlined in Chapters 6 and 8.
- 7.5.19 The NPPF encourages "limiting the need to travel and offering a genuine choice of transport modes. This can help to reduce congestion and emissions, and improve air quality and public health" (paragraph 105) and provides that the planning system should help to "shape places in ways that contribute to radical reductions in greenhouse gas emissions, minimise vulnerability and improve resilience; encourage the reuse of existing resources, including the conversion of existing buildings; and support renewable and low carbon energy and associated infrastructure" (paragraph 152).
- 7.5.20 By definition passengers who use the electrified railway will be contributing towards sustainable travel. During the construction period, through a combination of careful construction logistics planning, the production of a Travel Plan and the establishment of rail-linked construction hubs, significant capacity and congestion impacts on the highway network will be avoided and sustainable construction transportation promoted, in accordance with paragraphs 110-111 and 113. Additionally, electrification of the railway will bring important air quality benefits, aligning with the NPPF requirement for planning to contribute to improved air quality. The aims of the NPPF chapter 9 on promoting sustainable travel are thus met.
- 7.5.21 Overall, the Order Scheme design is appropriate in terms of land use, scale, layout, access and appearance in the context of a working railway environment in the community it serves. The Order Scheme delivers infrastructure that, where appropriate, will deliver high quality design, is visually attractive and incorporates effective landscaping, is sympathetic to local history and landscape and provides safe and convenient access. The Scheme therefore complies with the design policy set out in Chapter 12 (Good Design) of the NPPF.
- 7.5.22 The impact of the Order Scheme on the Green Belt is fully discussed in section 7.4 on local plan policy but in summary it is considered that the Order Scheme elements falling within the Green Belt are either local transport infrastructure which can demonstrate a Green Belt location or that very special circumstances exist to permit rail development in the Green Belt which would otherwise be inappropriate (Chapter 13 (Green Belt)).
- 7.5.23 The Order Scheme will help to implement a number of important Government objectives to meet the challenge of climate change, reducing our carbon

footprint and meeting sustainable development targets through facilitating the electrification of the line between Leeds and York. This will contribute towards improving the capacity and efficiency of the rail system, encouraging a modal shift to rail, helping to reduce the number of road vehicles and carbon emissions both on the roads and rail system. Additionally, mitigation and control measures will be used to minimise the impact of the Order Scheme in terms of sustainability and climate change. This will include embedding carbon reduction measures in the design process, the return of temporary land to its original condition at the end of the works, the use of hybrid or solar power at construction compounds, the development of a Resource Efficiency and Materials Sourcing Strategy (including a waste hierarchy principle of prioritising preventing waste in the first place and then in turn preparing waste it for reuse, then recycling, then recovery, and finally disposal). Such matters will also be governed through the CoCP and the preparation of a Materials Management Plan (MMP), again secured by means of an appropriate planning condition.

- 7.5.24 In terms of flood risk and climate vulnerability a detailed assessment has found that none of these vulnerabilities are significant, as embedded mitigation sufficiently adapts the design and operational processes in compliance with Network Rail engineering standards to remove and reduce to acceptable levels all otherwise significant vulnerability impacts.
- 7.5.25 It is therefore considered that the Order Scheme is compliant with chapter 14 (Meeting the issue of Climate Change) of the NPPF in taking a proactive approach to mitigating and adapting to climate change and by avoiding increased vulnerability to the risks of climate change.
- 7.5.26 The Order Scheme, through the preparation of detailed Ecology and Landscape Management Plans (LEMP), will seek to protect the existing landscape and biodiversity as far as possible. The LEMP and dedicated planning condition relating to Biodiversity Net Gain will be the mechanisms by which the Scheme will enhance biodiversity in compliance with chapter 15 (Conserving and enhancing the Natural Environment). Additionally, protection of the existing environment during the construction phases of the Scheme will be achieved through implementation of the Code of Construction Practice (**NR17**) part B (the subject of a specific condition) governing issues such as contaminated land, noise and vibration, waste and materials management.
- 7.5.27 The Order Scheme will have an adverse effect on three listed structures (Austhorpe Lane, Brady Farm and Ridge Road bridges) and a positive effect on one other listed bridge (Crawshaw Woods). The works are necessary to facilitate the installation of O(H)LE through the increase in height of bridges

that are too low to safely accommodate overhead wires. Whilst it is recognised that substantial harm will occur in the loss of three bridges, the public benefit that will accrue from the overall Upgrade outweighs this loss. Further detail on the justification for the removal of the bridges and design of the new structures is given in the planning statement (**NR13**, section 4).

- 7.5.28 Similarly, any elements of archaeological interest will be protected or recorded in accordance with the relevant planning condition and compliance with chapter 16 (Protecting the Historic Environment) .

Conclusion

- 7.5.29 In summary, the elements of the Order Scheme for which deemed planning permission is sought will comply with the LCC development plan. By improving reliability, increasing capacity and reducing risk to the public by eliminating level crossings, the Scheme will satisfy those policies seeking to contribute to low-carbon economic growth and promoting sustainable travel in the region. It will also meet the NPPF policy for sustainable economic growth by providing opportunities (in combination with other schemes on TRU) to allow for an increase in line speeds and providing additional capacity for more trains to run on the route, improving connectivity between cities and towns either side of the Pennines and beyond to Scotland and the North East, by a mode of sustainable transport. It will make a positive contribution to targets to reduce carbon emissions by providing a credible alternative to the car. The proposals therefore comply with the aims and policies of the NPPF and the existing and emerging Local Plan for Leeds.

8. SCHEME DEVELOPMENT

8.1 Introduction

- 8.1.1 This chapter provides a brief overview of the process of developing the Order Scheme. Chapters 1 and 3 of this Statement of Case describes the Order Scheme in outline and Chapter 9 describes the Order Scheme in more detail and the construction strategy and details.

Network Rail GRIP process

- 8.1.2 The Governance for Railway Investment Projects (GRIP) is Network Rail's management and control process for delivering projects on the operational railway and is mandatory for all significant rail projects.
- 8.1.3 Network Rail has developed this approach to managing projects to minimise and mitigate the risks associated with delivering projects that enhance or renew the operational railway.
- 8.1.4 The approach is based on best practice within industries that undertake major infrastructure projects and best practice as recommended by the major professional bodies.
- 8.1.5 GRIP divides a project into eight distinct stages. The overall approach in GRIP is product rather than process driven, and within each stage an agreed set of products are delivered. The eight GRIP stages are listed below.

GRIP 1 - Output Definition

GRIP 2 - Pre-Feasibility

GRIP 3 - Option Selection

GRIP 4 - Single Option Development

GRIP 5 - Detailed Design

GRIP 6 - Construction Test & Commission

GRIP 7 - Scheme Hand Back

GRIP 8 - Project Close Out

GRIP stage-gate process

- 8.1.6 Formal stage-gate reviews are held at varying points with the GRIP lifecycle. The stage-gate review process examines a project at critical stages in its lifecycle to provide assurance that it can successfully progress to the next GRIP stage.

- 8.1.7 The Order Scheme has completed its GRIP 4 stage gate and is currently in GRIP 5 option development, which is being undertaken in parallel with the Order process.

Stakeholder involvement

- 8.1.8 Between winter 2021 and spring 2023, Network Rail undertook a programme of stakeholder and public consultation on its proposals for the Scheme. This involved relevant statutory authorities, landowners, tenants, lessees, occupiers of land and the general public within the Scheme area. A formal statutory and public consultation exercise on the Scheme proposals took place in October – November 2022 and is summarised in the Consultation Summary Report (NR07).
- 8.1.9 Consultation has been important in helping to identify potential alternative options and factors that should be taken into account when evaluating options, thereby informing the final proposed designs of the various Scheme elements. Consultation and engagement is discussed further in Chapter 14 of this Statement of Case.

Order Scheme options

- 8.1.10 The approach taken to the development of the different types of Order Scheme element, including consideration of alternative options, is described in turn in the following sections.

8.2 Bridge replacement works compounds

- 8.2.1 The Order Scheme includes land for construction compounds required to carry out bridge replacement or demolition works at: Kirkgate Viaduct Temporary Compound; Marsh Lane Viaduct Temporary Compound; Osmondthorpe Lane; Austhorpe Lane Overbridge; Crawshaw Woods Bridge Raising; Brady Farm Bridge Temporary Compound and A656 Ridge Road Replacement. In each case, construction compounds are required adjacent to the bridges to facilitate the construction works. These compounds need to be of a size suitable to accommodate the specific construction requirements for each bridge. Land for these construction compounds was selected to meet these construction requirements, including access from the public highway to allow the access/egress of construction vehicles.
- 8.2.2 The selection of land for bridge replacement works compounds sought to minimise impacts on private land generally and avoid land take from residential properties.
- 8.2.3 Additionally, compound requirements were assessed against environmental considerations and compound areas and layouts were located and designed

where possible to minimise impacts on trees and protected species and their habitats and on nearby sensitive receptors such as residential properties.

8.3 Strategic works compounds

- 8.3.1 The Order Scheme includes land for strategic works compounds, adjacent to the railway, which are required to facilitate construction of the TRU E2-4 Project, for example wider track renewal and electrification works. These Order Scheme strategic works compounds are Wykebeck Avenue Compound, Manston Lane Compound and Phoenix Avenue Compound. Of these three strategic works compounds Wykebeck Avenue is a small compound for transferring equipment to the railway whereas Manston Lane and Phoenix Avenue are larger compounds to accommodate offices, parking and material storage.
- 8.3.2 These compounds need to be located strategically along the E2-4 Project route to allow rapid access by construction teams to sections of track, to minimise nightly travel time during 'rules of the route possessions' (see Chapter 9), thereby optimising the delivery programme.
- 8.3.3 In selecting suitable land for these compounds, a key requirement was the ability to transfer material and workforce from the public highway to the railway via road-rail access points (**RRAP**). A more detailed description of the construction strategy that informs the selection of locations for strategic works compounds is provided in Chapter 9.
- 8.3.4 Central to the selection of the locations of Wykebeck Avenue Temporary Compound and Phoenix Avenue Temporary Compound is the presence of an adjacent existing RRAP. At Manston Lane Compound, a new RRAP will be created adjacent to the Compound, which will replace an existing RRAP located approximately 200m to the west of the new RRAP location. All of the strategic works compounds benefit from occupying land that is available, with lease or licence agreements in place or under negotiation (see Chapter 13). Other key considerations in selecting locations for strategic works compounds were accessibility to the public highway and minimising impacts on sensitive receptors, particularly residential properties. All of the strategic works compounds benefit from adequate public highway access arrangements and the larger Manston Lane Compound and Phoenix Avenue Compounds are located away from residential areas. The Wykebeck Avenue Compound is located in an urbanised section of the E2-4 route where there are no alternative locations offering road to rail access further away from residential properties.

8.4 Listed bridges

- 8.4.1 Three of the Order Scheme elements, at Austhorpe Lane, Crawshaw Woods Bridge Raising and A656 Ridge Road Replacement, involve the dismantling or demolition and reconstruction of a Grade II listed overbridges. Chapter 11 of this Statement of Case provides an overview of the two main aspects of design development for these listed overbridges: strategic options to facilitate the installation of overhead line equipment (i.e. either lowering the track, slewing the track or raising the height of the bridge) and, once raising the height of the bridges was selected as the preferred strategic option, the development of a bespoke design solution for replacement bridges that is acceptable in heritage terms.
- 8.4.2 Section 8.5 of this Statement of Case goes on to provide a summary of the highway design options considered for the two listed bridges that carry a public highway over the railway.

8.5 Highway overbridges – highway design development

- 8.5.1 This section provides a description of the highway design development of the two Order Scheme elements that carry a public highway over the railway, Austhorpe Lane Overbridge and the A656 Ridge Road Overbridge.

Austhorpe Lane Overbridge

- 8.5.2 In the first stage of design option development for a replacement Austhorpe Lane Overbridge, the design followed the existing horizontal alignment and single lane arrangement for the carriageway, with a road width of 4.0m, a 2.0m footpath incorporated on the eastern side of the highway carriageway and a 0.5m raised hard strip on the western side.
- 8.5.3 This design also involved the demolition of the existing footbridge to the west of the main Austhorpe Lane overbridge, resulting in a change to the pedestrian route across the bridge on the eastern side. The eastern side placement of the footpath would provide some forward visibility for southbound traffic, which is acknowledged as substandard in the existing arrangement.
- 8.5.4 The vertical profile of the Austhorpe Lane Overbridge in this first design option was to follow the existing alignment also, but at a higher level above the railway due to the increased headroom requirements for the OLE equipment.
- 8.5.5 During consultation with Leeds City Council Highway Authority, the Authority specified that a two-lane highway design was required.

- 8.5.6 In the second stage of design option development, a number of options were considered that could accommodate a two-lane highway and pedestrian footway. The proximity of residential properties on the northeast and southwest corners of the Overbridge severely limits design flexibility for the two-lane requirement, resulting in the rejection of several design options for varying carriageway widths that involved land take from these residential properties.
- 8.5.7 Following an iterative design review process involving the Highway Authority, a preferred design option was selected that delivers a two-lane 5.5m wide carriageway, a 2.0m footpath on the western side of the bridge and a 0.5m wide raised hard strip on the eastern edge of Austhorpe Road and over the bridge.

Ridge Road Overbridge

- 8.5.8 The first stage of design option development for Ridge Road overbridge involved a design that matched the existing horizontal and vertical alignment where possible, providing a 5.97m wide two-lane carriageway, 1.45m wide raised hard strip on the western edge of the bridge and a 2.0m wide footpath on the eastern edge of the bridge. The overall width of the carriageway between inner faces of parapets was 9.42m.
- 8.5.9 As Ridge Road is straight at this location, the horizontal highway profile needs to follow the existing road centreline. Ridge Road is also gently rising over the bridge, so the design intent was to follow this, but at a higher level above the track to provide the necessary clearances for the overhead line electrification equipment.
- 8.5.10 Following consultation with Leeds City Council Highway Authority, a further stage of design development was undertaken to consider relatively small amendments to the first stage design concept. These amendments included widening of the road width to 7.3m, reduction of the raised hard strip to 760mm and the footpath width reduced to 1.8m, resulting in a widening of the bridge to 9.86m between the inner faces of the bridge parapets. To maintain the existing centreline, the bridge deck was also moved 245mm towards the east. To maintain the existing 2 x 3.0m lane widths on Ridge Road, edge of carriageway lane markings would be used, providing an extra 500mm between the edge of the lane and the face of the kerb.
- 8.5.11 This second stage of design development also looked at options to accommodate the existing and future utilities requirements, with the Utility final iteration involving the relocation of all the utility diversions to utility bays under the bridge deck, which has allowed the overbridge surfacing over the

bridge structure to be reduced from 322mm at the road centreline to 232mm, the kerb upstands to be reduced and the overall parapet height to be shortened.

8.6 Level Crossings

Barrowby Lane and Barrowby Foot Level Crossing

8.6.1 Barrowby Lane and Barrowby Foot Level Crossings are located approximately 0.75km and 0.3km west of Garforth respectively. Barrowby Lane and Barrowby Foot level crossings are PRow bridleway and footpath level crossings respectively, connecting land north and south of the railway.

8.6.2 Two main options to mitigate the closure of these level crossings were considered.

- Option 1: A new ramped bridleway bridge to the west of Barrowby Lane Bridleway Crossing to provide an alternative route across the railway. Option 1 included two alternative routes to connect the diverted bridleway to the PRow network on the north side of the railway, as illustrated in Figure 12.

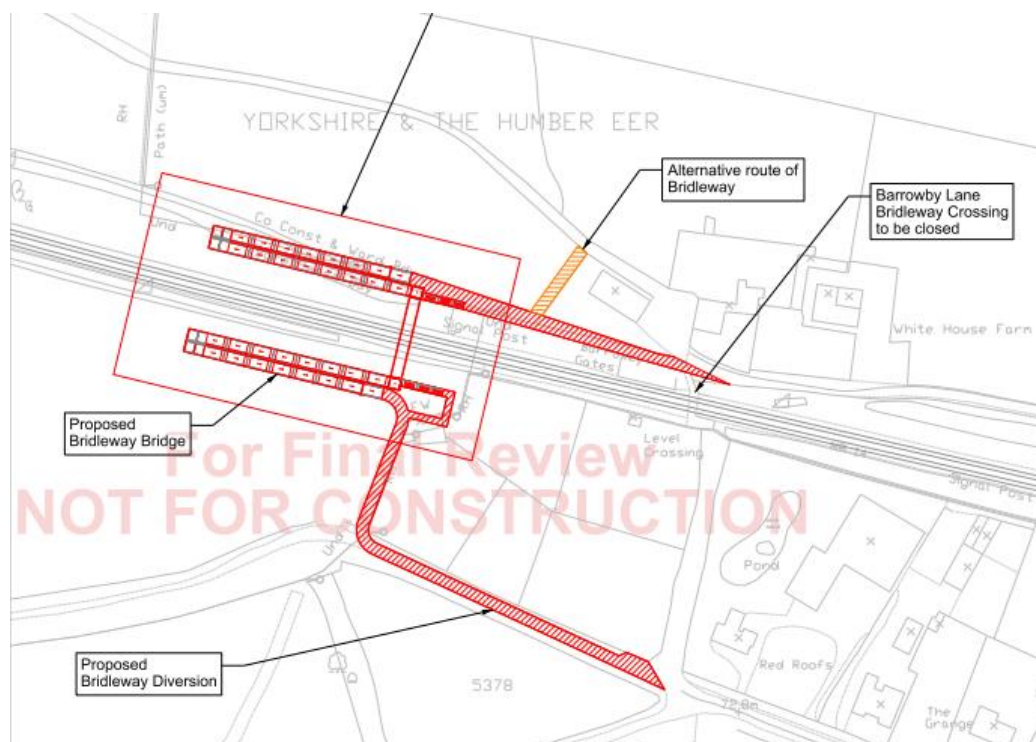


Figure 12 - Option 1 – New ramped bridleway bridge at Barrowby Lane Level Crossing

- Option 2: A new subway at the location of Barrowby Foot footpath Crossing, as illustrated in Figure 7.

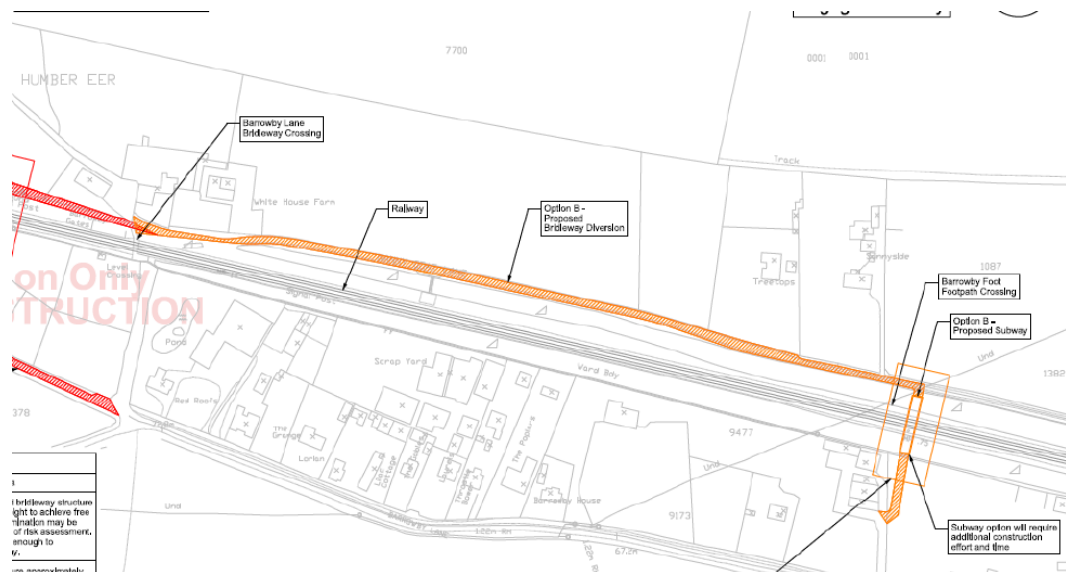


Figure 13 - Option 2 – New subway at Barrowby Foot level Crossing

- 8.6.3 Option 2 was ruled out due to cost, land, environmental and engineering constraints. In particular, the construction of an underpass in this location with poor quality embankment material would be complex, involving embankment stabilisation works requiring disruptive railway possession and impacts on property. Further, an underpass would be at risk of flooding and require long term maintenance and moving the bridleway crossing to this location, approximately 0.5km east of the existing Barrowby Lane bridleway level crossing, would be less convenient than option 1.
- 8.6.4 Option 1 was taken forward for further development and consultation, on the basis that it facilitates the closure of both level crossings, minimises land take and environmental impact, provides a convenient bridleway Public Right of Way diversion and is deliverable from an engineering, construction, maintenance and cost perspective.
- 8.6.5 Option 1 was subject to a targeted consultation process, involving local landowners, residents, interest groups and the local authority. Taking account of feedback from the consultation process, Option 1 involving the alternative bridleway route (Figure 14) was endorsed as the preferred Order Scheme design option, as it has a better connection to the wider PRow network to the north.

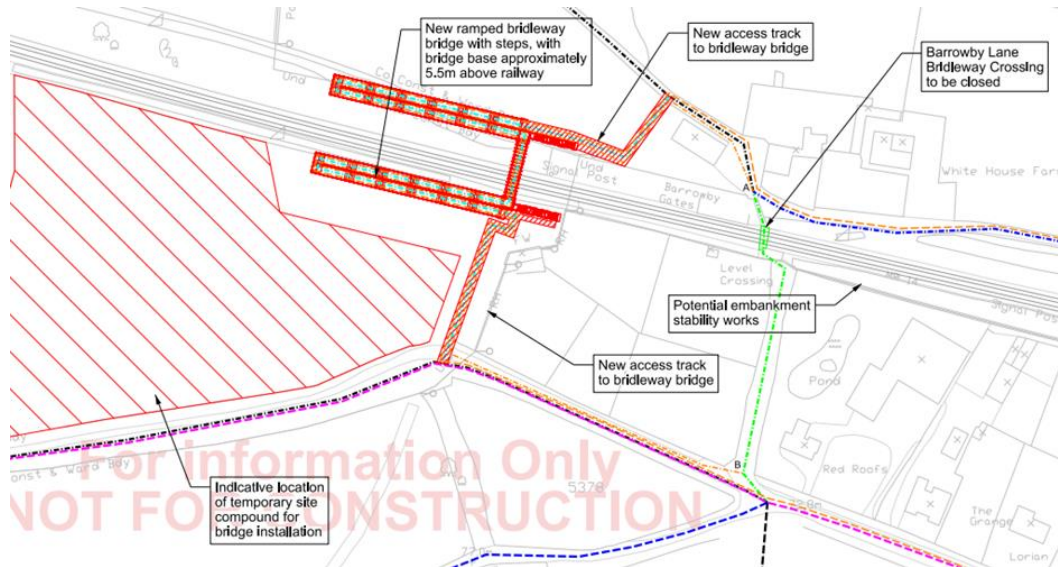
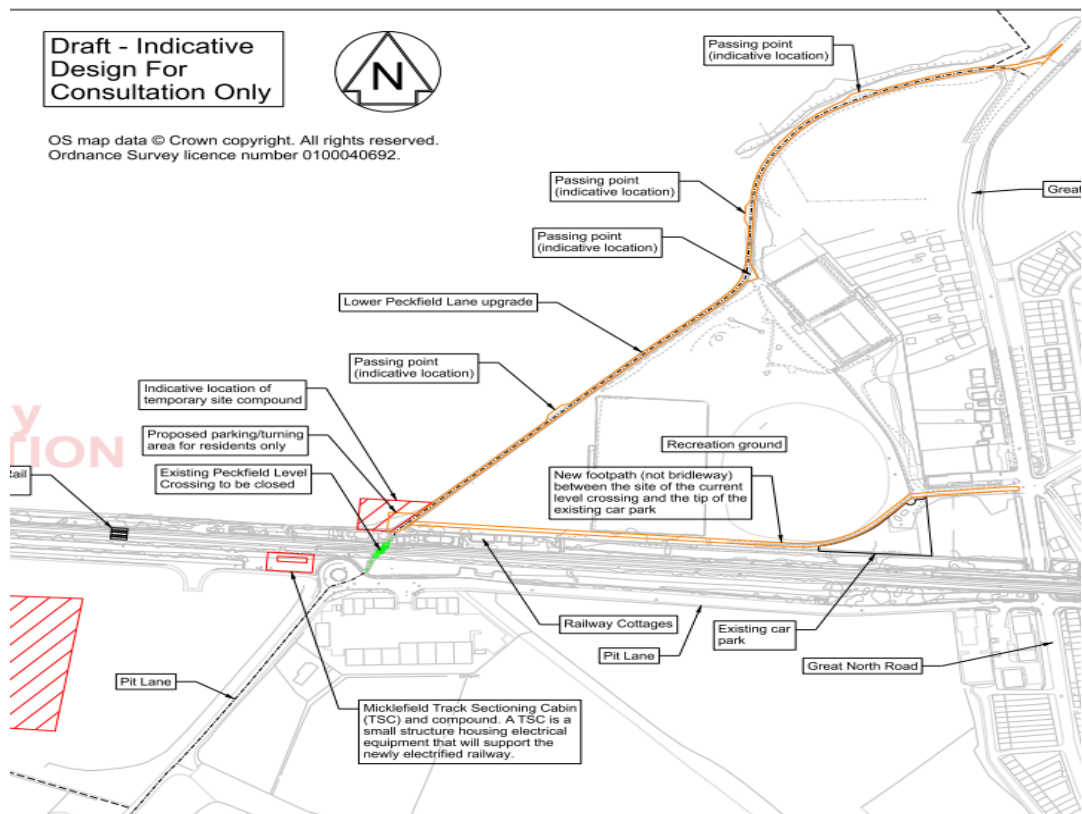


Figure 14 – Order Scheme design

Peckfield Level Crossing

- 8.6.6 Peckfield Level Crossing is located approximately 0.3km west of Micklefield Railway Station and is a PRow bridleway crossing connecting the land north and south of the railway. Figures 15 - 18 illustrate the five alternative options considered to mitigate the closure of the level crossing.



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Figure 15 – Option 1: New footpath and improvements to Lower Peckfield Lane (north of railway)

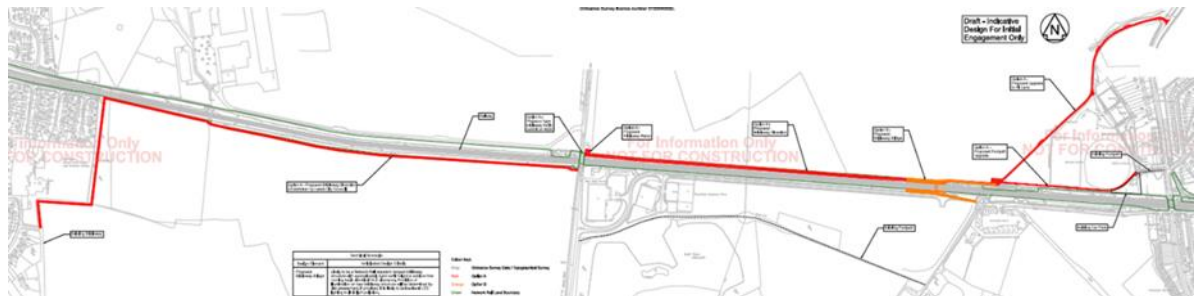


Figure 16 - Option 2: As Option 1 plus new brideway to East Garforth, via A656 crossing

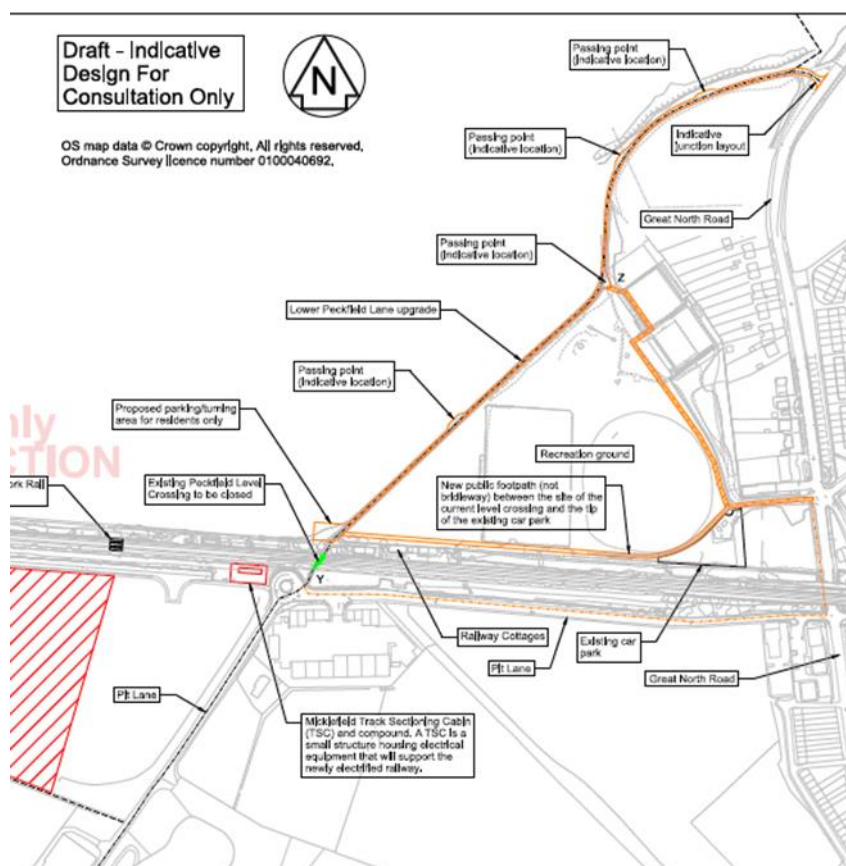


Figure 17 - Option 3: As Option 1 plus new brideway (SE-NW alignment) through recreation ground

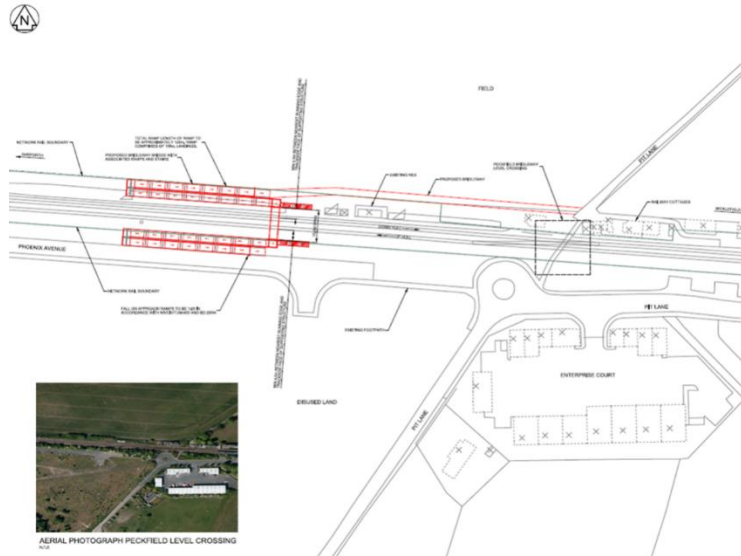


Figure 18 - Option 4 new ramped bridge and Option 5 new stepped bridge (same location – figure shows ramped option)

- 8.6.1 Following the identification and evaluation of these potentially feasible options, Option 1 was selected as the preferred option. Option 1 involves the creation of a new public footpath north of the railway, to connect the Lower Peckfield Lane Bridleway, to the Old Great North Road. A route south of the railway, via the existing public highway (footpaths) is then available to the junction of Pit Lane and Pit Lane Bridleway. The new public footpath will also provide a new direct pedestrian access between five residential properties located off Pit Lane adjacent to the railway (the Railway Properties) and Micklefield and the railway station. A variant of Option 1 is also included in the Order application, involving upgrading the proposed footpath referred to in this paragraph to a bridleway, in response to feedback from a final round of consultation promoting the benefits of this variant.
- 8.6.1 Both variants of Option 1 require very limited construction work and perform well against environmental and sustainability considerations, whilst also involving minimal land take and low impact on agricultural land. Option 1 also provides accessibility improvements for the Railway Properties and a pleasant, accessible (level), alternative route, via an existing footway on Pit Lane, the Old Great North Road and a new footpath or bridleway through the recreation ground. On cost, design, construction, maintenance and deliverability factors, Option 1 performs the best of all options considered.
- 8.6.2 Option 2 would involve the creation of a new PRow bridleway link between Micklefield and Garforth, requiring a new crossing of the A656 at Ridge Road Bridge. Following consultation with Leeds City Council Highway Authority, Option 2 was rejected, primarily due to highway safety and congestion

concerns, but also due to additional cost and land impacts compared with Option 1.

- 8.6.3 Option 3, involving the creation of a new PRow bridleway route through the Micklefield Recreation Ground on a broadly southeast to northwest trajectory, was rejected, primarily due to safety concerns raised during consultation regarding the interface between horses/cyclists and users of the recreation ground.
- 8.6.4 Option 4 and 5 both involve building a new large bridge structure, close to residential properties and would not perform as well as Option 1 on environmental, sustainability, cost, design, construction, maintenance and deliverability factors.
- 8.6.5 Further, extensive level crossing user survey information collected over a ten-year period indicates that weekday pedestrian user levels are limited to an average of between 22 and 45, increasing at weekends to a peak daily usage of between 59 and 126. Usage by cyclists is very low, with a maximum of 3 crossings per day recorded. Usage by persons of restricted mobility is very low and there are no survey records of equestrian use. The main findings of a recent origin-destination survey were that the main use purpose of using the Lower Peckfield Lane Bridleway was dog walking and almost all crossings were made by people from the local area, rather than long-distance walkers.
- 8.6.6 These low usage figures for users requiring an accessible bridleway route across the railway were not considered to justify the construction of Option 4 (ramped bridleway bridge), when the environmental impacts, impact on land and the significant construction costs involved are also taken into account.
- 8.6.7 Network Rail also considered a stepped footbridge option, but this Option 5 shares many of the disbenefits of Option 4. The stepped bridge would involve a pedestrian only diversion of approximately 300m, compared with the at-level Option 1 diversion of between approximately 100m and 900m, depending on the journey being taken, and this difference is not considered significant in the context of the predominantly local recreational use of the level crossing.
- 8.6.8 In addition to above, the bridge options would not provide the Option 1 benefit of a new direct footpath access into Micklefield for the Railway Properties, which is shorter and more convenient than the existing arrangement requiring passage over the level crossing.

Garforth Moor Level Crossing

- 8.6.9 Garforth Moor level crossing, located approximately 0.4km west of Garforth Station in Garforth, is a PRow footpath crossing connecting the north and

south of the railway. This level crossing is currently closed on safety grounds so is not presently accessible for public use.

- 8.6.10 Following an assessment of options, an alternative route between the south and north sides of the railway is proposed via the Barwick Road public highway and Barwick Road underbridge and then via the existing PRoW footpath Garforth No.7, which runs in a west-east and then north-south direction on a dog-leg route from Barwick Road, back to the railway and allotment gardens on the north side of the railway. Figure 19 illustrates the proposed route.

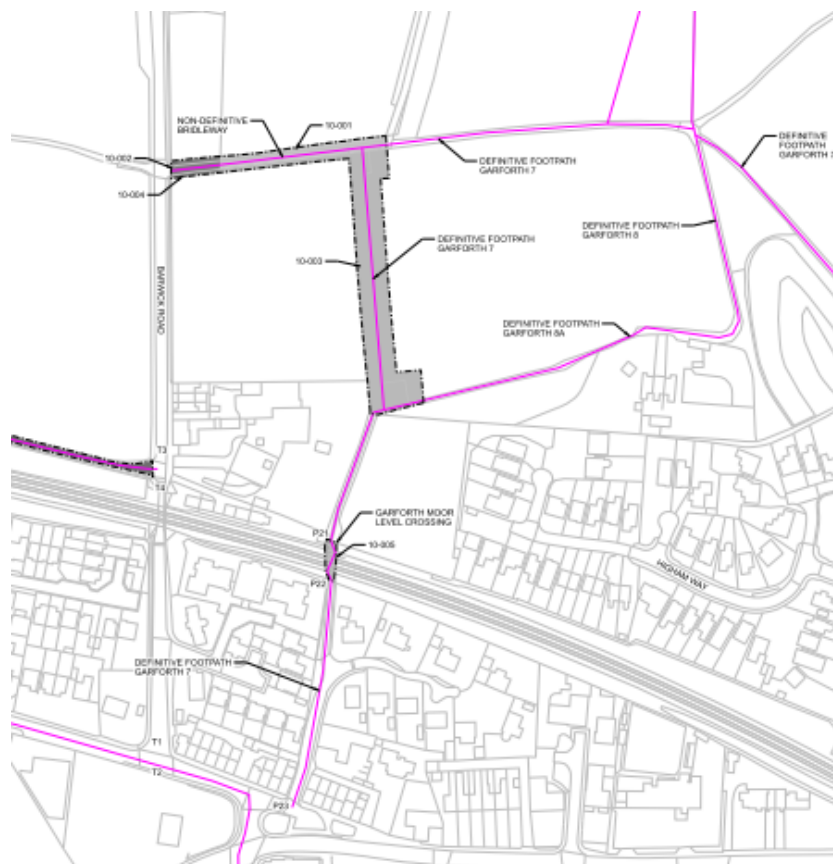


Figure 19 - Garforth Moor level crossing closure

- 8.6.1 A separate planning application for the construction of a vehicular access track, to facilitate vehicular access for allotment holders, is the subject of an extant planning application made to Leeds City Council (application reference 22/03144/FU/E).
- 8.6.2 This preferred option was selected because it makes use of existing public highway and footpath PRow, avoiding the need to create new public highway routes or new access from the public highway and does not adversely impact private land, agricultural businesses or residential properties that would otherwise be affected by the creation of a new access route.

Highroyds Wood Level Crossing

- 8.6.3 Highroyds Wood Level Crossing, located about 0.5km east of Micklefield is a PRow footpath crossing connecting the north and south of the railway. This level crossing is currently closed on safety grounds so is not presently accessible for public use.
- 8.6.4 Following an assessment of options, a diversion of the PRow along the route of an existing footpath track, via an existing railway underpass, is proposed. This diversion, together with the proposed permanent closure of Highroyds Wood Level Crossing, is the subject of a current Rail Safety Diversion application under section 119/119A of the Highways Act 1980. Figure 20 illustrates the proposed diversion.



Figure 20 - Highroyds Wood level crossing closure

- 8.6.5 This preferred option was selected as it provides a convenient alternative PRow route, taking advantage of an existing railway underpass, via a diversion of less than 300m if travelling in a south to northerly direction.

9. SCHEME DESCRIPTION AND CONSTRUCTION

9.1 Introduction

9.1.1 This chapter provides a summary description of the principal works that make up the Scheme and the construction of the same, in the context of the wider works and upgrades which make up the E2-E4 Project.

9.1.2 The Scheme has been split into 2 Route Sections based on geography;

- E2 - Leeds to Neville Hill East
- E3/4 – Neville Hill East to Church Fenton

The E2-E4 Project: an overview

E2 - Leeds to Neville Hill East

9.1.3 To achieve the required journey time improvements, line speed is increased from 25 mph to 55 mph over Marsh Lane Viaduct (from Leeds Station East junction to approximately 80m west of Shannon Street footbridge) and 55 mph to 75 mph for the remainder of the project area. Track renewals through this project area are extensive and the section of track through Richmond Hill cutting is remodelled to enable greater operational flexibility of trains between Leeds Station and Neville Hill Depot (the remodelling through Richmond Hill does not form part of the Scheme).

9.1.4 There are three bridges that are either under strength or life expired and need to be replaced, these are Kirkgate Viaduct, Marsh Lane and Osmondthorpe Lane. Three of the compounds in connection with these works are included within the Order Scheme (Osmondthorpe Lane temporary compound, Kirkgate Viaduct temporary compound and Marsh Lane temporary compound).

9.1.5 The line is currently electrified between Leeds Station and Neville Hill West. The electrification will be extended to Neville Hill East which is the boundary with Project E3.

9.1.6 The linespeed increase and electrification requires new signalling assets to be installed through this project area. As a part of these signalling upgrades, opportunity is taken to modernise the signalling interlocking system to a new computer based interlocking additionally this provides a more resilient system

E3/4 - Neville Hill East to Church Fenton

- 9.1.7 To achieve the required journey time improvements, line speed is increased from 80 mph to 100 mph between Neville Hill East and Thorpe Park. Line speeds are then maintained as existing at 90 mph. Track renewals will be required in the areas of increased line speeds, and a degree of re-railing will be required in the areas of unchanged line speed.
- 9.1.8 New electrification will continue from Neville Hill East and tie into the existing electrification at the 10 Mile Post at Church Fenton. There will be new track sectioning cabins at Micklefield Junction and Church Fenton.
- 9.1.9 The linespeed increase requires new signalling assets to be installed through this project area. As a part of these signalling upgrades, opportunity is taken to modernise the signalling interlocking system to a new computer-based interlocking additionally this provides a more resilient system.
- 9.1.10 The new Signalling System will require an upgraded telecommunications system and new Low Voltage Power.
- 9.1.11 8 Level Crossings are to be closed as part of the works, of which 5 are included within the Order Scheme. Barrowby Lane, Barrowby Foot, Peckfield, Highroyds Wood and Garforth Moor.
- 9.1.12 There are a number of structures that require renewal due to being either under strength or life expired or too low to allow for the installation of OLE with the required track clearance. 4 are included in the Order: Ridge Road, Crawshaw Woods, Austhorpe Lane and Brady Farm. Brady Farm Bridge is to be demolished.

9.2 Scheme Delivery Timescales and Construction Overview

Delivery Programme

- 9.2.1 Some of the elements of the Order Scheme are associated with a proposed interim timetable, with improved journey times and service frequency, which is proposed for December 2025.
- 9.2.2 These are Barrowby Lane and Barrowby Foot Level Crossings, Peckfield Level Crossing, Highroyds Wood Level Crossing and Garforth Moor Level Crossing.
- 9.2.3 All must be delivered if the proposed electrification between Neville Hill West and Church Fenton and the 2028 timetable changes are to go ahead.

Construction Strategy

- 9.2.4 There are two compounds which will be classed as major construction compounds to provide administration and construction support for the E2 to E4 Project works, at Marsh Lane and Red Lane respectively. These are located within the railway boundary, on land already owned by Network Rail and not included in the Order Scheme.
- 9.2.5 Marsh Lane compound will be situated on former railway sidings, owned by Network Rail and leased to Tarmac. Agreement has been reached for the Network Rail to take over this area of land for use as a compound for the duration for this programme. This compound will be the primary compound for the renewal of Marsh Lane bridge and Kirkgate Viaduct. This compound will provide a location where the proposed bridges are constructed and transported to site , and the existing bridges taken to, to be dismantled.
- 9.2.6 Where possible materials will be brought to site by rail, this will include items such as ballast with other materials being brought into compounds by rail and/or road.
- 9.2.7 Red Lane compound is located at former railway sidings at Neville Hill, previously leased to a freight operating company. It will be required to provide administration, welfare and construction support for the renewal of the bridge at Osmondthorpe Lane.
- 9.2.8 Both these compounds are required for the enabling works and the main works Spring 2027, where lay down areas will be provided for materials, material storage, welfare for operatives and offices.
- 9.2.9 In addition to these two main compounds, associated with key bridge works, Network Rail proposes to provide a number of smaller compounds listed below to enable it to carry out the wider track renewal electrification works within the E2-E4 Project Area as well as specific works associated with works to other bridges.
- 9.2.10 In terms of the temporary compounds included in the Order Scheme:
- Wykebeck Avenue Temporary Compound is required for the Neville Hill works. It will provide storage and delivery for materials, additionally there will be a Road Rail Access Point.
 - Manston Lane Compound is required for the Electrification works. It will provide storage and delivery for materials, additionally there will be a Road Rail Access Point.

- Brady Farm Temporary Compound is required for the Electrification works as well as the works to demolish Brady Farm Bridge. It will provide storage and delivery for materials, additionally there will be a Road Rail Access Point.
- Phoenix Avenue Temporary Compound is required for the Electrification works. It will provide storage and delivery for materials, additionally there will be a Road Rail Access Point.
- A series of smaller bridge replacement compounds are required adjacent to bridges to facilitate construction works at: Kirkgate Viaduct Temporary Compound; Marsh Lane Viaduct Temporary Compound; Osmondthorpe Lane; Austhorpe Lane Overbridge; Crawshaw Woods Bridge Raising; and A656 Ridge Road Replacement.

- 9.2.11 Sites will be suitably fenced/hoarded and will have access control in place. For the larger compounds 24/7 manned security is envisaged to be in place. For the satellite compounds 24/7 staffing will be targeted around high-risk activities and an assessment of the trespass and vandalism risks.
- 9.2.12 Where reasonably practicable works will be undertaken from the railway corridor. Much of the rail systems work such as track, track drainage, signalling and overhead electrification can be readily undertaken in this manner to minimise the impact of the scheme on the highway network and to our neighbours.
- 9.2.13 Where rail-based delivery is not reasonably practicable the highway network will be used. Public transport for the workforce and staff will be promoted as far as possible with shuttle bus services provided from key location to minimise additional traffic placed onto the highway. Left turns into and out of site compounds will be used as far as is possible to reduce congestion at site entrances and exits.
- 9.2.14 Deliveries of material and equipment will be carefully planned and co-ordinated with local authorities to best balance off peak delivery, construction logistics and deliveries outside of normal working hours which can equally be disruptive to stakeholders.
- 9.2.15 Where movement of material by is required by road it will go via the SRN where practicable. Strategic geographical requirements for location of a series of Main Compounds along the route to allow rapid access by construction teams to short sections of track, to minimise nightly travel time and optimise delivery programme.

High level principles of railway access

- 9.2.16 A multi-discipline construction strategy has been carefully planned, which will progressively bring the new railway into use whilst maintaining, as far as reasonably practicable, passenger services and operational stations during the construction period.
- 9.2.17 This is a large and complex programme to deliver, and disruptive railway access is an inevitable consequence of the required works. Where passengers are affected by planned closures of the railway, a combination of alternate diversionary rail routes and replacement bus services is planned. The Proposed types of railway access methods are described below, and the specifics of the access requirements will to be agreed with the various rail train and freight operating companies.

Rules of the Route (RoR)

- 9.2.18 These are access windows planned into the normal operational timetable for the railway, usually for essential maintenance. They take the form of short midweek nights (typically 4 to 6 hours) and longer weekend nights. The amount of access depends on the operational requirements of the railway, between Leeds and York the local conditions are particularly restrictive, due to late night Transpennine airport services and freight services on the Calder Valley route.
- 9.2.19 Within the E2 project RoR access to the railway is severely restricted to only weekend opportunities of approximately 4 hours where no trains are running.
- 9.2.20 For this Scheme extensive use of RoR (and possibly some extended RoR) possessions is planned, especially where we must re-align (slew) existing tracks. These works will have no, or in the case of extended RoR, extremely limited effects on the travelling public.

Weekend Possessions

- 9.2.21 This is the most common form of disruptive access used in the rail industry and typically takes the form of 28-hour or 52-hour periods planned around an end of service on Friday or Saturday night through to early hours on a Monday morning.
- 9.2.22 Extended Weekend Possessions. Exceptionally some works require longer periods than a standard weekend possession and it is necessary to extend the required possession duration to 72-hour or 100-hour+ periods based around a single weekend of disruption.
- 9.2.23 Bank Holiday Possessions. There are some periods in the calendar where multiple bank holidays allow for extended possessions to be planned with

reduced disruption to the travelling public. Traditionally these are over the Christmas and Easter periods.

Closures (sometimes referred to as “Blockades”)

- 9.2.24 A closure is defined as period affecting multiple weekends over an extended duration. It is accepted that this may cause extensive disruption to the travelling public and consequentially a great deal of thought is given before employing this technique and in planning appropriate mitigation measures. It must however be recognised that this is a very efficient way of delivering major works and thereby maximising the use of taxpayer’s money.

Multi-Disciplinary Works Delivery

- 9.2.25 Construction activities on the railway require very careful co-ordination of a vast number of inter-related disciplines covering the wide range of traditional civil & building engineering works (such as bridges, earthworks, stations), track works and operating system works (such as signalling and telecommunications).

Civil & Building Engineering

- 9.2.26 In general, major civil and building works have been planned so they can be built off-line (i.e. clear of the operational railway). This includes many of the bridgeworks. Building off-line is safe, efficient and minimises the use of disruptive possessions.
- 9.2.27 OLE foundations and structures will typically be installed within RoR track access or within access opportunities where other works such as bridges or track are undertaking their works to reduce the impact on the railway and the travelling public.
- 9.2.28 The wiring commences once other disciplines are complete within the same window of access.

9.3 Kirkgate to Marsh Lane

Temporary compounds at Kirkgate Viaduct and Marsh Lane

- 9.3.1 The works to the two bridges in this area will be managed from the Marsh Lane main construction compound supported by the secondary compounds located next to the bridges referred to as Kirkgate Viaduct and Marsh Lane Viaduct. Those temporary compounds will provide local welfare, security, administration, construction management and for activities including but not limited to; installation of scaffold around the bridge, and the recovery of

signalling and telecommunication cables and the removal of the existing bridge and the installation of new bridge.

- 9.3.2 The bridges at Kirkgate and Marsh Lane are planned to be renewed over the 2027 Christmas period within a 200 hour disruptive possession of the railway. The temporary compounds for these bridges will be in place approximately two months in advance and two months beyond the proposed main construction dates.
- 9.3.3 Track renewals within this project area will be undertaken whilst the bridge reconstruction is being undertaken.
- 9.3.4 The renewal of bridges at Marsh Lane and Kirkgate Viaduct planned for Christmas 2027 is a highly complex undertaking involving numerous railway disciplines. This will involve driving both existing bridge decks out from their current position by use of a Self-Propelled Modular Transporter (SPMT) and driven by the highway network into Marsh Lane compound for safe demolition.
- 9.3.5 The new bridge decks will be constructed offline within Marsh Lane compound and driven into position by SPMT.
- 9.3.6 The size and complexity of the structures dictates the duration required to undertake the works and in particular the duration where no trains can run. This task is planned to be undertaken over Christmas 2027 within a 200 hour disruptive possession of the railway. This is considered to be the most effective way to deliver the structures in one 'hit' rather than undertaking separately which would cause disruption for a time cumulatively greater than 200 hours.
- 9.3.7 Full highway and footway closures will be required for the removal and installation of both bridges. Agreement of suitable advance notices and diversions will be agreed with Leeds City Council and other stakeholders.

Penny Pocket Park works (delivery from railway only)

- 9.3.8 A new signal gantry and new lock out devices required as part of the signalling upgrades are to be constructed on land outside the railway boundary.
- 9.3.9 Land outside the railway boundary has been identified to locate new assets. These new assets (Lock Out Devices) are required to provide safe access for railway operatives or train personnel.

- 9.3.10 Lock out devices are needed as access along Marsh Lane Viaduct is difficult to achieve due to the narrow corridor. The provision of these new assets allows for maintenance/inspections to be undertaken safely.
- 9.3.11 The positioning of these assets has to achieve a minimum clearance from the running lines, which in this narrow corridor is outside the railway boundary.
- 9.3.12 The signal gantry will be constructed from the railway in a disruptive possession Christmas 2026 possession. The lockout devices will be constructed from the railway in RoR access This work is expected to be undertaken August 2026.

Neville Hill Access

- 9.3.13 The introduction of a new railway asset within the existing Red Lane car park and the proposed track alignment effectively cuts off an existing access route to the railway but is not included within the Order Scheme.
- 9.3.14 Access is a key requirement for the project to ensure maintenance teams can quickly and safely access the railway to undertake planned or emergency maintenance works.
- 9.3.15 The new access road from Newmarket Approach provides construction access during the works and access for Network Rail and other stakeholders once the works have been completed.

Osmondthorpe Lane compound (for Osmondthorpe lane bridge works)

- 9.3.16 A secondary compound is required in close proximity to this bridge to provide local welfare, security, administration and construction management. This compound will also be required to site a crane to facilitate the installation of bridge.
- 9.3.17 On site works are currently scheduled between August 2024 and December 2028.

Compounds at Wykebeck Ave, Manston Lane

- 9.3.18 Compounds are a key requirement for the Project, they are strategically located and will provide access to the railway via new Road Rail Access Points. Compounds will facilitate track renewals and electrification, they will provide welfare facilities for the teams as well as storage for materials.

Main Compound at Phoenix Ave

- 9.3.19 This compound is a key requirement for the Scheme, is strategically located and it provides access to the railway via a Road Rail Access Points. The compound will facilitate track renewals, electrification and access for the new Track Sectioning Cabin and the works to close Peckfield Level Crossing. There will be welfare facilities for the teams as well as storage for materials.

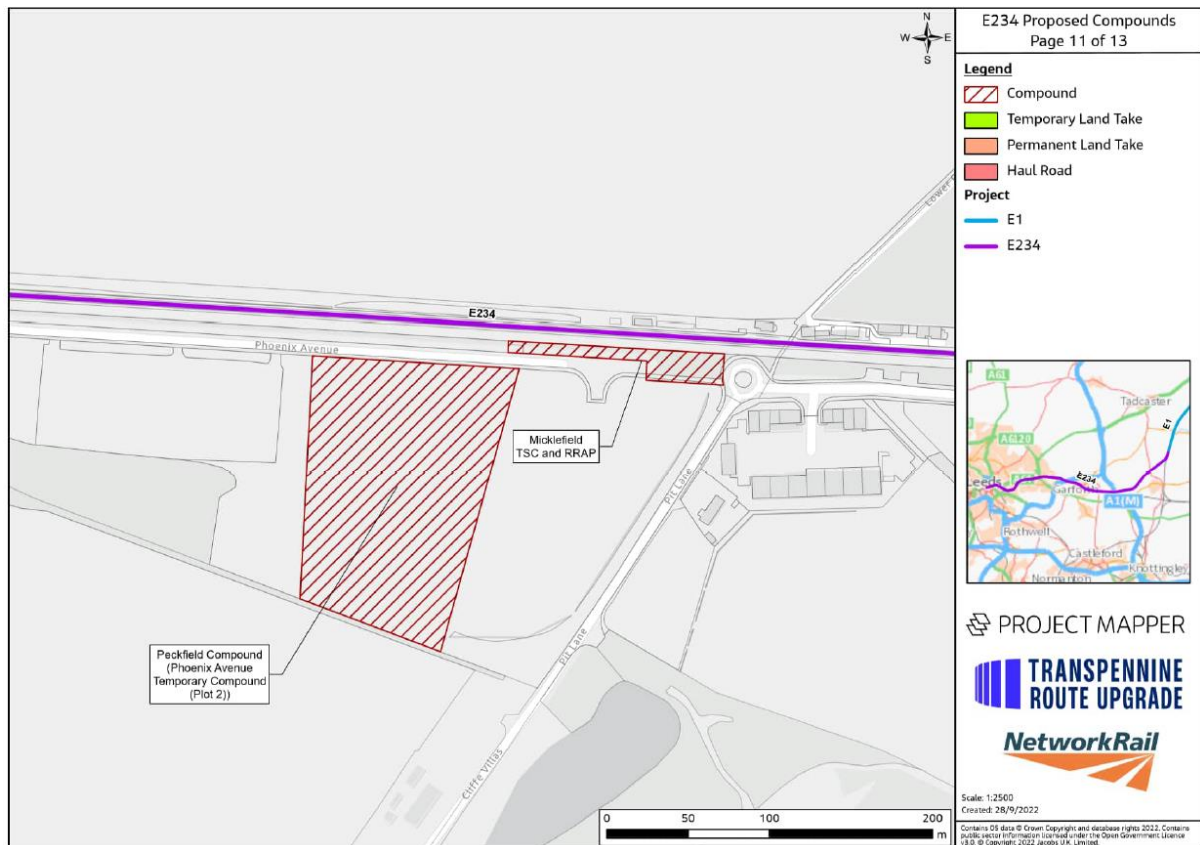


Figure 20a - E4 Main Compound at Phoenix Ave

- 9.3.20 On site works are currently scheduled between August 2024 and April 2028.

Brady Farm Temporary Compound required for Bridge Demolition

- 9.3.21 The temporary compound at Brady Farm is required for the demolition of Brady Farm Bridge. The stone from the bridge will be kept and repurposed on other bridges within the scheme.

- 9.3.22 On site works are currently scheduled between April 2025 and December 2028.

Replacement Austhorpe Lane Bridge and Gas Main Diversion

Austhorpe Lane Bridge

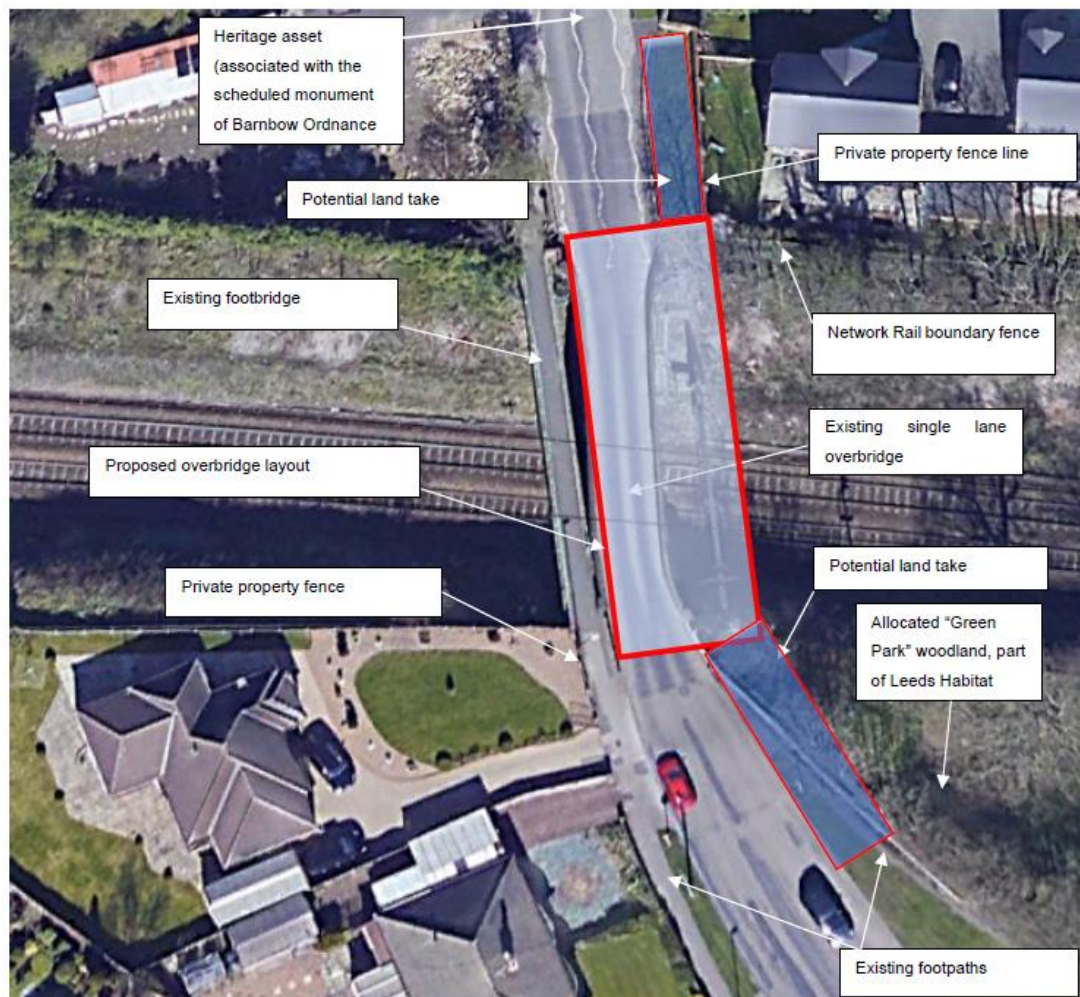
- 9.3.23 Austhorpe Lane overbridge is situated over the Neville Hill East to Hull line on Austhorpe Lane in Cross Gates, Leeds. This structure requires renewing due to the fact it is too low for electrification.
- 9.3.24 The overbridge carries a single public carriageway, Austhorpe Lane, which narrows to one lane over the bridge. There is no footpath on this overbridge. Pedestrians are directed via footbridge on the West side of the bridge. Statutory undertakers records show there is street lighting and three water pipes beneath the carriageway. an existing gas main and an existing water main beneath the carriageway.
- 9.3.25 The replacement bridge deck has been designed to house the existing LV and water services. The utility providers plan to undertake 2 stage diversions i.e. onto temporary scaffold service bridge during construction, then reinstated into the new bridge deck.

Austhorpe Lane Gas Main Diversion

- 9.3.26 Adjacent to Austhorpe Lane Bridge there is an HP Gas Main pipe, bridge structure (Austhorpe Lane Gas Main Pipe Bridge) that is being diverted by Northern Gas Networks (NGN) via a micro-tunnel under the railway. These works are being designed and managed by NGN.

Site Specific Constraints

- 9.3.27 The site is considerably constrained from a build point of view, with residential properties in close proximity to the bridge, a new housing development in the northeast corner and mature development of semi bungalows in the southwest. In the northwest corner there are two small holdings and in the southeast corner there is an allocated "Green Park" area.
- 9.3.28 Proposed areas of temporary land take for the construction compound have been designed to minimise impact on this woodland.
- 9.3.29 Austhorpe Lane provides access routes to three local schools, therefore the full closure of the road for the main construction works has been programmed out with term time to minimise the disruption to local traffic and access routes.



Compounds and Construction Access

- 9.3.30 The works will predominantly be run from a proposed temporary compound located in the south-east corner of the bridge. The below image highlights the proposed temporary and permanent land take for the construction of Austhorpe Lane.

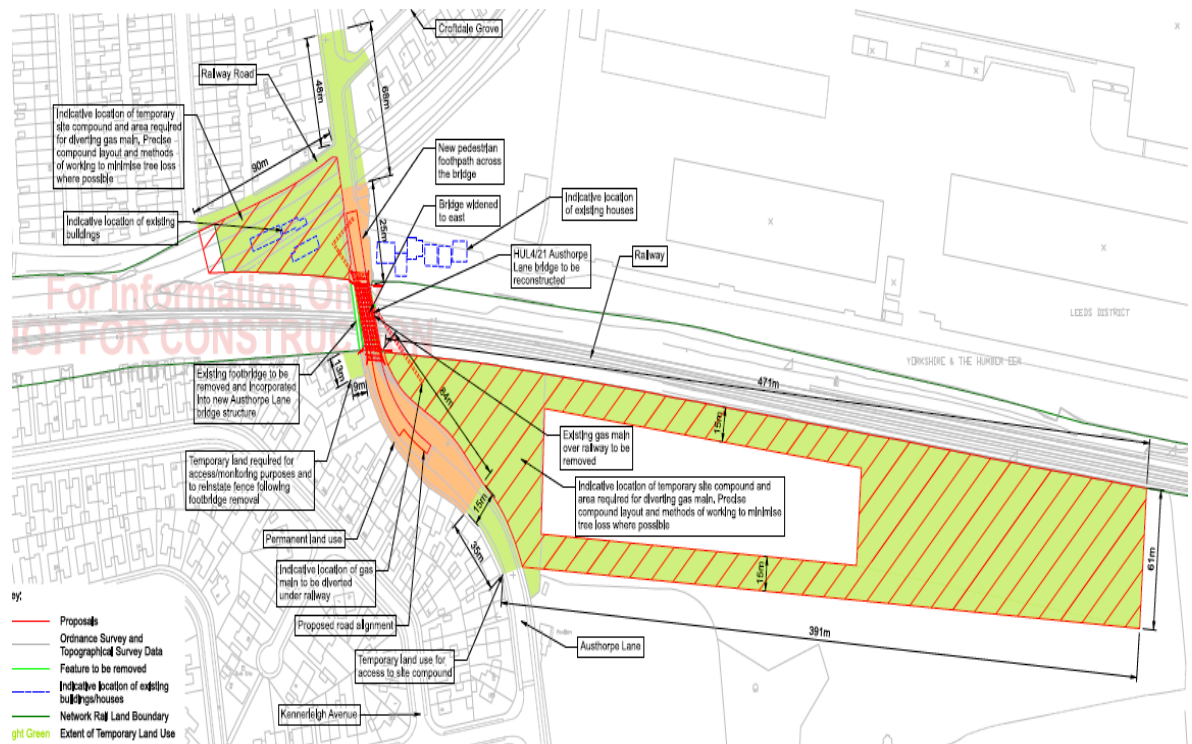


Figure 20b – Compound and Construction access

- 9.3.31 Deliveries from the north to the compound northwest of the bridge will be managed through lane/road closures. Access will be via Manston Lane which links to the A1M via the new East of Leeds Orbital Road. Very minor disruption is expected on the local road networks.
- 9.3.32 Deliveries to the main compound to be situated in the southeast corner of the bridge will be via a newly made access through an existing opening in the hedgerow to the rear of the woodland area.
- 9.3.33 It is envisaged that the majority of preparation works including service diversions will be managed through lane closures and night time closures, with full road closure being restricted to the main reconstruction works, which as currently planned are over summer out of term time.
- 9.3.34 Pedestrian/cycle access can be maintained throughout the construction with the exception of some night time closures and short-term closures when construction operations require exclusion zones for safety reasons.
- 9.3.35 Construction is currently scheduled for January 2025 – January 2026 for the NGN Gas Main Diversion and the bridge works currently scheduled for January 2026 – January 2027.

9.4 Works to Crashaw Woods Bridge

- 9.4.1 Crawshaw Woods Bridge is a single span, small farm accommodation overbridge. This structure requires renewing due to the fact it is too low for electrification.
- 9.4.2 The existing route over the structure is a Public Right of Way.



Figure 21 - Existing cast iron arch structure



Figure 22 - Visualisation of the proposed bridge

Project Proposal

- 9.4.3 Following the optioneering and consultation with Historic England and Leeds City Council Conservation Team, a refurbishment and lifting of the cast iron arches and overslabbing deck was the agreed preferred option to develop.
- 9.4.4 Once removed the cast iron arches will be refurbished whilst modifications are carried out to the existing abutments and approach ramps to facilitate their reinstatement at a higher level, suitable for electrification.

Site Specific Constraints

- 9.4.5 Crawshaw Woods Bridge is located within arable land to the east of the current Thorpe Park development. As well as the heritage significance of the structure, some environmental constraints have been identified and taken into consideration in the planning of the works.
- 9.4.6 The structure also supports a public right of way which will require temporary closure/diversion for the duration of the reconstruction works.

Compounds and Construction Access

- 9.4.7 The works will predominantly be run from a proposed temporary compound for the TRU works, located off Manston Lane and from smaller bridge replacement works compounds either side of the bridge.

Construction Sequencing

- 9.4.8 Before any works start on the bridge the bridge is closed to the landowner and general public. The bridge will then be carefully dismantled.
- 9.4.9 To remove the existing 1940's timber deck, B.S.B girders and parapets to enable the removal of the cast iron superstructure.
- 9.4.10 The stone masonry abutments will be taken down and re-used. The remaining part of abutments and foundations would be retained.
- 9.4.11 The refurbished cast iron arches will be reassembled and lifted back in using the lifting frame onto the modified springer level.
- 9.4.12 Following reinstallation of the cast iron arches, the new over-spanning concrete filler beam deck will be installed.
- 9.4.13 New parapet railings will be installed and surfacing work completed prior to re-opening of the PRow.

9.4.14 Works to reinstate temporary land take areas will be completed and handed back to the respective landowner.

9.4.15 On site works are currently scheduled between July 2025 and September 2026.

9.5 New Barrowby Land Bridge

9.5.1 This bridge is to be provided to facilitate the closure of two public level crossings – Barrowby Lane Bridleway crossing and Barrowby Foot footpath crossing. The proposal is to construct a new bridleway bridge with ramps and steps to provide continued public access across the railway whilst enhancing public safety by closing 2 level crossings.

9.5.2 The new Barrowby Lane Bridge scheme consists of the following:

- Construction of the new Barrowby Lane Bridge – a ramped and stepped structure to replace the existing Barrowby Lane bridleway crossing and the existing Barrowby Foot footpath crossing – located approximately 100m to the west of the existing bridleway crossing;
- Creation of a new bridleway PRow from the existing bridleway network to the south of the railway, over the new ramps and bridge to join the existing bridleway network to the north of the railway;
- Creation of a new footpath PRow over the steps and bridge;
- Permanent acquisition of land to install the new Barrowby Lane Bridge;
- Temporary acquisition of land to set up the Barrowby Lane Bridge construction compound, located to the southwest of the new Barrowby Lane Bridge, and construction access for bridge installation to the north and south of the railway;
- Creation of a new bridleway PRow on Nanny Goat Lane between the access points to the Barrowby Lane Level Crossing and the Barrowby Foot Level Crossing, and the new bridge;
- Replacement of two field gates on Nanny Goat Lane;
- Closure of Barrowby Foot Level Crossing and the extinguishment of the Public Right of Way (Definitive Footpath Garforth 6);
- Closure of Barrowby Lane Level Crossing and the extinguishment of the Public Right of Way (Definitive Bridleway Austhorpe 9);

The Network Rail (Leeds to Micklefield Enhancements) Order
Statement of Case NR19
17 November 2023

- Temporary access requirements from Barwick Road to the new Barrowby Lane Bridge;
- Land for landscape and ecological mitigation planting.

9.5.3 Figure 23 shows the proposed location of the new bridleway bridge and the proposed construction compound located in land to the south west of the new structure.

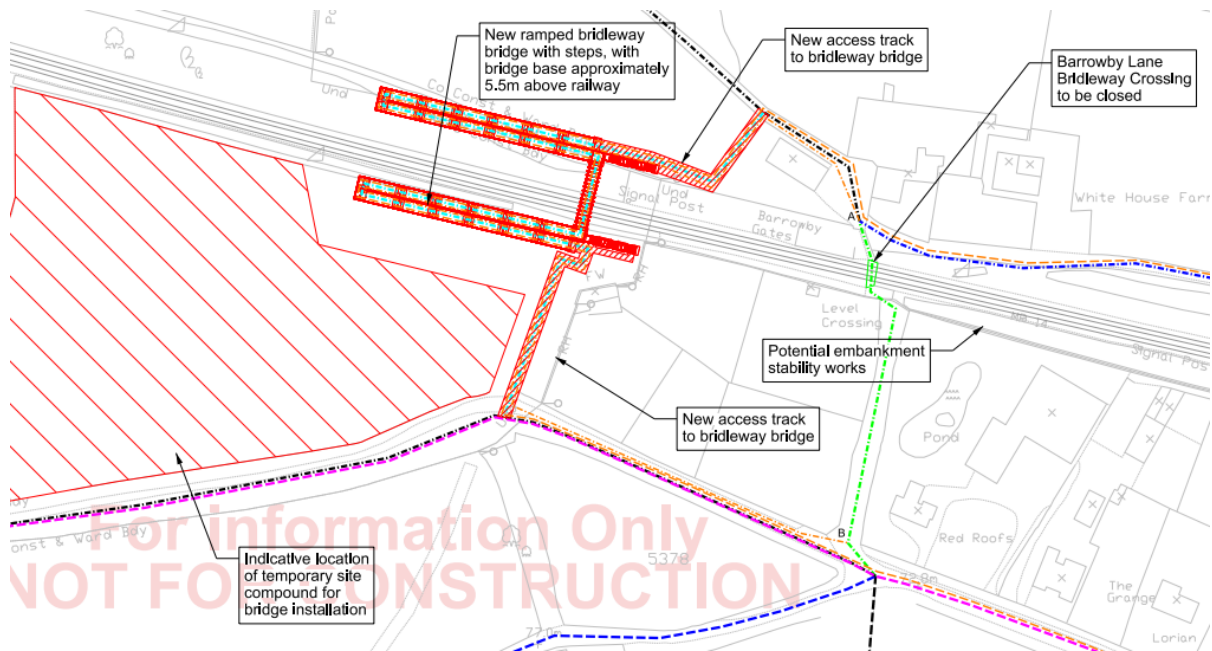


Figure 23 - Proposed ramped and stepped bridleway bridge structure, construction compound and existing level crossing locations

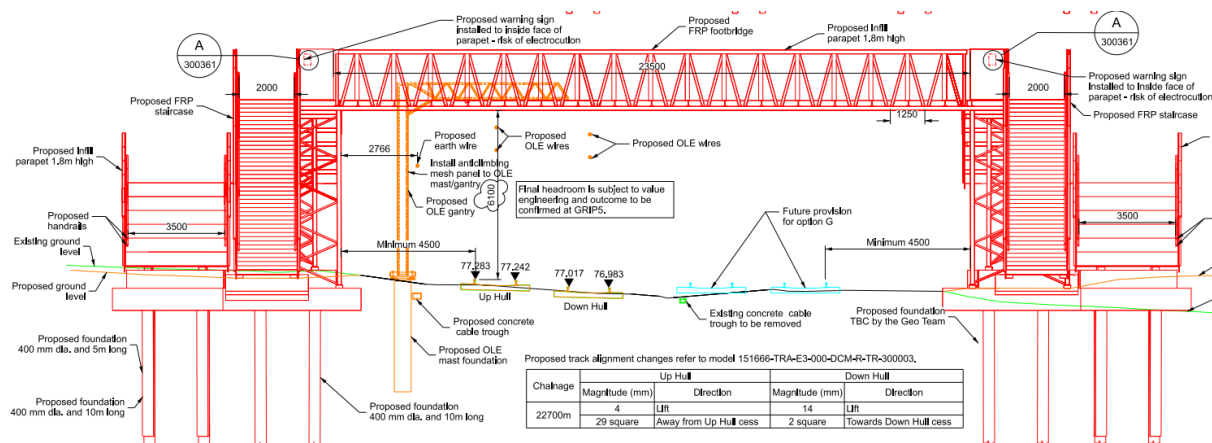


Figure 24 - Proposed ramped and stepped bridleway bridge general arrangement drawing



Figure 25 - Visualisation of ramped and stepped brideway bridge



Figure 26 - Example of steel ramped and stepped brideway bridge

Compounds and Construction Access

- 9.5.4 The Barrowby Lane Bridge construction compound will accommodate staff welfare facilities, parking as well as plant and material storage space.
- 9.5.5 While no direct road closures are anticipated to undertake the proposed works, additional traffic management may be required for delivery/transportation of large bridge elements under abnormal load

conditions. These will be coordinated with the local highway authority to minimise disruption to the community and surrounding road network.

- 9.5.6 The planning and delivery of these abnormal loads will be managed by the logistics team who will ensure that any disruption is discussed through the Traffic Liaison Group (TLG) in which the local authorities, emergency services and local communities will be made aware.
- 9.5.7 All abnormal loads will be notified to relevant authorities. Site visits with the highway authority may need to be carried out along with the local police for safety reasons.
- 9.5.8 The compound itself will be accessed via Barrowby Lane – the compound will provide access to the south side of the railway to construct the bridge. The north bridge construction will be accessed via Nanny Goat Lane.
- 9.5.9 Road Rail Vehicle (RRV) plant will access the railway via a temporary Road Rail Access Point (RRAP) which is to be installed at the compound location.

Construction Sequence

- 9.5.10 As the new structure is located in an area of shallow mine working, ground treatment will be required under the footprint of the new concrete foundations to ensure stability of the new structure.
- 9.5.11 In order to facilitate the construction of the new bridleway bridge, the following works will be required:
- Install compound and new temporary track access RRAP to both sides of the footbridge site;
 - Undertake mine working groundworks and drainage required for the bridge;
 - Set out and excavate the foundations and ground beams;
 - Pour and install concrete foundations;
 - Lay drainage runs and gullies;
 - Erect bridge structure
 - Remove any temporary works including temporary fencing and RRAP;
 - Complete new footpaths and groundworks around the new footbridge;

- Remove main compound and hard standing areas on both sides and make good the ground to suit the landowner;
- Following completion and commissioning of the new bridge and access tracks, the bridge will be opened to allow the 2no. level crossings to be physically closed and the existing crossing decks/signage removed.

Delivery Timescales

- 9.5.12 The Barrowby Lane Bridge Compound is expected to be required from October 2024 to July 2025.
- 9.5.13 The duration of construction activity for the new Barrowby Lane Bridge is anticipated to be from November 2024 to June 2025.
- 9.5.14 Works will be carried out during daylight hours, however night shift working will be required to lift in the bridge columns, ramps, landings, steps and deck as these activities involve lifting operations close to/over the railway and will need to be undertaken in non-operational periods of the railway i.e. night time closures.

9.6 Ridge Road Bridge and Gas Main Works Diversion

- 9.6.1 Ridge Road Overbridge is situated over the Neville Hill East to Hull line between Micklefield and East Garforth stations. This structure requires renewing due to the fact it is too low for electrification.
- 9.6.2 The structure comprises a single span masonry arch structure with masonry parapets. The substructure consists of stone abutments and wingwalls (see Figure 27 below).
- 9.6.3 The overbridge carries a single carriageway public highway (A656 Ridge Road) with varying width footpaths over the two existing railway lines.
- 9.6.4 Approximately 15m to the east side of the existing bridge, there is a 625mm diameter gas pipe structure which crosses over the railway lines.



Figure 27 - Ridge Road Bridge

Project Proposals

- 9.6.5 Ridge Road bridge is to be demolished and reconstructed.
- 9.6.6 The new structure has been designed to replace the existing bridge with a “feature” structure. It will maintain an arched feature, albeit a modern one, with the aim of retaining the thematic arch structures across the TRU Route.



Figure 28 - New Design for Ridge Road Bridge

Ridge Road Gas Main Diversion

- 9.6.7 The HP Gas Main pipe bridge structure that is being diverted by Northern Gas Networks (NGN) via a micro-tunnel under the railway. These works are being designed and managed by NGN.

Site Specific Constraints

- 9.6.8 The western compound is located to the south of the Selby railway line and benefits from an existing dropped access to the east via A656 Ridge Road, which appears to have previously facilitated agricultural activities.
- 9.6.9 The eastern compound benefits from an existing gated access to the north via Church Lane, which has also previously facilitated agricultural activities. Church Lane runs on roughly on an east-west alignment between A656 Ridge Road and Great North Road, providing access to the northern extent of Micklefield.

Compounds and Construction Access

- 9.6.10 There are three designated areas that are going to be used for these bridge works:
- a) North East Corner (as detailed in the diagram below)
 - b) South West Corner (as detailed in the diagram below)
 - c) North West Corner (as detailed in the diagram below)
- 9.6.11 Temporary haul roads connecting these compounds to Ridge Road will be built to allow easy safe access to the storage and compound areas.
- 9.6.12 The removal/diversion of the high pressure gas pipe will be done from the compounds in the NW and NE. These works will be done in advance of the bridge reconstruction.
- 9.6.13 The works have been programmed to avoid as much disruption as possible, so the bridge reconstruction will start as soon as the gas pipe diversionary works have been completed.

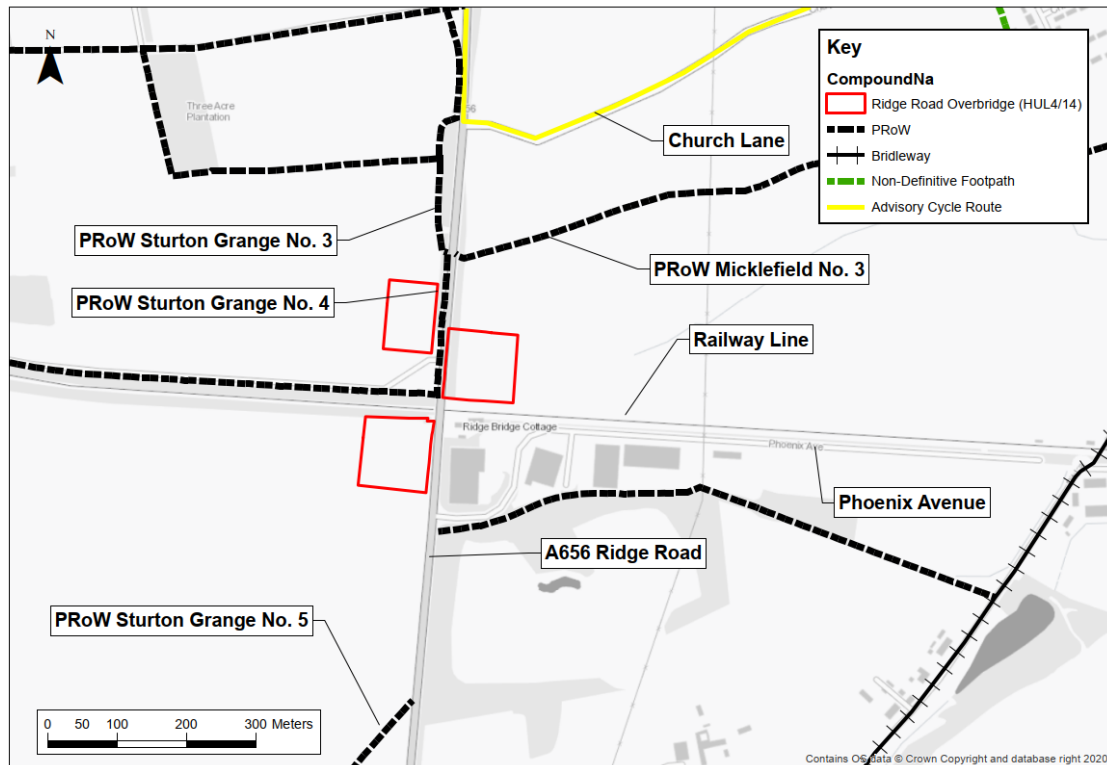


Figure 29 – Location of compounds

Construction Sequencing

- 9.6.14 Prior to bridge demolition and re-construction, the NGN gas main will be diverted via a bored tunnel.
- 9.6.15 The proposed full bridge reconstruction option would involve removal of the existing arch superstructure and construction of a new arch bridge with new substructure and new foundation.
- 9.6.16 The parapets are to be made up from reclaimed stone from Ridge Road Bridge, Brady Farm Overbridge and Austhorpe Lane Bridge, maintaining the curved profile of the existing, with the addition of central reinforcement installed through cored holes in the reclaimed stone to improve containment capacity.
- 9.6.17 A temporary services bridge will be installed and the existing water and LV services under the carriageway will be diverted temporarily out of the bridge deck.
- 9.6.18 A large mobile crane will be used to lift into position the new bridge deck in braced pairs during a second 29hr weekend possession of the railway.
- 9.6.19 All bridge deck works will be undertaken from behind solid hoardings above the rail infrastructure.

- 9.6.20 Construction is currently scheduled for March 2025 – January 2026 NGN Gas Main Diversion, followed by the bridge works currently scheduled for January 2026 – April 2028.

9.7 Peckfield Level Crossing Closure

- 9.7.1 The Order includes the powers which would enable the delivery of two alternative options for the replacement of the PROW which would be extinguished over the level crossing: a new footway or bridleway (on the same alignment).

- 9.7.2 The Peckfield Level Crossing closure scheme would consist of the following:

- The closure of Peckfield Bridleway Crossing and extinguishment of the existing PROW over the crossing;
- The creation of a new footpath or bridleway to the north of the railway between Lower Peckfield Lane and Great North Road to provide a footpath or bridleway link from the PROW (Definitive Bridleway Micklefield 8) on Lower Peckfield Lane. It would also provide pedestrian access from the residential properties north of Peckfield Level Crossing to Micklefield itself;
- Highway works to Lower Peckfield Lane to the north of the railway to upgrade (including three passing points) and to provide a small car parking area/turning area for residents of the railway properties;
- The existing section of bridleway between the level crossing and Great North Road to the north of the railway, on Lower Peckfield Lane, will remain and is not to be downgraded to a Footpath on either option;
- The existing section of bridleway between the level crossing and the A63 to the south of the railway, on Pit Lane, will remain and is not to be downgraded to a Footpath on either option.

- 9.7.3 Figure 30 shows the proposed location of the new footpath/bridleway.



Figure 30 - Proposed footpath (or bridleway) between Lower Peckfield Lane and Great North Road, upgrade to Lower Peckfield Lane, parking/turning area and existing level crossing locations

Compounds and Construction Access

- 9.7.4 The Phoenix Avenue construction compound will support works route wide on TRU as well as the works associated with the Peckfield Level Crossing closure scheme.
- 9.7.5 The Phoenix Avenue construction compound will accommodate staff welfare facilities, parking as well as plant and material storage space and will be accessed via Phoenix Avenue itself. In this area there is a well-documented existence of bell pits from historical mining which can lead to soil instability. As the compound is temporary, the layout will adhere to any exclusion zones identified rather than undertaking expensive soil stabilisation works.

- 9.7.6 While no direct road closures are anticipated to undertake the proposed works associated with Peckfield Level Crossing, additional traffic management may be required for highway improvements. These will be coordinated with the local highway authority to minimise disruption to the community and surrounding road network.

Construction Sequence

- 9.7.7 In order to facilitate the construction of the scheme, the following works will be required:
- Upgrade existing access road on Lower Peckfield Lane, including provision of passing places and temporary works hard standing area that will eventually become a new parking area/turning area for residents;
 - Construct the new footpath (or bridleway) through the Recreation Ground;
 - Remove any hard standing areas and make good ground to suit land owner;
 - Completion works to car parking area for the Peckfield Cottage residents on the north side;
 - The completion and commissioning of the scheme will allow the level crossing to be physically closed and the existing level crossing deck/signage removed.

Delivery Timescales

- 9.7.8 The Phoenix Avenue construction compound is expected to be required from April 2025 to 2028.
- 9.7.9 The duration of construction activity for the highway works to Lower Peckfield Lane, the parking area/turning area for residents and the new footpath (or bridleway) through the Recreation Ground is anticipated to be from March 2025 to June 2025.
- 9.7.10 Works are expected to be carried out during daylight hours. However night shift working may be required to remove the existing level crossing surface and will need to be undertaken in non-operational periods of the railway i.e. night time closures.

9.8 Installation of Track Sectioning Cabinet (TSC) at Micklefield

- 9.8.1 The scope of works requires a new installation of a Mid-Point Track Sectioning Cabin (MPTSC) modular building at Micklefield. The new steel

modular housing is to include 8 No. of 25kV switchboard panel to support the power supply upgrade works.

- 9.8.2 There is a MP gas main and an LV street lighting cable under the proposed footprint of the new TSC that will need to be diverted around the proposed building compound.
- 9.8.3 Figure 31, is an extract of the proposed general arrangement layout for the new TSC, indicating the presence of the LV and gas services. The options for diverting the services around the proposed compound are being reviewed by the service providers.

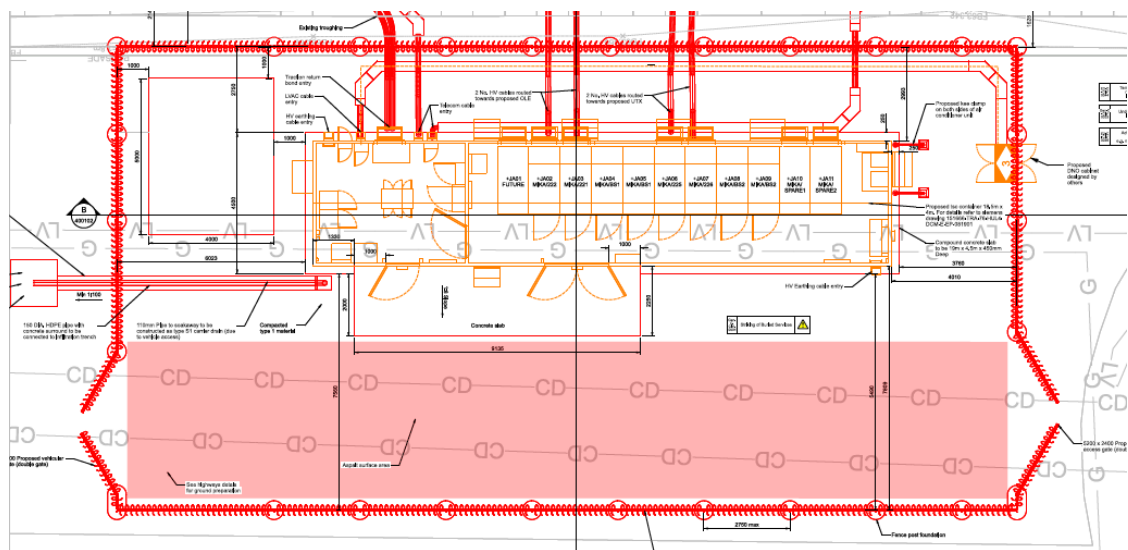


Figure 31 - Extract from Proposed GA for Micklefield TSC compound

Compounds and Construction Access

- 9.8.4 The works will predominantly be run from the Phoenix Avenue temporary compound.
- 9.8.5 The current build schedule is June 2027 – April 2028.
- 9.9 Highroyds Wood Level Crossing**
- 9.9.1 The draft Order provides for the permanent closure of Highroyds Wood Level Crossing, which has been temporarily closed by Network Rail on safety grounds, and to divert the PRoW to the east via a new path to an existing railway underpass.
- 9.9.2 This will involve the construction of footpaths to the south and north of the railway and enhancements to the underfoot conditions of the existing underpass.

9.9.3 The proposals at Highroyds Wood Level Crossing are shown on the schematic below.

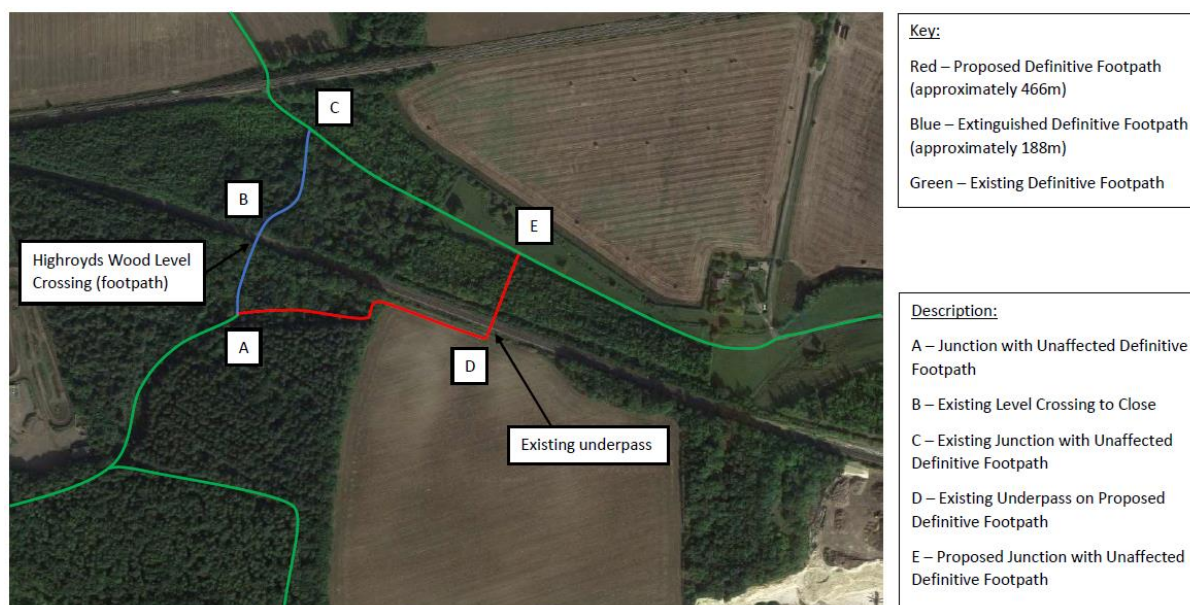


Figure 32 - Schematic of the Highroyds Wood Level Crossing proposals

9.9.4 The timescales of these works are yet to be confirmed.

9.5 Garforth Moor Level Crossing

9.5.1 The draft Order provides for the permanent closure of Garforth Moor Level Crossing, which has been temporarily closed by Network Rail on safety grounds, and to formalise the current access arrangements being used by the Garforth Bank Row Allotment holders to access the allotments from Barwick Road.

9.5.2 The proposals at Garforth Moor level Crossing are shown on the schematic below.



Figure 33 - Schematic of the Garforth Moor Level Crossing proposals

9.5.3 The timescales of the access track and parking area construction, are to be confirmed.

10. ENVIRONMENT EFFECTS

10.1 Introduction

- 10.1.1 This section of the Statement of Case (SoC) presents the key aspects of environmental design and construction management incorporated into the Scheme and outlines recommended mitigation measures to address the environmental effects as identified in the Environmental Report (**NR16**).

Environmental Impact Assessment Screening

- 10.1.2 The entire E2 to E4 Project (including the Order Scheme elements), was originally screened for an Environmental Impact Assessment (EIA) and submitted to LCC for a screening decision in February 2022 under the Town and Country Planning (Environmental Impact Assessment) Regulations 2017. Network Rail received a screening opinion from LCC in April 2022, confirming that an EIA was not required for the for the E2 to E4 Project. (**NR22**).
- 10.1.3 An Environmental Impact Assessment (EIA) screening decision was also sought from the Secretary of State in April 2023 for the Order Scheme under the Transport and Works (Applications and Objections Procedure) (England and Wales) Rules 2006. The Screening Decision letter dated 17 May 2023 (**NR10**) confirmed that the Order Scheme was not EIA development, and on that basis, a formal Environmental Statement was not submitted with the Order application. Network Rail has nevertheless submitted an Environmental Report with the application to examine the environmental effects of the Order Scheme. Section 10 provides more detail on the Environmental Report (**NR16**).
- 10.1.4 Network Rail noted the comments in the Screening Decision letter from the Secretary of State and incorporated reporting on those matters in the Environmental Report (**NR16**) that was submitted with the Order application. The relevant subjects are outlined in the SoC in section 10.3 Environmental Design and section 10.4 Construction Environmental Management.

The Environmental Report

- 10.1.5 The Environmental Report (**NR16**) comprises 3 volumes as follows:
- Volume 1: Main text
 - Volume 2: Figures (to support the main text)
 - Volume 3: Appendices (to support the main text)

10.1.6 The Environmental Report highlights the environmental effects of the Scheme and identifies the relevant mitigation to address the environmental effects and how this mitigation is secured either by way of:

- Commitments made in the TWAO submission via the Code of Construction Practice (CoCP) Part A (**NR17**);
- Draft planning conditions as part of the Deemed Planning Permission (DPP); or,
- Additional commitments made to Leeds City Council (LCC) in a Letter of Environmental Commitment.

10.2 Environmental Commitments

10.2.1 As described in section 1.3 of the ER (Scope of Environmental Report) the ER directly addresses and provides mitigation for the elements of the Scheme that require Deemed Planning Permission (DPP). The elements of the Scheme that require DPP are identified at paragraph 4.2.3 of this Statement of Case.

10.2.2 In addition, the ER also addresses and provides mitigation for temporary construction compound areas and accesses to those areas directly associated with the DPP elements, also described at paragraph 4.2.3 of this Statement of Case.

10.2.3 The ER does not consider the effects of certain additional elements either being carried out as permitted development or through planning permission obtained separately from the local planning authority. The additional elements are included in the Order to secure the powers to acquire or use land and are otherwise taken forward under Network Rail's permitted development rights. In these cases, as would be normal practice for works conducted under Network Rail's permitted development rights, these works will be managed in compliance with Network Rail's Contract Requirements – Environment (CR-E) (NR/ENV/015). CR-E requires that all contractors demonstrate compliance with environmental legislation and other Network Rail environmental requirements for works on Network Rail infrastructure.

10.2.4 However, Network Rail will make additional specific commitments relating to providing mitigation to LCC through a Letter of Environmental Commitment.

10.3 Environmental design

Mitigation Measures (general)

10.3.1 In accordance with the risk mitigation hierarchy, mitigation measures proposed through the ER prioritise avoiding risks, reducing risks, offsetting the impact and then compensation. Where possible, environmental effects have been avoided through embedded mitigation developed as part of the design of the Scheme. Examples of embedded mitigation include:

- retention and restoration of Crawshaw Woods Overbridge, thereby retaining the key historic element and enhancing heritage significance;
- provision of a new ramped bridleway bridge at Barrowby Lane, with integrated steps to provide pedestrians with a shorter access route across the railway line;
- reuse of materials to be reclaimed from the bridge to be demolished (Brady Farm Bridge) in the reconstruction of other bridges (The Replacement Austhorpe Lane Bridge, Crawshaw Woods Bridge and the Replacement Ridge Road) to minimise waste and maintain the aesthetic of the retained historic elements;
- avoiding works within Flood Zones 2 and 3 at Kirkgate to Marsh Lane Land;
- employing geogrid or similar materials for temporary construction compounds, to help avoid risk of impact to unknown archaeology and allow for permeability of the ground to be retained;
- colour and design of bridge structures will be sympathetic to the local context to minimise visual impact.
- Various design refinements to minimise the de-vegetation across the Scheme including:
 - Kirkgate to Marsh Lane Land: Boundary allows for design refinements to avoid tree loss along the embankments and minimise the area of land required outside the railway boundary during installation of the new railway assets, with works being undertaken from the railway line.
 - The Replacement Austhorpe Lane Bridge and Austhorpe Lane Gas Main Diversion: Design of Austhorpe Lane Southeast Compound to exclude a central area of land to reduce the loss of wet grassland and ephemeral water areas, and the extension of the compound into the

grassland area to the east of the woodland block to reduce priority woodland loss as far as practicable and to retain trees with bat roost suitability.

- Works to Raise Crawshaw Woods Bridge: Design of northern access track to avoid a mature hawthorn and design of southern access track to avoid damage to a pond and an existing hedgerow.
- The New Barrowby Lane Bridge: Extension of Barrowby Lane compound further west to enable two mature trees to be retained that have bat roost suitability, one of which is a veteran tree.
- Access routes and compounds amended to reduce tree and scrub loss along the embankments and along the edge of Ridge Road.
- The Peckfield Level Crossing Closure and the Micklefield Track Sectioning Cabinet (TSC): Micro-siting of passing places along Lower Peckfield Lane to avoid tree loss.

Landscaping

- 10.3.2 As defined mitigation for unavoidable vegetation loss, Network Rail proposes draft condition 5 Landscaping and Ecology, that is included in the DPP. The condition requires Network Rail to submit and gain approval from LCC for a Landscape and Ecological Management Plan (LEMP) that will be based on draft landscape plans included in the ER in Volume 2 Figures: Figures 8.5.1 to 8.5.6 Outline Landscape and Ecological Mitigation Proposals.
- 10.3.3 For specified compound areas and associated accesses within the Scheme that are not subject to DPP but are associated with the delivery of the works requiring DPP, Network Rail will submit to LCC and gain approval for Land Restoration Plans that will be secured through the agreement formed with LCC as a letter of environmental commitment. The land restoration plans will be based on those draft plans included in the ER for reference in Volume 2 Figures: Figures 8.6.1 to 8.6.5 Outline Draft Land Restoration Proposals.
- 10.3.4 For other land areas temporarily acquired through the Order that will be utilised for the wider TRU works that will also include servicing the Scheme works, the works carried out in these locations will comply with CR-E and land shall be returned to the landowner in its pre-works condition as far as practicable to the satisfaction of the relevant landowner.

Biodiversity Net Gain

- 10.3.5 Network Rail complies with the TRU commitment for Biodiversity Net Gain (BNG) across its schemes from Manchester Victoria to York. BNG is defined as replacing the value of all habitat lost through de-vegetation and then adding 10% enhancement in addition to that replaced.
- 10.3.6 The Request for Deemed Planning Permission (**NR12**) includes draft condition 10 Biodiversity Net Gain, that secures the delivery of this commitment for the DPP elements of the Order Scheme.
- 10.3.7 As noted in section 10.3.5, Network Rail is committed to BNG across the whole of TRU and can confirm that in addition to draft condition 10 securing BNG for the DPP elements of the Scheme, NR is committed to delivering BNG wherever there is de-vegetation on the Scheme, whether under DPP or Permitted Development. This commitment has been re-stated in the Letter of Environmental Commitments to be agreed with LCC.

10.4 Construction environmental management

Mitigation measures (general)

- 10.4.1 The Code of Construction Practice (CoCP) acts as an environmental management framework for construction. It sets out the relevant measures and standards to be followed throughout the construction period. The CoCP is split into two parts; Part A (**NR17**) sets out the principles for the Project Team to comply with and is included in the Order submission; Part B of the CoCP will provide the detail of the delivery of management controls on site and will include the following environmental management documents, as draft condition 6c that must be submitted to and approved by LCC, as included in the Request for Deemed Planning Permission (**NR12**):
- An external communications programme;
 - A pollution prevention and incident control plan;
 - A waste management and materials plan;
 - A nuisance management plan concerning dust, wheel wash measures, air pollution and temporary lighting;
 - A noise and vibration management plan including a construction methodology assessment; and
 - A demolition methodology statement for relevant buildings.

- 10.4.2 The CoCP Part A, as an environmental framework, will apply to all works on the Order scheme whether under DPP or Permitted Development.
- 10.4.3 The CoCP Part B is conditioned against the DPP elements of the Scheme. However, the CoCP Part B that is approved to discharge planning condition 6 conditions (i) to (vi) as listed in section 10.4.1, will be implemented across the entire Order Scheme, whether DPP or Permitted Development. This commitment will be secured in the letter of Environmental Commitment.
- 10.4.4 Network Rail will also require its contractors to comply with CR-E for all permitted development works. It should be noted that on its own CR-E includes a suite of environmental requirements that reflect the requirements of the CoCP Part B, specifically compliance with CR-E section 6 Environment Requirements.
- 10.4.5 In addition to the CoCP parts A and B that secure general environmental mitigation during construction works, the following draft conditions as submitted in the Request for Deemed Planning Permission (**NR12**), address other issue-specific elements during the construction period as follows:
- Condition 4: Landscaping and Ecology – Preliminary Works
 - This condition requires Network Rail to submit and acquire approval from LCC for a plan showing existing trees and tree features that will be protected in advance of any preliminary works.
 - In addition, Network Rail must seek approval from LCC for a plan showing how the ecological assets identified in the ER will be protected during the preliminary works before these works can commence.
 - Condition 7: Construction Traffic Management and Travel Plan
 - This condition requires Network Rail to submit and acquire approval from LCC for construction traffic management matters as outlined in the ER. The CTMP must include details for a travel plan for construction staff and details on temporary diversions of both highways and rights of way.
 - Condition 9: Archaeology
 - This condition requires Network Rail to submit and acquire approval from LCC for the construction methodology in considering potential effects on archaeological remains.

- Network Rail must then submit for approval to LCC, a written scheme of archaeological investigation (WSI) for areas identified as requiring a WSI in consideration of the submitted construction methodology.

10.5 Archaeology

Mitigation

Construction

- 10.5.1 Avoidance measures have been incorporated into construction design to remove impacts in specific areas, for example the looped gatehouse at Austhorpe Lane or gravestones at Penny Pocket Farm.
- 10.5.2 Within areas of temporary land use, depth of topsoil strip will be minimised by utilising a method of ground stabilisation by laying terram and geogrid protection. This will minimise the risk to below-ground impacts to unknown potential archaeological assets.
- 10.5.3 As outlined in section 10.4.5, condition 9 Archaeology secures the requirement for Network Rail to submit and acquire approval from LCC for the construction methodology in protecting unknown potential archaeological assets. In consultation with LCC, the defined construction methodology may then require a specific WSI to be completed for areas of the Scheme where unknown sub-surface assets may be present.

Operation

- 10.5.4 There are no operational effects on any potentially existing but unknown archaeology and no mitigation is proposed.

10.6 Ecology

Mitigation

Construction

- 10.6.1 The phase 1 habitat assessment and identification of risk from the Scheme during the construction phase results in standard mitigation measures as follows:
 - Lighting Measures: to avoid unnecessary lighting as far as is possible included in Section 3.4 of the CoCP Part A (**NR17**) and as part of the CoCP Part B Nuisance Management Plan, secured through planning condition);

- Noise Reduction: measures to reduce noise as far as is practicable (included in Section 8 of the CoCP Part A (**NR17**) and as part of the CoCP Part B Noise and Vibration Management Plan, secured through planning condition);
- Surface Water Drainage and Pollution Prevention: pollution control and incident response measures (included in Section 7 of the CoCP Part A (**NR17**) and as part of the CoCP Part B Pollution Prevention and Incident Control Plan secured through planning condition);
- Biosecurity Measures: specific and targeted measures to contain non-native invasive species and prevent spread as part of an Invasive Species Management Plan, supported by relevant Network Rail guidance notes and within the Landscape and Ecological Management Plan (LEMP), secured through planning condition 5 as detailed in Section 4.4 of the CoCP Part A (**NR17**).

10.6.2 There are identified impacts on specific species during the construction period that will be mitigated as follows:

- Bat Mitigation – Confirmed Bat Roost and suitable roosting features: securing appropriate bat mitigation licence(s), supported by a detailed mitigation strategy, as outlined in Section 4.3 of the CoCP Part A (**NR17**); and,
- Great Crested Newt Mitigation: working in accordance with Network Rail Great Crested Newt Organisational Licence and supporting measures, as outlined in Section 4.3 of the CoCP Part A (**NR17**).

10.6.3 Other good practice measures will be adopted for the Scheme as follows:

- Habitat Demarcation for Trees and Hedgerows: measures to protect retained trees and hedgerows as outlined in the Tree Protection Plan and LEMP;
- Scheme Clearance and Preparation: measures for pre-construction works including watching brief, suitable timing of works, and further pre-construction surveys;
- Breeding Bird Mitigation: measures for vegetation clearance, including avoidance during bird-nesting season, or nest checks and watching briefs by trained personnel, as outlined in Section 4.2 of the CoCP Part A (**NR17**); and,
- Ecology tool box talks: delivery of relevant information to site workers.

Operation

- 10.6.4 As outlined in section 10.3.1, layout design or embedded mitigation, avoids impact on the ecological resource where possible to do so. The detailed description of these avoidance measures can be reviewed in the ER in Volume 3 Appendices: Appendix 7: Section 4.1.
- 10.6.5 As described in section 10.3.5, temporary and permanent habitat loss will be mitigated by way of commitment to BNG, secured through draft planning condition 10: Biodiversity Net Gain. The Biodiversity Metric 3.0 will be utilised to measure habitat loss and gain and then develop biodiversity enhancement, habitat creation, and management commitments in consideration of the requirement to offset the loss and provide a 10% enhancement.

10.7 Landscape and Visual Amenity

Mitigation

Construction

- 10.7.1 Various measures to protect landscape features during the construction of the Scheme are detailed in the ER in Volume 3: Appendix 8: Section 6.1.7 and this includes:
- Positioning, size and maintenance of site hoarding and perimeter fencing;
 - Protecting existing valued trees and woodland adjacent to the Scheme in accordance with BS5837:2012 (specific protection details identified in Table 6.1 of Volume 3 Appendix 8);
 - Soil protection measures are implemented for temporarily occupied land; and,
 - Reduce unnecessary light spill as a requirement of CoCP Part B: (iv) Nuisance Management Plan.
- 10.7.2 The protection measures identified above in section 10.7.1 are secured thorough compliance with condition 4 Land and Ecology – Preliminary Works, and condition 5 Landscape and Ecology.

Operation

- 10.7.3 As outlined in sections 9.3 and 9.4, substantial optioneering has been undertaken to identify suitable designs for all the relevant work components of the Scheme to limit visual and landscape impacts and effects. This includes design of temporary and permanent works to limit loss of trees and

woodland. The full detail can be reviewed in the ER (**NR16**) in Volume 3 Appendices: Appendix 8: Section 6.

- 10.7.4 In considering the landscape effects of the Scheme in operation, the mitigation is secured via draft condition 5 Landscape and Ecology as described in section 10.3.2 with the requirement to produce a LEMP to be submitted to and approved by LCC. The LEMP will detail hard and soft landscaping works, compensate for the loss of mature vegetation, provide habitat connectivity and be designed to integrate the Scheme elements into the receiving landscape.

10.8 Arboriculture

Mitigation

Construction

- 10.8.1 The ER reports on Arboriculture in section 9 with reference made to Indicative Tree Protection plans in the ER in Volume 2 Figures: Figure 9.2.
- 10.8.2 The requirement to protect trees and woodland adjacent to the Scheme is secured through the implementation of condition 4 Land and Ecology – Preliminary Works, and condition 5 Landscape and Ecology, that must be submitted to and approved by LCC.

Operation

- 10.8.3 The embedded mitigation to eliminate the need to remove valued trees and woodland through Scheme design is described in section 10.3.1.

10.9 Noise and Vibration

Mitigation

Construction

- 10.9.1 Controls to minimise noise and vibration effects are secured through Condition 6 Code of Construction Practice Part B: (v) Noise and Vibration Management Plan (NVMP), that must be submitted to and approved by LCC.

10.9.2 Standard methods to reduce noise and vibration to the lowest practicable levels will be in compliance with BS5228 “Best Practicable Means” (BPM) and will include measures such as:

- Accurate and reliable advanced notice of specific works to residents that will also be detailed in the CoCP Part B: (i) External Communications Programme, that must be submitted to and approved by LCC;
- Modern plant is used and construction techniques consider BPM;
- Site hoardings are of solid design where benefit from noise reduction is established; and,
- Comprehensive site briefings ensure there is appropriate site behaviour in minimising any unnecessary noise.

10.9.3 The NVMP will state a requirement for Network Rail to prepare and submit a Section 61 application under the Control of Pollution Act 1974 to LCC for their agreement. The Section 61 assessment systematically predicts the noise that will occur during the works and then when any site specific mitigation is recommended beyond the standard mitigation identified above in section 10.9.2.

Operation

10.9.4 The design of the Micklefield TSC will be in compliance with Network Rail standard NR/SP/ELP/21030 and will include a prefabricated housing structure that will attenuate noise levels.

10.9.5 Further noise mitigation as may be required will be considered in the detailed design stage, for instance the possible requirement for additional solid housing and positioning the ventilation facing away from sensitive receptors.

10.10 Traffic and Transport

Mitigation

Construction

10.10.1 Construction traffic routing is described in detail in the ER in Volume 3 Appendices: Appendix 11.1: Section 3.4. The Request for Deemed Planning Permission: Condition 7 Traffic Management and Travel Plan that must be submitted to and approved by LCC, acts as the mechanism to secure the

mitigation and controls in considering traffic management during the construction stage. The CTMP shall include at minimum:

- Temporary and permanent road closures;
- Construction traffic routes, both local and from trunk roads;
- Any specified traffic restrictions;
- Any temporary traffic control measures;
- Monitoring construction HGV compliance with traffic routing;
- Site specific controls to manage nuisance; and
- Prohibition of parking along public roads.

10.10.2 A Highways Working Group that includes Network Rail and relevant project members meeting with LCC (Highways Authority) on a periodic basis to discuss and agree traffic provisions as stipulated in a future Order will be set up. The CTMP will reflect agreements made in the Highways Working Group forum.

10.10.3 The CTMP will include a Travel Plan to encourage sustainable transport for construction staff, both in getting to site and when moving round site locally.

10.10.4 The CTMP will also address the detail of level crossing closures, Public Right of Way (PRoW) and non-PRoW diversions as required for the Scheme.

Operation

10.10.5 There is no requirement for specified mitigation for the Scheme in operation.

10.11 Geoenvironment

Mitigation

Construction

10.11.1 The ER in general addresses geoenvironmental issues and recommends mitigation across the Scheme in Volume 1 Main text: Table 12.2. Main elements of mitigation during construction include:

- Acquisition of coal authority permit as may be applicable that will determine gas and groundwater sampling requirements during works;

- Dust generation suppression and protection of controlled waters will be controlled via Condition 6 CoCP Part B (iv) Nuisance Management Plan and (ii) Pollution Prevention and Incident Control Plan that must be submitted to and approved by LCC;
- Re-use of excavated material in accordance with the CL:AIRE Definition of Waste Development Industry Code of Practice;

10.11.2 The management of potentially contaminated land will be in accordance with standard Network Rail requirements as required for Permitted Development works. This requires compliance with CR-E and particularly section 6.2 of CR-E that requires an initial hazard review of each site that may or may not then result in a Preliminary Risk Assessment (PRA).

10.11.3 The PRA, if required, for example for piling activity at the Kirkgate to Marsh Lane Land, includes a conceptual site model that facilitates an evaluation of the risks associated with the pollution linkages.

10.11.4 If the PRA identifies potential risks, a Generic Quantitative Risk Assessment (GQRA) will be conducted on the relevant site. This assessment will identify options to remediate or otherwise prevent or minimise any potential effects.

10.11.5 This process will be confirmed in the letter of commitment that will be agreed with LCC and will include liaison with the relevant officer in the local authority at each stage described.

10.11.6 The letter of commitment will also detail the process to be undertaken should unexpected contamination be encountered on any site during construction works.

Operation

10.11.7 As identified above in section 10.11.2, should the process lead to an identification of specific ground conditions that has the potential to affect proposed infrastructure, for example in relation to the Micklefield TSC site, this may then require designs to be adjusted, for example using concrete and service pipes in the presence of any chemically aggressive ground conditions.

10.12 Water Environment

Mitigation

Construction

- 10.12.1 The CoCP Part A (**NR17**) identifies the mandatory protection measures that must be employed across the Scheme. This includes management of surface flooding during the construction phase during periods of heavy rainfall.
- 10.12.2 The relevant protection measures will be included in the Pollution Prevention and Incident Control Plan (PPICP) that is required by condition (CoCP Part B: (ii) PPICP, that must be submitted to and approved by LCC.

Operation

- 10.12.3 An outline drainage strategy will be prepared for the Micklefield TSC with discharge runoff from the Scheme area proposed to the existing track drainage and existing highway drainage. The outline drainage strategy will be submitted to LCC for approval.
- 10.12.4 There is no additional risk to flooding created by the Scheme design and therefore no applicable mitigation is proposed.

10.13 Agriculture

Mitigation

Construction

- 10.13.1 The mandatory measures to mitigate the impact on soil resources and ensure that land used temporarily will be returned to the landowner in the same condition as existing is set out in the CoCP Part A (**NR17**) in section 10.4. This requires that materials management during construction will include good practice measures for storage, handling and reinstatement of soils to avoid compaction and biodegradation of soils, and maintain their quality.
- 10.13.2 Further to the requirements set out in the CoCP Part A, the Request for Deemed Planning Permission (**NR12**) includes Condition CoCP Part B 6(iii) Waste Management Plan and Materials Plan, that must be submitted to and approved by LCC where all relevant information will be detailed.
- 10.13.3 In considering the requirement to restore farmland to its previous state, this will also be detailed in the LEMPs that are required through condition 5 Landscape and Ecology and will provide the detail of what has been

submitted in outline in the ER in Volume 2 Figures: Figures 8.5.1 to 8.5.6 Outline Landscape and Ecological Mitigation Proposals.

- 10.13.4 For areas of farmland occupied temporarily that are not subject to DPP, these will be restored to pre-works activity condition in accordance with ER Volume 2 Figures: Figures 8.6.1 to 8.6.5 Outline Draft land Restoration Proposals. The detail of these outline restoration proposals will be submitted to LCC for approval as will be required through the letter of environmental commitment.

Operation

- 10.13.5 There is no requirement for mitigation in considering Agriculture for the Scheme in operation.

10.14 Sustainability and Climate Change

Mitigation

Construction

- 10.14.1 The details contained in the TWAO application by default identify that the Scheme will be delivered sustainably in the construction stage. Reference can be made to the ER in Volume 3 Appendices: Appendix 15 Sustainability and Climate Change: Table 2.1. Table 2.1 examines the details of the Scheme as assessed by the Building Research Establishment Environmental Assessment Methodology (BREEAM) Infrastructure methodology.
- 10.14.2 The commitment to various environmental plans and required controls for the construction stage as outlined in sections 10.4 to 10.13 score well in the BREEAM assessment for a sustainable approach to construction activity in that works will be appropriately managed and impacts or effects reduced where feasible. Examples of this sustainable approach are taken from Volume 3 Appendices: Appendix 15 Sustainability and Climate Change: Table 3.1 and examples are listed here:
- Restoring land to pre-works condition;
 - Drainage study for relevant components, e.g. Micklefield TSC;
 - Site setup including solar / hybrid generators;
 - Resource efficiency and materials management;
 - Heritage protection and enhancement; and
 - Biodiversity Net Gain – replacing habitat loss and adding 10%;

- Embedded design to avoid mature tree loss where practicable;

Operation

10.14.3 The Scheme works are required in support of the wider electrification for the TRU route from Manchester Victoria through to York. Electrification of main routes such as TRU form part of Network Rail's strategy to be carbon neutral by 2050 by removing direct consumption of fossil fuels with diesel units.

11. HISTORIC ENVIRONMENT

11.1 Introduction

- 11.1.1 This section of the Statement of Case (SoC) is concerned with effects of the Scheme on the historic environment. It places the Scheme within its heritage context and outlines the key elements of the Scheme with relevance to the historic environment. It provides a commentary on embedded mitigation measures to address any harm caused to identified heritage assets.
- 11.1.2 All aspects of the historic environment are considered within the Environmental Report **(NR16)** (this excludes the Listed Building Consents which are covered in the Alternative Options Evaluation Studies **(NR46)** and Heritage Statements **(NR47)**). The ER identifies designated and non-designated heritage assets with the potential to be impacted by the Scheme. Information to inform this assessment was taken from the National Heritage List for England (NHLE) and the West Yorkshire Historic Environment Record (WYHER). A heritage asset, as defined by the National Planning Policy Framework (2023), is *‘a building, monument, site, place or landscape identified as having a degree of significance meriting consideration in planning decisions because of its heritage interest’* (being archaeological, architectural, artistic or historic).
- 11.1.3 The historic environment in relation to the Scheme reflects the transition from agricultural landscape to industrialisation and the establishment of West Yorkshire as a leader in the textile trade. The Scheme follows the existing railway, the origins of which lie in the mid-19th century. As such, the historic context is largely defined by railway structures and those assets which developed around it. This includes a number of listed buildings, as well as a scheduled monument.
- 11.1.4 With regard to the potential to impact below-ground archaeology, embedded mitigation has been incorporated into the final designs of the Scheme as detailed in the Environmental Report **(NR16)**. Mitigation measures will be secured through Section 5 of the Code of Construction Practice (CoCP) Part A **(NR17)** and an associated planning condition of the Order. This condition will ensure that works will only commence in areas of archaeological interest with an agreed construction methodology approved by the local planning authority.
- 11.1.5 Works within the Leeds City Centre Conservation Area are limited to the installation of small-scale electrification and signalling infrastructure mounted on metal staging structures between Kirkgate Viaduct and Marsh Lane Viaduct at Penny Pocket Park in Leeds City Centre. The proposed works

involve small-scale electrification and signalling infrastructure within an area of existing railway infrastructure. As such it is not considered that there will be any harm to the Conservation Area or heritage assets that lie within it.

- 11.1.6 The scheme will lead to physical impacts to four Grade II listed buildings, including the total loss of three and physical changes to the fourth. Listed Building Consent (LBC) applications have been submitted to Leeds City Council for the approval of these works. The applications have been referred to the Secretary of State for determination alongside the TWAO application. The LBC applications are submitted alongside Heritage Statements and Alternative Options Evaluation Studies. Mitigation has been embedded into the scheme through design evolution and opportunities to better reveal significance where harm is being caused.

11.2 Heritage Context

- 11.2.1 The route reflects the agricultural development of the landscape until the arrival of the industrial age in the late 18th and 19th century. This had a striking effect on the landscape, particularly at the western end with the expansion of Leeds as an industrial centre.
- 11.2.2 The railway formed an important part in this industrialisation, cutting through the rural landscape to link the burgeoning towns of West Yorkshire. The railways took over from the canals in transporting goods and people across the country. The canals were focussed around the existing waterways, linking the industrial towns of the midlands and northwest. The railways provided the opportunity to link more rural areas and smaller towns to encourage a larger movement of people, and thus a workforce, over goods.
- 11.2.3 The East Leeds to Selby line (now forming part of the Transpennine railway) was one of the first main lines to open after the Liverpool and Manchester Railway. The line was granted permission in 1830 and was constructed to the designs of notable engineer James Walker. The railway was opened to the public on the 22nd of September 1834, delayed due to shortages of labour and materials and embankment landslides. In the 1840s, Walker designed the Hull & Selby Railway which extended the line to the docks. By 1869 the line had been lengthened from Marsh Lane to Leeds centre and from Micklefield to Church Fenton under the North Eastern Railway. A number of works were undertaken under their ownership. This included the rebuilding of many of the intermediate stations in the house style, including Garforth Station in 1873. The Marsh Lane tunnel was widened and re-opened in 1894.
- 11.2.4 Rapid urban growth continued into the mid-20th century, now as the result of housing developments rather than industry. Settlements along the railway

expanded accordingly to accommodate the growing commuter population, particularly around Crossgates and Garforth. Austhorpe Lane, which had been a rural lane linking Austhorpe Hall and Manston Lodge, was lined with housing to the north by the early 20th century as a result of the rebuilding of Cross Gates Station in 1902. In the mid-20th century housing was also encroaching to the south, with the gradual coalescence of Cross Gates and Austhorpe. Ridge Road in contrast carried a turnpike road over the railway, following the alignment of an earlier Roman route. With the area dominated by coal mines and limestone quarries, it remained free of development.

- 11.2.5 A number of Walker's structures still remain as part of the line which has continued to grow and evolve as demand for its services has increased and railways have modernised. A survey of the structures was carried out in 2014 and, as a result, a number of structures, principally bridges, were given Listed Building status. The reasons for listing mainly focused on the uniform and unusual design attributed to James Walker and William Burges, notably the 'basket' arch bridges. These have group value, designed to the same aesthetic and local materials. The design also represents an unusual solution to the accommodation of four tracks, although only two were ever constructed. The semi-elliptical 'basket' arch is unique to the Leeds to Selby line and represents a feat of engineering specifically designed for their location. Originally there were 43 bridges across the Leeds to Selby route, 13 of which survive. Those which survive largely unaltered have been designated as grade II listed buildings; however, a number of similar structures survive which, due to subsequent loss of historic fabric and modern intervention, do not meet the high standards of designation, but nevertheless add to the group value of the railway.
- 11.2.6 Three of those bridges subject to LBC applications represent examples of Walker's 'basket' arch structures. Roman Ridge Road Overbridge, Brady Farm Overbridge and Austhorpe Lane Overbridge. The Statement of Significance for these bridges has identified historic interest as part of the original Leeds to Selby Railway. The Leeds to Selby line was constructed during the early phase of railway building, before the 'railway mania' of the mid-19th century, and is therefore one of the earliest in the world. This interest is reinforced by its association with the renowned engineer James Walker who constructed a pioneering railway intended for four tracks, despite only two ever being installed. This led to the unusual single span arch design which was unprecedented at the time of construction, extending over 18.6m. Architecturally the bridges are also of interest due to the high level of craftsmanship and materials used, with attention to detail in ashlar voussoirs and notably pronounced tooling and curved piers to its parapet.

- 11.2.7 All the bridges have been subject to alteration due to their continued use. In particular, damage has been done to Brady Farm Overbridge due to the installation of the modern railings to both sides and across the entire length, while Austhorpe Lane Overbridge has been altered through the addition of modern crash barriers and a pedestrian footbridge.
- 11.2.8 Other structures were added to the railway as it developed and contribute to the understanding of the evolution of the railway and enhance its group value. Notable among these is Crawshaw Woods Overbridge, (1419062; listed as Crawshaw Woods (Shippen House Farm) bridge), a grade II listed cast iron overbridge. Crawshaw Woods Overbridge was designed by Walker and Burges as one of only two cast-iron bridges built over the Leeds and Selby railway 1830-34. The bridge comprises a segmental cast-iron single arch with a 50 ft span with pierced balustrade, made by Stanningley Ironworks in Leeds. The bridge is of particular historical interest as the earliest cast-iron bridge still in-situ over an operational railway.

11.3 Heritage Options Assessed

- 11.3.1 As set out in Chapter 5, TRU requires the full electrification of the Transpennine Route east of Leeds, overhead line electrification (OLE) infrastructure is needed to power faster and more environmentally friendly electric trains. Due to the historic construction of the line, a number of historic structures cannot accommodate the proposed electrification in their current form. This includes a number of historic overbridges which are not of sufficient height to accommodate the operational minimum requirements for clearance distances between the trains and the OLE.
- 11.3.2 For each structure an initial engineering review was undertaken to identify alternative options which would facilitate OLE construction through the structures. This process looked at various high-level options to achieve electrical clearance for the installation of OLE while maintaining the historic fabric. These options focussed on:
- track movement to increase soffit height;
 - structure intervention to increase soffit height; and
 - reduced electrical clearances with additional control mitigations.
- 11.3.3 The outcome of the initial engineering review concluded that a more extensive engineering review was required for four structures (Ridge Road Overbridge; Brady Farm Overbridge; Crawshaw Woods Overbridge; Austhorpe Lane Overbridge) where clearance distances could not be

accommodated. For these structures further potentially feasible options were examined, including:

- Option A – Structure Intervention to raise soffit height
- Option B - Track Slue
- Option C - Track Lower

| Option | | | | | |
|---|---|----------------|----------------|---|--|
| Option A Structure Intervention | <table> <tr> <th data-bbox="614 636 1002 672">Bridge Jacking</th><th data-bbox="1002 636 1394 672">Reconstruction</th></tr> <tr> <td data-bbox="614 672 1002 1088">A patented technique which involves cutting the arch free from its abutments and wing walls so it can be jacked upwards to increase the electrical clearance space below it. The option involves physical intervention to the listed fabric, but retains the basket arch structure.</td><td data-bbox="1002 672 1394 1088">Involves the removal of the present arch and a replacement flat bridge deck at a greater soffit height to achieve the necessary clearance for electrification.</td></tr> </table> | Bridge Jacking | Reconstruction | A patented technique which involves cutting the arch free from its abutments and wing walls so it can be jacked upwards to increase the electrical clearance space below it. The option involves physical intervention to the listed fabric, but retains the basket arch structure. | Involves the removal of the present arch and a replacement flat bridge deck at a greater soffit height to achieve the necessary clearance for electrification. |
| Bridge Jacking | Reconstruction | | | | |
| A patented technique which involves cutting the arch free from its abutments and wing walls so it can be jacked upwards to increase the electrical clearance space below it. The option involves physical intervention to the listed fabric, but retains the basket arch structure. | Involves the removal of the present arch and a replacement flat bridge deck at a greater soffit height to achieve the necessary clearance for electrification. | | | | |
| Option B Track Slue | <p>Track slue involves moving the tracks to install OLE and enable trains to pass under the bridge at its highest point. To achieve this, the rail, sleepers, track drainage and track level services would need to be moved horizontally. Track slues would need to extend beyond the structure due to track geometry rules.</p> <p>For each structure a minimum of three slue options were considered, including options which would achieve sub-functional electrical clearance.</p> | | | | |
| Option C Track lower | <p>Track lower involves lowering the track, but retaining its current horizontal alignment in order to achieve the necessary clearance. A Track lower involves locally lowering the level the rails, sleepers, track drainage, track level services, ballast and sub ballast layers to provide clearance. Track lower also requires excavation over a significant length due to restrictions on the change of gradient on the approaching tracks.</p> <p>For each structure a minimum of three lower options were considered including options which would achieve sub-functional electrical clearance.</p> | | | | |

Table 3 - Options considered in initial engineering review

11.3.4 All options were assessed against set criteria defined in order to allow an objective and consistent assessment of alternative options across all options. The criteria included:

- Environment and Consent Risk – addressing environmental concerns, planning risks and consents risk.
- Land & Property – addressing land access and availability concerns.
- Cost – addressing capital and maintenance cost constraints.
- Design / engineering feasibility – to address varying levels of design complexity.
- Construction – to address varying levels of construction complexity.
- Maintenance – to address varying levels of maintenance burdens.
- Deliverability – to address the impact on wider project programme timescales.

11.3.5 For all the bridges, both track slue and track lower were rejected due to the natural geology of the area which would require extensive rock breakout and re-stabilisation of the adjacent cutting slope, considered to be a high risk construction activity requiring track closures outside the permitted allowances. This risk is increased by the presence of historic mine workings in the area which could de-stabilise the track. The cost required to deliver such works would also be prohibitive and it was concluded that a track lower and slue option was not a feasible solution.

11.3.6 The remaining options involved a structural intervention in order to achieve the necessary clearance. Initial optioneering sought to retain the current form of the bridge, initially through bridge jacking (lifting) which would retain the historic fabric, or through reconstruction. Where this was not possible, alternative options were assessed.

| Option | | |
|---|--|--|
| Bridge Jacking | Reconstructed Arch | Flat Deck |
| Bridge jacking has never been tested on a live railway or on a masonry bridge. It would involve significant intervention into the | For both Roman Ridge Road (HUL4/14) and Austhorpe Lane (HUL4/21), reconstructing the arch at a higher level would result in a significant lift of the highway worsening the visibility for | The flat deck reconstruction option would involve the total loss of the historic structure and replacement with a new flat deck type structure. This would provide the necessary |

| Option | | |
|---|--|--|
| Bridge Jacking | Reconstructed Arch | Flat Deck |
| <p>fabric of the bridge. Critical failure could lead to loss of historic fabric and prolonged closure of the railway.</p> <p>For both Roman Ridge Road and Austhorpe Lane , the lifting of the structure would result in a significant lift of the highway worsening the visibility for road users. This would require the redesign of existing road junctions and would impact on adjacent landowners.</p> <p>Bridge jacking is not appropriate for Crawshaw Woods Overbridge (HUL4/20) due to its construction.</p> | <p>road users. This would require the redesign of existing road junctions and would impact on adjacent landowners.</p> <p>For Crawshaw Woods Overbridge, reconstruction is possible as there are no highway restrictions. In addition, the use of cast iron over masonry means that the bridge can be deconstructed within permissible track closures.</p> | <p>clearance while maintaining highway levels above.</p> <p>This option provides the opportunity for sensitive bridge design solutions which recognise the historic significance of the railway.</p> |

Table 4 - options considered in initial engineering review

- 11.3.7 For the three stone bridges (Roman Ridge Road Overbridge, Brady Farm Overbridge and Austhorpe Lane Overbridge), it was not possible to keep the historic fabric, with the remaining option being a flat deck replacement. This will result in total loss of significance, in accordance with the National Planning Policy Framework (paragraph 201). In consultation with Historic England and Leeds City Council it was concluded that a standard composite flat deck could not compensate for the loss of the historic structures; therefore, a bespoke design solution was sought. This design incorporates a curved steel arch feature to mimic the geometry of the existing historic basket arch design used on several bridges in the East of Leeds railway route and the use of reclaimed stone facing material. The aim of the new design was to replicate the key architectural interest of the structures through the use of a basket arch, while creating an obviously modern feature.

- 11.3.8 In respect of Brady Farm Overbridge, as there is no functional need to provide a crossing at this location (the current bridge being redundant in functional terms), it was decided not to replace this bridge. The removal of Brady Farm in its entirety provides the material necessary to raise the abutments and the parapets on the bridges, which are being reconstructed to ensure a consistent aesthetic.
- 11.3.9 Roman Ridge Road Overbridge and Austhorpe Lane Overbridge will both be reconstructed using the bespoke design solution, which is described in more detail in section 11.4.
- 11.3.10 For Crawshaw Woods Overbridge it was concluded that the present bridge could be lifted to achieve the desired clearance. The cast iron metalwork will be removed and restored off site, while the abutments will be raised and faced with reclaimed stone. The unsympathetic steel panel parapet will also be removed and more sympathetic railings inserted. The effect on the historic structure is considered to be beneficial as a result of the restoration work and the opportunity to better reveal the significance of the structure.

11.4 Mitigation and Compensation

- 11.4.1 The removal of a listed structure constitutes total loss of significance and thus substantial harm to a designated asset in terms of the NPPF. Compensation has been incorporated through the design of the new structures for both Ridge Road Overbridge and Austhorpe Lane Overbridge. These designs have evolved through a collaborative process involving designers, engineers, heritage professionals and statutory bodies. Brady Farm Overbridge will not be reconstructed as part of the scheme. The bridge is currently redundant and no longer accommodates its original function.
- 11.4.2 In recognition of the harm caused, embedded mitigation has been incorporated into the Scheme. This includes the following specific design measures.
- The adoption of a bespoke new feature structure which take cues from the key historic characteristic of the listed structures.
 - The adoption of a single design to create a cohesive aesthetic along the railway.
 - The incorporation of stonework from the removed bridge to maintain the aesthetic of the retained historic elements, including the abutments and the parapets.

- Repair works to be carried out to retained historic elements to secure future stability.
- 11.4.3 In addition to the embedded mitigation, compensation will also be secured through the archaeological recording of heritage assets affected, in agreement with the historic environment stakeholders. This would take the form of compensation for the harm caused by demolition or alteration, not mitigation.
- 11.4.4 Any historic fabric removed as part of the Scheme, including stonework recovered during the demolition of Brady Farm Overbridge, will be assessed for re-use elsewhere to ensure a consistency of materials. The priority will be for re-use at Roman Ridge Road Overbridge and Austhorpe Lane Overbridge. Any remaining stone will be used for undertaking repairs within the Scheme as necessary. This will be secured through condition.

11.5 Listed Building applications

- 11.5.1 Four Listed Building Consent applications have been submitted alongside the Transport and Works Act Order. These are submitted in support of the replacement of two and demolition of one Grade II listed bridges and the reconstruction of one Grade II listed bridge. Those structures subject to Listed Building Consent applications are:
- Roman Ridge Road Overbridge
 - Brady Farm Overbridge
 - Crawshaw Woods Overbridge
 - Austhorpe Lane Overbridge
- 11.5.2 Each application is supported by an Alternative Options Evaluation Study (**NR46**) and a Heritage Statement (**NR47**). The Alternative Options Evaluation Studies outline the engineering options considered to facilitate OLE construction. They identify the reasonable alternatives considered, alongside the specific alternatives for each structure. The document sets out the criteria against which each option was considered and provides an evidence base to support the preferred option.
- 11.5.3 The Heritage Statements provide the necessary information to comply with current legislation and planning guidance, namely the Planning (Listed Building and Conservation Areas) Act 1990 and the National Planning Policy Framework. The Statements serve the following purpose.

- Provide a statement on the specific significance of each structure in terms of their architectural and historic interest, alongside their group value as part of the historic railway.
 - Present the process of design development and optioneering which has led to the design proposal for the Scheme in relation to the structure, with reference to the Alternatives Options Evaluation Studies.
 - Identify the impacts of the proposal on the significance of the structure in terms of harm to significance and whether that harm is substantial or less than substantial.
 - Outline proposed mitigation and/or compensation put forward to address the identified harm.
 - Consider the need for the Scheme and the public benefits to be gained in order for the proposals to be weighed in the planning balance.
- 11.5.4 In its representation on the Order, Leeds City Council has raised no concerns regarding the Listed Building Consent applications. The objection letter submitted by Leeds City Council notes in paragraph 71 that '*mitigation has been agreed with Network Rail for the substantial harm caused by the demolition of the listed buildings as part of the proposed Scheme*'. Historic England have not objected to the application.
- 11.5.5 Ongoing engagement has been undertaken with both parties during the design process and comments addressed through the optioneering process. This has taken the form of a series of meetings and workshops between Leeds City Council and Historic England alongside the Scheme engineers and heritage specialists. Initial consultation concentrated on the discussion of reasonable alternatives resulting in all parties acknowledging that substantial harm is caused to three of the listed structures (Roman Ridge Road Overbridge , Brady farm Overbridge and Austhorpe Lane Overbridge , but that this harm is necessary in order to achieve the Scheme. Further consultation focussed on the need for a bespoke design and a number of design options were proposed. All parties agree that the current bespoke design solution is the preferred replacement and that this should be used for both bridges in order to achieve consistency.
- 11.5.6 Both Historic England and Leeds City Council welcome the opportunity to restore Crawshaw Woods Overbridge and better reveal its significance. It is agreed that the detailed methodology for the removal, restoration and reinstated of the bridge are subject to further work and that this will be undertaken in consultation with both parties through the production of a

Conservation Implementation Management Plan. This will be secured through a condition of the Listed Building Consent.

12. OPEN SPACE

12.1 Introduction

- 12.1.1 Section 12 of the TWA provides that, should an order authorise the compulsory acquisition of certain ‘special’ categories of land (e.g. commons, open spaces or National Trust land), the provisions in the Acquisition of Land Act 1981 (the “ALA”) apply which govern when a compulsory purchase order is subject to special parliamentary procedure.
- 12.1.2 Under the ALA, special parliamentary procedure applies to an order that contains powers to compulsorily acquire such land, unless the Secretary of State has certified that certain ‘tests’ have been satisfied in respect of the land subject to the relevant powers.
- 12.1.3 Network Rail has identified that the only relevant ‘special category’ of land in the context of the Order is “open space”, which is defined in section 19 of the ALA as: *“any land laid out as a public garden, or used for the purposes of public recreation, or land being a disused burial ground”*
- 12.1.4 Network Rail has identified those plots of land that fall within the order limits that could, taking a precautionary approach, fall within the definition of ‘open space’ in the ALA. These are identified in the Book of Reference (**NR08**).
- 12.1.5 Plots 5-008, 5-009, 7-017, 12-007, 12-010, 13-006 and 13-008 as identified in the Book of Reference as ‘open space’ and shown on Sheets 3, 4, 12 and 13 of the Land Plans (**NR09**), are not subject to the power of compulsory acquisition in the draft Order and are subject only to the powers of temporary use in articles 7 and 28 of the draft Order. These plots are accordingly listed in Schedule 12 to the draft Order. The provisions of section 12 of the TWA (and therefore the ALA) do not apply to these plots.

Proposed Open Space Acquisition

- 12.1.6 Powers of acquisition are sought over four plots in the draft Order which may be considered ‘open space’ at Penny Pocket Park. These are plots 2-001, 2-002, 2-003 and 2-004 (the “Open Space Plots”) as shown on Sheet 1 of the Land Plans (**NR09**) and Book of Reference (**NR08**).
- 12.1.7 The Open Space Plots are subject to powers of outright acquisition under article 22 of the draft Order, subject to article 25 (power to acquire new rights) and article 26 (power to acquire subsoil or airspace only).
- 12.1.8 Accordingly, Network Rail applied to the Secretary of State for Levelling Up, Housing and Communities on 18 July 2023 for a certificate under section

19(1)(b) of the ALA. If granted, the certificates would mean the Order (if made) would not be subject to special parliamentary procedure.

12.1.9 The application was made on the following basis:

- the Open Space Plots taken together are below the 250 square yard threshold set out in section 19(1)(b) of the ALA. The total area of land that is proposed to be acquired is 191.9m² (209 square yards);
- that the giving in exchange of other land is unnecessary, due to the proposed land to be acquired being comprised of small plots of land immediately adjacent to the active railway on a steep railway embankment; and
- the quality, location and size of the land means it has no inherent value or significance either in terms of being used for public recreation or as a burial ground. Providing replacement land would not yield any benefits nor serve any practical purpose.

12.1.10 Network Rail received notice (**NR46**) on 25 September 2023 that the Secretary of State was minded to grant the certificate. Network Rail will publicise the objection period in accordance with the statutory requirements.

13. LAND AND PROPERTY

13.1 Introduction

13.1.1 The Works and Land Plan (**NR09**) outlines the land parcels required in connection with the Scheme. A full schedule of land and rights required for the Order is presented in Appendix 3.

13.1.2 The Order, if approved, will grant Network Rail the powers to compulsorily acquire land or temporarily use land to:

- Reconstruct Kirkgate Bridge, Marsh Lane Bridge, Osmandthorpe Lane Bridge, Ridge Road Bridge and Crawshaw Woods Bridge.
- Divert gas mains which currently cross over the railway at Austhorpe Lane and Ridge Road.
- Demolish Brady Farm Bridge.
- Construct rail infrastructure outside the railway boundary at Penny Pocket Park.
- Close level crossings and divert rights of way at Peckfield Level Crossing, Highroyds Wood Level Crossing and Garforth Moor Level Crossing.
- Close the Barrowby Lane Level Crossing and Barrowby Foot Level Crossing, including the construction of a new bridleway bridge for the diversion of the public rights of way.
- Set up temporary compounds required for TRU works at Phoenix Avenue, Wykebeck Avenue and Manston Lane.
- Create a new permanent access road to the Neville Hill Sidings.
- Construct a Track Sectioning Cabin at Phoenix Avenue in Micklefield.

13.1.3 Where land is not already within the freehold ownership of Network Rail, it will be necessary to permanently acquire and/or to temporarily use land which is in private ownership.

13.1.4 In summary the main forms of acquisition proposed by the Order will consist of:

- permanent acquisition of land; and
- temporary use of land for construction of works

13.1.5 Additionally, the Order seeks to confer powers on Network Rail to:

- temporarily using land for maintenance of works;
- temporarily using land for access;
- permanently stop up rights of way;
- permanently acquire new rights;
- permanently acquire subsoil or airspace only; and
- acquire rights under or over streets.

13.1.6 All the areas of land which are sought in the draft Order are necessary for the Scheme and no land will be compulsorily acquired either permanently or temporarily unless required for the construction, operation, or maintenance of the Scheme.

13.2 Proposed Land and Property Acquisition

13.2.1 The Draft Order (**NR02**) and the Works and Land Plans (**NR09**) identify the extent to which Network Rail may exercise these compulsorily acquisition powers in relation to land situated within the Order limits.

13.2.2 The Book of Reference (**NR08**) details the owners and occupiers of land and property within the Scheme limits and is to be read in conjunction with the Works and Land Plan (**NR09**) which accompanies the Draft Order (**NR02**).

13.3 Permanent Land and Property Acquisition

13.3.1 In order to construct the Scheme works to be authorised by the Order, permanent land take will be required from third-party land/property owners. Those plots of land identified as required on a permanent basis are identified in the schedule of land and rights in Appendix 3.

13.4 Temporary Possession and Access

13.4.1 Powers of temporary possession and access are required in relation to land which is needed for construction and access purposes. Those plots of land identified as required on a temporary basis are identified in the schedule of land and rights in Appendix 3.

13.5 Stopping Up (and Diversion) of Rights Across Level Crossings

- 13.5.1 Powers are required to stop up and divert existing rights of way across the Peckfield, Barrowby Lane, Barrowby Foot, Highroyds Wood and Garforth Moor level crossings which are identified on the Works and Land Plan (NR09).

13.6 Acquisition of Permanent Rights

- 13.6.1 Powers are required to acquire new rights of way at Garforth Moor for the benefit of third parties, to allow for continued access to adjoining allotments following the closure of the adjoining level crossing.
- 13.6.2 Powers are also being sought to acquire a new right of way along the unadopted section of Phoenix Avenue in Micklefield, for the benefit of Network Rail, to access the existing Road Rail Access Point and the proposed Track Sectioning Cabin (TSC).

13.7 Engagement with Landowners and Acquisition Strategy

- 13.7.1 Over the course of the design development of the Scheme, regular engagement has taken place with impacted landowners, the full details of which are described in the Consultation Report (NR07 and section 14 and 15 of this Statement of Case).
- 13.7.2 Network Rail contacted all landowners impacted along the route to explain the project and seek site meetings to provide explanation of the proposed works. At these meetings Network Rail recorded the concerns and opinions of impacted parties, feeding this back to the project team and addressing these where possible.
- 13.7.3 Examples of this engagement has included a regular working group for key stakeholders at Thorpe Park, regarding the proposals for the Manston Lane Compound and the Crawshaw Woods Bridge reconstruction. Through engagement with key landowners and stakeholders in this area, the Manston Lane compound was repositioned to avoid interference with the Thorpe Park Station Development. Furthermore, discussions were held on how to mitigate any impact of our proposals on the Scarborough Group's proposed industrial development adjoining Willian Parkin Way.
- 13.7.4 In addition, regular engagement was held with Leeds City Council as an impacted landowner. One of the results of this engagement, was placing the Phoenix Avenue compound in a position which avoided interference with land that the Council was negotiating the sale of.

- 13.7.5 At Peckfield Level Crossing, landowners were consulted regarding the options considered for schemes to mitigate the closure of the level crossing and bridleway, with the opinions recorded and feeding into the option selection stage.
- 13.7.6 Network Rail has sought to negotiate agreements with landowners where possible.
- 13.7.7 Engagement and negotiations are on-going with landowners for the land requirements included in the Order. The design and land requirements, as promoted through the Order, represent the requirement as it currently stands to deliver the Scheme. Detailed design will continue, and NR will only acquire the necessary proprietary interests required to construct the Scheme.

13.8 Justification for Compulsory Purchase and the Compensation Code

- 13.8.1 The compulsory purchase of the land/property and associated interests included within the Order is required to ensure the Scheme can be implemented and for Network Rail to deliver the Scheme within a reasonable timescale.
- 13.8.2 In preparing and making the Order application and preparing this Statement of Case, Network Rail has had due regard to the guidance on compulsory purchase, entitled "Guidance on Compulsory Purchase Process and the Crichel Down Rules", introduced in October 2015 and last updated in July 2019 by the Ministry Housing, Communities & Local Government ("the Guidance").
- 13.8.3 The Guidance advises that an acquiring authority should only make a compulsory purchase order where there is a compelling case in the public interest to do so. Network Rail is satisfied that there is a compelling case in the public interest for the use of compulsory purchase powers for the reasons set out in this Statement of Case (see section 6 of this Statement of Case for more details).
- 13.8.4 The timely acquisition of these interests on reasonable terms and within a realistic timescale will very likely only be achievable through securing powers of compulsory purchase.
- 13.8.5 The Order makes provision for those parties whose land/property or interests in land/property are compulsorily acquired or used (either temporarily or permanently) to be entitled to claim compensation and for Network Rail to make payments of compensation in line with the Statutory Compensation Code. That Code is a series of statutes, supported by case law, that form the

legal framework under which compensation is assessed following the compulsory acquisition of land.

- 13.8.6 All efforts will be taken to settle compensation claims in a timely and reasonable manner. Where agreement cannot be reached between Network Rail and the affected parties, both the acquiring authority and the claimants will have the right to refer the claim to the Upper Tribunal (Lands Chamber) or seek to settle the matter via Alternative Dispute Resolution.

13.9 Schedule of Land and Rights Required

- 13.9.1 For a detailed overview as to the specific land and property requirements please see the schedule attached at Appendix 3.

14. CONSULTATION AND ENGAGEMENT

14.1 Consultation

- 14.1.1 Network Rail has consulted widely on the Order Scheme with stakeholders (statutory and landowner)³ and the public (including ward councillors and access groups) from November 2021 to April 2023. The purpose of consultation has been to ensure statutory bodies, landowners, members of the public and other stakeholders had an opportunity to understand and comment on the Order Scheme and potential environmental effects.
- 14.1.2 Prior to starting public consultation in October 2022, Network Rail shared its proposed Approach to Community Consultation (**AtCC**) with Leeds City Council (**LCC**) for comment. The approach summarised the engagement to date and how Network Rail proposed to consult with communities affected by the Order Scheme and lineside neighbours. Network Rail considered LCC's comments on the AtCC and worked to incorporate them and agree a robust consultation approach.
- 14.1.3 Stakeholder (statutory and landowner) consultation took a five-phase approach. The number of phases was guided by the number of design iterations required for Order Scheme elements, as well as the addition of new Order Scheme elements.
- 14.1.4 Public consultation (including with ward councillors and access groups) took a two-phase approach. The first phase focussed on the majority of the Order Scheme elements and the second phase covered two new level crossings (Garforth Moor and Highroyds Wood Level Crossings), which were added into the Order Scheme after the first public consultation.
- 14.1.5 A summary of consultation phases is set out in Table 8 below. The Consultation Report (**NR07**) sets out in detail who was consulted, on what issues, during each of the phases of the consultation.
- 14.1.6 Consultation has been important in helping identify potential issues at an early stage of the Order Scheme's development. It has enabled Network Rail to collate as much feedback as possible as the design of the Order Scheme has progressed. Comments from both stakeholders (statutory and landowner) and the public have been considered and have informed the iterative design process.

³ Stakeholder (statutory) consultation includes engagement with the organisations listed in column (2) of the table in Schedule 5, and column (2) of the table in Schedule 6 of the Application Rules.

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| Date | Consultation activity |
|---------------------------------|--|
| November 2021 – May 2022 | Phase 1 stakeholder (statutory and landowner) and targeted public consultation (Barrowby and Peckfield Level Crossings) |
| July – August 2022 | Phase 2 stakeholder (statutory) and targeted public consultation (Peckfield Level Crossing) |
| October – November 2022 | Phase 3 stakeholder (statutory) consultation (all Order Scheme elements, excluding Highroyds Wood and Garforth Moor Level Crossings) |
| October – November 2022 | Phase 1 public consultation (all Order Scheme elements, excluding Highroyds Wood and Garforth Moor Level Crossings) |
| March – April 2023 | Phase 4 stakeholder (statutory and landowner) and targeted public consultation (Highroyds Wood and Garforth Moor Level Crossings) |
| March – April 2023 | Phase 2 public consultation (Highroyds Wood and Garforth Moor Level Crossings) |
| April 2023 | Phase 5 stakeholder (statutory and landowner) consultation and targeted public consultation (Peckfield Level Crossing) |

Table 5 - Summary of public and stakeholder (statutory and landowner) consultation

14.2 Publicising the consultation

14.2.1 Consultation was ongoing with all consultees throughout the consultation period with meetings, phone calls, emails, presentations and site visits.

14.2.2 The public events were promoted using several methods, which are detailed below.

- An A5 double-sided flyer was sent to 8,600 households. A 250m radius was used to identify households within Leeds City Centre and a radius of between 500m-1km was used for the other proposed work sites between Cross Gates and Micklefield.
- Posters were displayed at local stations and community spaces. Tweets about the public consultation were posted from the @theGNRP Twitter account (Network Rail's account for the Great North Rail Project) and shared from the @NetworkRailLDS account (Network Rail's account for Leeds City Station).
- A sponsored Facebook and Instagram post was sent from Network Rail's accounts to users in a targeted area.
- Information on the Scheme and the public consultation was made

available on the Network Rail website: www.networkrail.co.uk/Leeds-Micklefield.

- During the public consultation periods, the link to the Network Rail Order Scheme webpage www.networkrail.co.uk/Leeds-Micklefield was sent to technical stakeholders with an offer to discuss the proposals further, either by telephone or meeting.

14.2.3 The project team reviewed all consultation feedback at each consultation phase and, where feasible, this feedback influenced the design proposals.

14.3 Ongoing engagement

14.3.1 Since first consulting with the public, landowners and key stakeholders (including ward councillors and access groups) in November 2021, many of the discussions with organisations and individuals have been iterative, with regular updates via meetings or site meetings. This approach will continue for the duration of the Order Scheme.

14.3.2 In particular, regular engagement has taken place with LCC (OBJ07) to update on the design and programme of the Order Scheme, and these meetings will continue post submission of the Order.

15. OBJECTIONS, REPRESENTATIONS AND STATEMENTS OF SUPPORT

15.1 Objection period

15.1.1 After the Order application was made to the SoS on 17 July 2023, the SoS invited objections, representations, and letters of support, in accordance with the 2006 Rules, to be sent to the SoS by 29 August 2023. During that period 30 objections, seven representations and three letters of support were received.

15.2 Issues raised in objections

15.2.1 Table 5 below groups together the key issues raised by objectors under broad headings and shows the number of times each issue was raised. It is not an exhaustive list and, where typographical errors were noted, these have not been included in this table. Each objector often referred to more than one issue.

| Issue | Number of times raised |
|---|------------------------|
| Closure of Peckfield Level Crossing. | 16 |
| Lack of bridge at Peckfield Level Crossing. | 7 |
| Impact on land and property. | 6 |
| Inadequate consultation and information provided. | 6 |
| Querying validity of user surveys completed at Peckfield Level Crossing. | 3 |
| Impact of proposed bridleway option at Peckfield Level Crossing. | 3 |
| Pedestrian safety at Austhorpe Lane. | 3 |
| Concerns over safety and design of proposed two-way road over new Austhorpe Lane Bridge | 3 |
| Request for clarification on application plans. | 3 |
| Concerns over demolition of Grade II listed Brady Farm Bridge. | 2 |
| Impact of Austhorpe Lane proposals on the environment and community. | 2 |
| Impacts on Public Rights of Way | 2 |
| Approach to level crossings. | 1 |

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| Issue | Number of times raised |
|--|------------------------|
| Approach to the replacement of structures. | 1 |
| Approach to use of Code of Construction Practice and LCC Highways Side Agreement. | 1 |
| Permitted development rights, planning policy, deemed planning conditions, deemed consents. | 1 |
| Disapplication of other legislation and consents sought within the draft Order. | 1 |
| Impact of construction work on train services. | 1 |
| Concern about increased anti-social behaviour due to the Barrowby Level Crossings proposal. | 1 |
| Cost to taxpayers. | 1 |
| Access route for demolition of Brady Farm Bridge. | 1 |
| Grade II listed Ridge Road Bridge being reconstructed using a new design. | 1 |
| Ensuring the development of adjacent land at Peckfield Level Crossing is not frustrated by Network Rail's proposals. | 1 |
| Safe access to compounds. | 1 |
| Proposed location of the footpath at Austhorpe Lane. | 1 |
| Demolition of Grade II listed Austhorpe Lane Bridge. | 1 |
| Impact of increased line speeds. | 1 |
| Impact on heritage across the whole of TRU East. | 1 |
| Impact on the environment, ecology, and land contamination. | 1 |
| Impact on the highway network, cycle network and Public Rights of Way at Neville Hill. | 1 |
| Potential construction on and future usage of land on the corner of Austhorpe Road and Railway Road. | 1 |
| Costs for new structure at Austhorpe Lane. | 1 |
| Request for sight of various environmental documents including Landscape and Ecological Management Plan (LEMP), Biodiversity Net Gain (BNG) and Invasive Non-Native Plant Species (INNPS). | 1 |

| Issue | Number of times raised |
|--|------------------------|
| Visual impact of Barrowby Level Crossing bridge proposal on local amenity, wildlife and the landscape. | 1 |
| Accessibility of new bridleway bridge at Barrowby Lane | 1 |

Table 6 - Issues raised in objections

15.3 Issues raised in representations

15.3.1 Table 6 groups together issues raised in representations under broad headings and shows the number of times each issue was raised. Each representation often referred to more than one issue.

| Issue | Number of times raised |
|--|------------------------|
| Impact of proposed bridleway option at Peckfield Level Crossing on the Recreation Ground. | 2 |
| Closure of Peckfield Level Crossing. | 2 |
| Traffic impact on Micklefield from the closure of Ridge Road during works. | 2 |
| Demolition of Grade II listed structure at Ridge Road . | 2 |
| Impact on highway network, cycleways and National Cycle Network at Neville Hill. | 1 |
| Impact of Peckfield Level Crossing closure on connectivity between the north and south of Micklefield. | 1 |
| Support for Public Right of Way diversion at Highroyds Wood. | 1 |
| Request for further information on protective provisions regarding flood risk and drainage in the Order. | 1 |
| Request for protective provisions to protect National Highways roads. | 1 |
| Request for appropriate security at Phoenix Avenue Compound. | 1 |
| Request for Phoenix Avenue compound to be restored to its previous state once no longer required. | 1 |
| Request that appropriate guidance and mitigations are applied to land that falls within the defined Development High Risk Area for mining. | 1 |

Table 7: Issues raised in representations

15.4 Managing objections and representations

Objections

- 15.4.1 Network Rail has contacted all objectors and, where relevant, has offered to meet with them either in person or virtually. Network Rail has also responded to all letters of objection to address the concerns raised.
- 15.4.2 Where the Order seeks compulsory powers to take temporary possession of land, or to permanently acquire interests in land, Network Rail's objective is to continue engagement with the affected landowner and to reach terms acceptable to that individual, which address the concerns raised in the letter of objection. This approach would enable Network Rail to enter into agreements with affected landowners, or to issue relevant commitments and assurances, which will allow affected landowners to withdraw their objection to the Order.
- 15.4.3 Table 7 below sets out the matters raised by statutory consultees who have made objections, and Network Rail's position in relation to these objections.
- 15.4.4 Table 8 sets out the matters raised by landowners who have made objections, and Network Rail's position relating to these objections.
- 15.4.5 Table 9 sets out the matters raised by all other objectors, categorised by Order Scheme element, and Network Rail's position relating to them.

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| OBJ07 | Leeds City Council | <ul style="list-style-type: none"> • Approach to use of permitted development rights • Approach to consents sought within the draft Order • Level of detail in application documents • Errors in the Order and application document • Application plans • Approach to land acquisition • Approach to level crossings • Biodiversity Net Gain • Ecology – Great Crested Newts and Other Protected Species • Deemed Consents and disapplication of other legislation • Planning policy and guidance • Deemed planning conditions | <p>LCC is supportive in principle of the outcomes of the Scheme, including improved safety and efficiency of the railway, modernised signalling, reduced costs and reduction in delays, however the council also raised several concerns in its objection.</p> <p>Prior to receiving this objection, Network Rail and the council have met regularly via a Highways Working Group and regular meetings to discuss ongoing requirements or concerns.</p> <p>Following receipt of LCC's objection, Network Rail issued letters on 29 September 2023, 13 October 2023 and 17 October 2023 addressing the concerns raised by the stakeholder in OBJ07, and are meeting regularly with LCC to work through outstanding issues, with a view to agreeing a Statement of Common Ground, Highways and Environmental Agreements and Deemed Planning Conditions prior to Inquiry.</p> <p>Network Rail has also responded to various points of detail as included in appendix 1 to OBJ07. This full response dated 13 October 2023 is provided at NR19 and some of the main themes are summarised below.</p> <p>Use of Permitted Development Rights</p> <p>Network Rail has explained its approach to use of permitted development rights, the approach to consents sought, deemed consents and disapplication of other legislation. It has also confirmed that it has only included land within the Order limits which is needed for the Order Scheme. Where possible Network</p> |

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| | | <ul style="list-style-type: none"> • Replacement of structures • Highways • Austhorpe Lane • Safe access to compounds • Code of Construction Practice • Side Agreement • Approach to closure of Peckfield Level Crossing • Communication Plan • Sustainable Transport and PROW • Heritage • Environmental Report • Land contamination | <p>Rail has sought to limit the powers to temporary possession powers only.</p> <p>Approach to Consents Sought in Order</p> <p>Network Rail has explained that the Transport and Works Act 1992 prescribes a number of powers which can be sought as part of a TWAO and places no restriction on the use of TWAO powers in combination with other consenting processes. Further, it was explained that the environmental report does not consider the effects of certain additional elements which are either being carried out as permitted development or through planning permission obtained separately from the local planning authority. The proposed Leeds to Micklefield Enhancements Order includes powers to acquire or use land to deliver these elements but does not include authorisation and deemed planning permission for these works.</p> <p>The CoCP (NR17) is clear that the measures set out in the CoCP will apply both to the works authorised by the Order, and the works carried out as permitted development. This is confirmed in para 1.3.1:</p> <p><i>“The CoCP sets out the relevant measures and standards to be followed throughout the construction period and has been prepared to ensure that the proposed construction-related incorporated mitigation identified in the Environmental Report (ER) (NR16) is implemented by Network Rail, whether the works</i></p> |

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| | | | <p><i>be elements requiring planning permission or come under general Permitted Development."</i></p> <p>Environmental Impact Assessment</p> <p>Network Rail has confirmed that the Scheme is not EIA development, noting that an EIA Screening Opinion was received from LCC confirming as much, and a further EIA screening direction was received from the Secretary of State on 17 May 2023, confirming that an EIA is not required for the Order Scheme.</p> <p>Approach to Level Crossings</p> <p>Works requiring planning permission are only proposed for the schemes to mitigate the closure of the Peckfield and Barrowby Lane level crossings, therefore the Order includes deemed planning permission applications only for these level crossing schemes.</p> <p>Any works required in connection with the closures of the other crossings fall within the scope of Network Rail's permitted development rights.</p> <p>For all four level crossings, powers necessary to stop up existing PRow across level crossings and to create new PRow via alternative routes are included in the Order.</p> |

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| | | | <p>Peckfield Level Crossing</p> <p>Network Rail continues to discuss the options proposed at Peckfield Level Crossing with LCC, alongside continuing to discuss matters of detail regarding the other elements within the Order.</p> <p>Heritage</p> <p>It is Network Rail's assessment that substantial harm will be caused due to the total loss of Austhorpe Lane, Brady Farm and Ridge Road Grade II listed bridges only (and mitigation has been agreed with LCC) and that less than substantial harm will be caused by the partial dismantling and reconstruction of Crawshaw Woods bridge.</p> <p>Replacement of Structures</p> <p>At Austhorpe Lane, Network Rail has, at the request of LCC, taken the opportunity to improve the current arrangement significantly. The current bridge is a single carriageway with below standard visibilities for road users. The proposed bridge provides two narrow lanes. The structural elements of the bridge will also provide significantly improved parapets for the safety of road and rail users.</p> <p>At the A656 Ridge Road, like-for-like replacement is appropriate given the nature and alignment of the road, but Network Rail has taken the opportunity to improve the vertical profile of the road so that it now meets current design standards. The overall carriageway width has been increased and the design meets</p> |
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| | | | <p>accessibility standards. Network Rail has designed the structures in accordance with NPPF paragraphs 110 and 112.</p> <p>Highways</p> <p>Network Rail has provided LCC with a detailed 'consents and works register' which includes information on the programming of the works within the administrative area of LCC to a specification requested by LCC.</p> <p>Network Rail will provide LCC with an updated version of this document periodically, when revised programme information is available. In addition, Network Rail will provide an overview programme document which will set out the construction phasing of the main elements of the Scheme.</p> <p>Details on how the impacts of construction work, including road closures and diversions, construction traffic, and impacts on emergency services will be discussed as part of the remit of the Network Management Group that is proposed under the draft Highways Agreement that is in the process of being negotiated.</p> <p>Sustainable transport and PRow</p> <p>The draft Highways Agreement which is currently being negotiated contains a series of protocols relating to the use of both temporary and stopping up powers under the Order.</p> <p>The purpose of the protocol is to ensure that LCC will have advance notification of any proposed stopping up or diversion of</p> |

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| | | | <p>routes and to agree measures to ensure that they are appropriately managed. Details of implications for cycle routes and mitigation requirements will be discussed with LCC under the remit of the Network Management Group</p> <p>Open Space</p> <p>Under s.19 of the Acquisition of Land Act 1981, replacement land is not required to be provided if the land to be acquired does not exceed 250 square yards in extent.</p> <p>Network Rail measured the area of open space within Penny Pocket Park, included in the Order, to be 191.9 m² or 229.5 sq yards.</p> <p>Biodiversity Net Gain</p> <p>It is in Network Rail's interest to reduce the required number of biodiversity units as assessed through the BNG metric by keeping any habitat creation and enhancement works immediately local. Where this is not practicable, delivery will be within the Council's administrative boundary.</p> <p>Network Rail has identified the areas of vegetation that will likely be required to be removed. There will be a minimum 10% net gain for biodiversity, secured by condition. The enhancement work by default will contribute to the Leeds Habitat Network that has been considered in the ER (NR16). Section 7.3 of the ER explains that significant adverse effects on the integrity and connectivity of the Leeds Habitat Network will be avoided due to</p> |

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| | | | <p>the retention and reinstatement of habitats and the proposed creation/enhancement of new habitats.</p> <p>Ecology</p> <p>Network Rail has reviewed the intention to use its operating organisational licence which includes a full assessment of the impact of the proposed works on great crested newts and their aquatic and terrestrial habitats and provides mitigation and off-site compensation that is proportionate to the predicted impact.</p> <p>Section 7.3.1 of the ER (NR16) identifies the general mitigation that is required in considering ecology in the absence of the need to acquire a licence. Network Rail will produce a draft licence application to be submitted to Natural England which will include the details of the mitigation strategy and compensation measures to address the predicted impact.</p> <p>Air Quality and Land Contamination</p> <p>The screening request submitted by Network Rail clearly stated why air quality effects during construction were not significant. In considering the Scheme, the ER requires a Nuisance Management Plan to be produced and this mitigation is secured through planning condition 6(c)(iv) CoCP Part B: "A nuisance management plan concerning dust, wheel wash measures, air pollution and temporary lighting", that must be submitted to and approved by LCC. It is at this point of approval that the Council</p> |

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| | | | <p>can satisfy itself that residual effects will be appropriately managed through the document submitted.</p> <p>In terms of operation of the new railway, rail engine diesel stock will be replaced by electrified trains as a positive environmental effect in considering air emissions.</p> <p>The EIA screening requests identified there were no significant effects from land contamination and on this basis, Network Rail would proceed with compliance with NR/L2/ENV/015, particularly Section 6.2 Contaminated Land of CR-E. This requires a Preliminary Risk Assessment (PRA) to be carried out on occupied land at minimum. Dependent on the PRA, additional assessment work may then be required in compliance with CR-E.</p> <p>Network Rail is content to agree a suitably worded condition based around the residual risk that land with contamination may be encountered and would need to be dealt with. The wording of the condition can reflect the general requirement to comply with CR-E Section 6.2 Contaminated Land.</p> <p>Any contamination encountered on the track bed will be managed through compliance with Network Rail's Ballast Handling Protocol.</p> <p>Communications</p> <p>The ER (NR16) identifies a commitment to complete an External Communications Plan, and this will be a recommended condition to be agreed with LCC.</p> |

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| | | | <p>LCC will approve the external comms plan which will, amongst other matters, identify any further pre-construction meetings or briefings of local residents / communities in advance of letter drops that occur immediately before works.</p> <p>Network Rail is committed to ensuring a good working relationship with LCC and has committed to holding a monthly strategic communications meeting with LCC to discuss any ongoing communications matters.</p> |

Table 8 - Objections from statutory consultees

| Objection number | Objector | Themes of objection | Network Rail's position |
|------------------|----------|---|---|
| OBJ 30 | J Makin | HUL4/15 Brady Farm Bridge, in particular details of proposed temporary access | <p>Network Rail has openly engaged with this landowner since November 2021 and has previously met with them to discuss the Order, prior to this objection being submitted.</p> <p>Following the receipt of the objection, Network Rail issued a letter on 25 August 2023 addressing the concerns raised by the landowner and is currently working towards agreeing a licence agreement to mitigate them.</p> <p>Network Rail have assured the landowner that they and their tenants will maintain access to the portion of their land not used as a compound for the duration of the works, and are committed to continuing to work with the landowner on the practicalities of how this access can be managed.</p> |

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| Objection number | Objector | Themes of objection | Network Rail's position |
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| OBJ03 | S Nightingale | HUL4/21 Austhorpe Lane, in particular concerns about loss of land | <p>Following the receipt of the objection, Network Rail issued a letter on 22 August 2023 addressing the concerns raised by the landowner in OBJ03. This contained an offer for a meeting to discuss their objection in more detail, and assurance that no part of their property is included in the Order Scheme, meaning that Network Rail is not applying for any compulsory purchase powers over it, and as such do not require any temporary possession, permanent acquisition or permanent or temporary rights over it.</p> <p>Network Rail clarified that the reason the landowner was included in the Book of Reference was that works will be undertaken in the public highway adjoining the property, to facilitate the diversion of a gas main. Network Rail also assured the landowner that access to their property will be maintained at all times.</p> <p>A follow up letter was sent on 14 September to reiterate the invite to discuss the proposals in more detail.</p> |
| OBJ06 | Ashdale Lane and Property | Peckfield level crossing, in particular the extent of land included for passing places on Pit Lane and the need to retain vehicle and pedestrian access between their land to the north and south of Pit Lane | <p>Network Rail has openly engaged with this landowner since November 2021 and has previously met with them to discuss the Order, prior to this objection being submitted.</p> <p>Following the receipt of the objection, Network Rail issued a letter on 11 September 2023 addressing the concerns raised by the landowner and is currently working towards agreeing a licence agreement to mitigate them.</p> |

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|------------------|----------|--|---|
| OBJ09 | C Makin | <p>HUL4/15 Brady Farm Bridge, in particular:</p> <ul style="list-style-type: none"> • Impacts to heritage • The loss of an irrigation water pipe carried by the bridge | <p>Network Rail has openly engaged with this landowner since November 2021 and has previously met with them to discuss the Order, prior to this objection being submitted.</p> <p>Following the receipt of the objection, Network Rail has conducted negotiations with the landowner's agent and issued various letters addressing the concerns raised by the landowner in OBJ09.</p> <p>With regards to heritage, Network Rail has explained that the decision to demolish the bridge is the result of an extensive optioneering process, and that the bridge in its current form cannot accommodate the clearance required for new overhead line equipment.</p> <p>Network Rail has concluded that the demolition of the bridge can be offset by the re-use of stonework in the construction of elements of the works, particularly to heighten the parapets at Ridge Road and Austhorpe Lane Bridges. Material of historic interest removed from the structure will also be re-used elsewhere. The landowner has accepted Network Rail's explanations on this matter.</p> <p>With regards to the water pipe, it is Network Rail's position that the landowner neither has the benefit of any easement nor consent for the installation of the pipe. In addition, the landowner does not own the land on the other side of the railway, and therefore it is unclear on what basis they assert there is a need for alternative water provision for farming on that side of the</p> |

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| | | | <p>railway, and it is notable that no such concern has been raised by that landowner on the issue of water supply.</p> <p>It is Network Rail's understanding that the irrigation of the landowner's own landholding will be unaffected by the removal of the unauthorised water pipe, given the purpose of the unauthorised pipe was to supply water from their land to the land on the other side of the railway. Accordingly, there is no obligation on Network Rail to make alternative provision for an unauthorised water supply.</p> |
| OBJ19 | John Michael Chapman, Andrew Christopher Chapman and David Alan Leeming | <p>Garforth Moor Level Crossing, in particular:</p> <ul style="list-style-type: none"> • The potential for the proposalsto adversely impact potential development in the area • Maintaining access to allotment gardens • Issues with existing licence to occupy land | <p>Network Rail has previously met with this landowner to discuss the Order, prior to this objection being submitted. Following the receipt of the objection, Network Rail issued a letter on 29 September addressing the concerns raised by the landowner in OBJ19 and is currently working with the landowner to reach agreement, including on noted issues with existing licences.</p> <p>Network Rail have noted to the landowner that its proposals are restricted to a right of way along the edge of the landowner's field, and therefore unlikely to sterilise any development they wish to bring forwards. Network Rail have also confirmed that its proposals will not interfere with access to the allotments, but rather ensure access into the future.</p> |
| OBJ23 | J Parkinson | Lack of information received and difficulty understanding impacts to property | Following the receipt of the objection, Network Rail issued a letter on 27 September 2023 addressing the concerns raised by the landowner in OBJ23. This contained an offer for a meeting to |

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| | | | <p>discuss their objection in more detail, including to explain the impacts on the landowner's property.</p> <p>Following this, Network Rail followed up with this landowner to further offer a meeting should the landowner wish to discuss the proposals.</p> <p>This objection has now been withdrawn.</p> |

Table 9 - Objections from landowners

Remaining objections

15.4.6 Network Rail has written to all other objectors, who are not statutory objectors or landowners, addressing the matters raised in their objections, with an offer to meet should they wish to discuss further.

| Objection number | Order element | Themes of objection/s | Network Rail's position |
|------------------------------|--------------------------|--|--|
| OBJ04 OBJ29 | Barrowby level crossings | <ul style="list-style-type: none"> • Visual impact on local amenity and landscape • Impact on wildlife and trees • Cost of proposals • Concerns that the proposals would attract anti-social behaviour | <p>Visual impact on local amenity and landscape</p> <p>A visual appraisal of the bridge has been undertaken from two viewpoints to the north of the bridge as indicated in the Environmental Report (NR16), and mitigation measures will be put in place to reduce any visual impact, including native woodland and species rich hedgerow planting to the north and south of the railway line. The proposed planting will form a connection to existing woodland and filter views, which will help to integrate the bridge into the existing landscape.</p> |

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| Objection number | Order element | Themes of objection/s | Network Rail's position |
|------------------|---------------|---|---|
| | | <ul style="list-style-type: none"> Accessibility of bridge | <p>Impact on wildlife and trees</p> <p>Substantial work has been undertaken to identify suitable designs to limit any ecological impacts from the proposals at Barrowby Lane. For example, the footprint of the associated temporary land use (including construction compound) and new access tracks to the bridge have been designed to ensure existing mature and veteran trees are retained and protected where feasible. Where effects have been identified, appropriate mitigation and habitat enhancement measures have been incorporated into the design.</p> <p>Network Rail will make all practicable effort to minimise tree and vegetation removal in relation to the delivery of the Scheme.</p> <p>Where tree removal is unavoidable, it shall be mitigated through implementation of a detailed Landscape and Ecological Management Plan that is intended to mitigate landscape and also ecological effects.</p> <p>Cost of proposals</p> <p>Concerns around the cost of the proposals have been acknowledged by Network Rail.</p> <p>Anti-social behaviour</p> <p>The level crossings in their existing status pose a serious risk to users and cannot be maintained. Barrowby Lane and Barrowby</p> |

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| | | | <p>Foot Level Crossings must therefore be closed and replaced with a safer alternative.</p> <p>The parapets on the bridleway bridge have been designed in accordance with the appropriate requirements/ standards for the anticipated users and protection of the proposed electrification of the railway. Low-level lighting in the handrails will also be provided over the new bridleway bridge, which should help dissuade anti-social behaviour.</p> <p>Accessibility of bridge</p> <p>The new ramped bridleway bridge at Barrowby Lane has been designed in accordance with the appropriate requirements and standards for the anticipated users and compliance with accessibility guidelines.</p> |
| OBJ12 | HUL4/14 Ridge Road Bridge | <ul style="list-style-type: none"> • Objection to the demolition and structure being rebuilt in a new design whilst using the original stone • Request for the bridge to be protected, maintained or reconstructed in the original design | <p>Objection to the demolition and request to rebuild using original stone</p> <p>Network Rail notes the objection to the full demolition of Ridge Road overbridge. The bridge is required to be demolished and reconstructed to enable Network Rail to install overhead wires, to power cleaner, quieter electric trains.</p> <p>Request for the bridge to be protected, maintained or reconstructed in the original design</p> <p>During the optioneering process Network Rail examined the possibility of rebuilding the bridge entirely in stone, or with a stone</p> |

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| | | | <p>facing, at a higher level. However, due to the height needed to achieve the basket arch, it cannot be accommodated within the existing highway arrangement. There is no feasible way of reconciling the two transport elements while adhering to safety constraints.</p> <p>Mitigation will be put in place to secure the archaeological recording of the structure prior to works commencing and material of historic interest removed from the structure will be re-used elsewhere within the Scheme.</p> |
| OBJ08 | Brady Farm overbridge HUL4/15 | <ul style="list-style-type: none"> • Objection to demolition of the bridge and request that the bridge is rebuilt • Loss of Public Right of Way | <p>This objector has since removed their objection.</p> <p>Objection to demolition of the bridge</p> <p>The decision to demolish the bridge is the result of an extensive optioneering process and Network Rail has worked closely with Historic England and the conservation team at LCC in developing its proposals for the bridge.</p> <p>The bridge in its current form cannot accommodate the clearance required for new overhead line equipment, which is needed to deliver the TRU. Network Rail has concluded that the demolition of the bridge can be offset by the re-use of stonework in the construction of other works, particularly to heighten the parapets at Ridge Road and Austhorpe Lane Bridges. Material of historic interest removed from the structure will also be re-used elsewhere.</p> |

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|---|--|--|--|
| | | | <p>Public Rights of Way</p> <p>There is no public right of way over the bridge and therefore Network Rail's proposals for this structure do not result in any loss of public rights of way.</p> |
| <p>OBJ01</p> <p>OBJ21</p> <p>OBJ22</p> <p>OBJ26</p> | <p>HUL4/21</p> <p>Austhorpe</p> <p>Lane Bridge</p> | <ul style="list-style-type: none"> • Demolition of the Grade II listed bridge • Highway design and safety, including concerns over the road being made two-lane, pedestrian facilities, and parapet and kerb heights • Request for cycle provision • Impact on community woodland • Insufficient consultation, and detailed queries about proposals • Alternatives considered for gas main diversion | <p>Objection to demolition of bridge</p> <p>The decision to demolish the bridge is the result of an extensive optioneering process. Network Rail has worked closely with Historic England and the conservation team at LCC in developing the Scheme. Network Rail acknowledges that there will be loss of designated assets as part of the scheme, but this has been limited through appropriate design solutions.</p> <p>Highway design and safety</p> <p>Network Rail has engaged extensively with LCC on the design for the replacement bridge, and the design of the new road has been reviewed by an independent Road Safety Audit Team in accordance with recognised National Standards.</p> <p>The proposed alignment provides two lanes across the bridge and design details have been included to improve road safety and to reduce the risks of head-on collisions. The lanes are slightly narrower than standard to encourage lower speeds across the bridge. As part of the works, two new road cushions (speed humps) will also be constructed to the south of the new bridge to discourage speeding vehicles. These will be constructed in</p> |

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| | | | <p>accordance with LCC guidance and will deter speeding vehicles but provide a smooth route for cyclists.</p> <p>Request for cycle provision</p> <p>A 3m wide shared use cycleway/ footpath is not required on the replacement bridge; it is not in place on the existing bridge and the proposed bridge will not be wide enough to accommodate one.</p> <p>Impact on community woodland</p> <p>An ecological appraisal has been prepared and is included in the Environmental Report (NR16). To inform the ecological appraisal, ecological surveys of habitats and species have been undertaken at Austhorpe Lane and the surrounding area, including the woodland. Survey results have been used to inform the potential ecological impacts and effects of the proposed works, identify mitigation measures and residual effects.</p> <p>The southern compound required to support the gas main and bridge replacement works at Austhorpe Lane has been extended into the grassland area to the east of the woodland block to reduce the loss of priority woodland as far as practicable and to retain trees with bat roost suitability.</p> <p>Insufficient consultation and queries about proposals</p> <p>A public consultation took place between 24 October and 18 November 2022. Details of the Scheme proposals were displayed</p> |

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| | | | <p>on Network Rail's virtual consultation room, and plans and information were also available at three in person events held in Garforth, Micklefield and Crossgates. Flyers, posters and social media adverts were also used to inform the local community about the Scheme and promote the public consultation.</p> <p>For a summary of consultation undertaken, please refer to the Consultation Report (NR07).</p> <p>Where objectors have asked for clarification about elements of the proposals, this has been provided via letter and email.</p> <p>Alternatives considered (gas main diversion)</p> <p>A number of options were considered for the diversion, in conjunction with Northern Gas Networks. The chosen option performs best in terms of cost, disruption to local communities, safety, security risk, and long-term maintenance. This has been carefully balanced against the impact on trees.</p> |
| OBJ20 | Public Rights of Way | <ul style="list-style-type: none"> Holding objection to the changes to the Public Rights of Way (PRoW) | <p>The objector noted their objection was a holding objection pending a final decision on their position on various changes to public rights of way affected by the Order, if confirmed. Network Rail advised via letter that details of any changes to footpaths are included in the Order application and advised where they could be viewed, and offered a follow-up meeting.</p> |

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|------------------|----------------------|--|--|
| OBJ29 | Public Rights of Way | <ul style="list-style-type: none"> • Difficulty in understanding plans and proposals, and insufficient consultation on them • Need for high quality diversion routes when Public Rights of Way are closed during construction (HUL4/20 Crawshaw Woods Bridge specifically mentioned) | <p>Difficulty in understanding plans and proposals, and insufficient consultation on them</p> <p>Network Rail explained each of the proposals specified in the objector's letter, explaining why they were needed for the Scheme, and where more information could be found.</p> <p>Public consultation took place between 24 October and 18 November 2022. Details of the Scheme proposals were displayed on Network Rail's virtual consultation room, and plans and information were also available at three in person events held in Garforth, Micklefield and Crossgates. Flyers, posters and social media adverts were also used to inform the local community about the Scheme and promote the public consultation.</p> <p>For a summary of consultation undertaken, please refer to the Consultation Report (NR07).</p> <p>Public Rights of Way</p> <p>Diversionary routes for Public Rights of Way will be agreed with LCC in advance of construction work and detailed in a Construction and Traffic Management Plan, which will be submitted to and approved by LCC. Disruption will be minimised wherever possible. All diversion routes will be designed in accordance with the appropriate design criteria and appropriate signage will be used.</p> |

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| Objection number | Order element | Themes of objection/s | Network Rail's position |
|------------------|--------------------------|--|---|
| OBJ02 | Peckfield Level Crossing | <ul style="list-style-type: none"> • Objection to closure in general | <p>Need for closure</p> <p>Network Rail is responsible for maintaining the railway network, including ensuring the safety of passengers. The TRU will bring faster, more frequent trains to the line and therefore the level crossing in its existing status poses a serious risk to users and cannot be maintained. Peckfield Level Crossing must therefore be closed and replaced with a safer alternative.</p> <p>Network Rail has included two options for the closure of the level crossing. The decision to include both options has been taken by Network Rail as a result of feedback received during the consultation process. One option would see a footpath provided on the north side of the railway, connecting the Railway Cottages to Great North Road. The other option would see the creation of a bridleway on the north side of the railway, running through the recreation ground. Both options provide a safe, convenient, accessible (level), alternative route, via existing footways on Pit Lane, Great North Road and either a new footpath or bridleway through the recreation ground.</p> <p>Provision of a footbridge</p> <p>Regarding the provision of a footbridge, a bridleway bridge or stepped footbridge to replace Peckfield Level Crossing was considered but these options were not taken forward due to concerns over visual impact, particularly on nearby residents, and the land take required. It was also noted that user surveys have recorded low level crossing usage at Peckfield Level Crossing by persons of reduced mobility and zero usage by equestrians. As</p> |
| OBJ05 | | <ul style="list-style-type: none"> • Desire for a footbridge to be provided | |
| OBJ10 | | <ul style="list-style-type: none"> • Scepticism about user survey results | |
| OBJ11 | | <ul style="list-style-type: none"> • Need to consider future development | |
| OBJ12 | | <ul style="list-style-type: none"> • Sustainability, climate change and increased noise | |
| OBJ13 | | <ul style="list-style-type: none"> • Impacts on recreation ground, including safety | |
| OBJ14 | | <ul style="list-style-type: none"> • Preference for bridleway extension | |
| OBJ15 | | <ul style="list-style-type: none"> • Impact on train services | |
| OBJ16 | | | |
| OBJ17 | | | |
| OBJ18 | | | |
| OBJ24 | | | |
| OBJ25 | | | |
| OBJ27 | | | |
| OBJ28 | | | |

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| Objection number | Order element | Themes of objection/s | Network Rail's position |
|------------------|---------------|-----------------------|--|
| | | | <p>such, it is Network Rail's view that the existing level crossing usage does not justify provision of a bridleway or stepped footbridge at this location.</p> <p>User surveys</p> <p>User surveys of the level crossing were undertaken in 2014, 2021 and 2023. These surveys were typically undertaken over a consecutive seven day period and an additional origin – destination survey was undertaken in 2023.</p> <p>Consideration of future development</p> <p>Network Rail can confirm that committed schemes and developments with planning approval have been considered in the development of the preferred option, as set out in the Planning Statement (NR13) submitted with the Order.</p> <p>It is also important to note that proposals need to be appropriate for the established current need. If future developments come forward that drive different requirements for recreation/ amenity or connectivity in the area, it should be those developments that are tasked with providing the corresponding improvement.</p> <p>Safety concerns about route on Great North Road</p> <p>In response to comments raised that the alternative routes increase hazards for bridleway users, a safety assessment has been undertaken to consider the safety of highway users along</p> |

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| Objection number | Order element | Themes of objection/s | Network Rail's position |
|------------------|---------------|-----------------------|--|
| | | | <p>one of the alternative routes along Great North Road once the level crossing is closed.</p> <p>Sustainability, climate change and increased noise</p> <p>Network Rail benefits from Permitted Development in running trains on the existing infrastructure and can intensify or reduce rail use as is applicable. In any event, it is not expected the new rolling stock will generate a significant operational noise increase. More information on how sustainability and climate change have been considered is included in the Environmental Report (NR16).</p> <p>Impacts on recreation ground</p> <p>In response to comments raised that the bridleway route through the recreation ground is not appropriate, level crossing survey data indicates there is low usage of the existing route by horses and cyclists. The alternative routes proposed have also been deliberately routed along the edge of the recreation ground to minimise conflict between organised recreational activities and use of the bridleway. As mentioned previously, the use of the current crossing by riders is extremely low which in itself will minimise interference with other users of the recreation ground.</p> <p>In addition, neither route will impede access to the Railway Cottages used as sports changing rooms.</p> |

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| Objection number | Order element | Themes of objection/s | Network Rail's position |
|------------------|---------------|-----------------------|--|
| | | | <p>Bridleway extension</p> <p>Network Rail previously considered and consulted on the provision of a new bridleway to the west to the A656 Ridge Road. This option was discounted on the grounds of safety following a Road Safety Review and discussions with LCC, particularly because a Pegasus crossing of the A656 Ridge Road with a fixed crossing movement was unfeasible, due to the road speed and insufficient width to accommodate a shared footway safely.</p> <p>Impact on train services</p> <p>Network Rail's proposals are to remove the existing bridleway crossing of the railway line at Peckfield Level Crossing, diverting existing users via Great North Road. These proposals will not interrupt train services, or lead to additional waiting times for trains along the line.</p> |

Table 10 - All other objections

Representations

15.4.7 Table 10 below identifies the key themes of the representations made to the Order. It also summarises the steps which are being taken by Network Rail in response to the representations.

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| Representation number | Representation | Themes of representation | Network Rail's position |
|-----------------------|----------------------------|---|---|
| REP01 | D Graham | <p>Neville Hill Newmarket Approach, including:</p> <ul style="list-style-type: none"> • Need for alternative signed routes while the Public Right of Way is closed • The need to minimise the length of any Public Right of Way closure | <p>Following the receipt of the representation, Network Rail issued a letter on 27 September 2023 addressing the concerns raised in REP01. This contained an offer for a meeting to discuss their representation in more detail.</p> <p>Diversion routes for the works will be agreed with LCC in advance of construction work and appropriate signage will be used for all diversion routes.</p> |
| REP02 | Environment Agency | Flood risk, dewatering and drainage | <p>Network Rail has consulted and engaged with the Environment Agency on a regular basis pre submission of the Order.</p> <p>The Environment Agency was not satisfied with specific wording in the draft Order, related to protective provisions and requirements for FRAP. Network Rail issued a letter on 25 September 2023 to address the concerns raised. Following this letter, the Environment Agency has now withdrawn its representation.</p> |
| REP03 | Micklefield Parish Council | <ul style="list-style-type: none"> • Demolition of HUL4/14 Ridge Road Bridge (heritage) and concerns about the road closures needed for construction • Peckfield Level Crossing, including the need for a footbridge or an underpass and questioning the need | <p>Network Rail has consulted and engaged with the Parish Council prior to this representation being submitted, including during the consultation in November 2022. Following the receipt of the representation, Network Rail issued a letter on 11 October 2023 addressing the concerns raised by the Parish Council. This contained an offer for a meeting to discuss their objection in more detail.</p> <p>With regards to Ridge Road Bridge, it needs to be demolished and reconstructed to enable the installation of overhead wires. Road closures of the A656 will be required to enable this, and Network Rail will work</p> |

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| Representation number | Representation | Themes of representation | Network Rail's position |
|-----------------------|----------------|---|---|
| | | <p>for a replacement bridleway, objecting to it running through the recreation ground</p> <ul style="list-style-type: none"> • Need for security at Phoenix Avenue compound, and site reinstatement once use is complete | <p>with LCC to minimise inconvenience to the local community and businesses when planning this road closure and the length of closure would be kept to a minimum.</p> <p>With regards to Peckfield Level Crossing, Network Rail has included two options for its closure, only one of which includes a bridleway through the recreation ground. It is Network Rail's position that both options provide a safe, convenient, accessible (level), alternative route, and do not pose new risks for recreation ground users. Network Rail can also confirm that committed schemes and developments with planning approval have been considered in the development of the options.</p> <p>Neither route will impede access to the Railway Cottages used by the Parish Council as sports changing rooms.</p> <p>Regarding the provision of a footbridge, a bridleway bridge or stepped footbridge to replace Peckfield Level Crossing was considered but these options were not taken forward due to concerns over visual impact, particularly on nearby residents, and the land take required. It was also noted that user surveys have recorded low level crossing usage at Peckfield Level Crossing by persons of reduced mobility and zero usage by equestrians. As such, it is Network Rail's view that the existing level crossing usage does not justify provision of a bridleway or stepped footbridge at this location. With regards to the suggestion for an underpass as an alternative option, an underpass was not considered for this area as the existing topography does not lend itself to that as an option.</p> |

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| Representation number | Representation | Themes of representation | Network Rail's position |
|-----------------------|----------------|---|--|
| | | | With regards to Phoenix Avenue Compound, Network Rail will take appropriate security measures at the site and it will be reinstated to its previous conditions once works are completed. |
| REP04 | Mann | <ul style="list-style-type: none"> Peckfield Level Crossing, in particular a new bridleway crossing the Recreation Ground (safety and cost), and the desire for a bridge or underpass Road closures required for HUL4/14 Ridge Road Bridge Support for Highroyds Wood Level Crossing proposals | <p>Following the receipt of the representation, Network Rail issued a letter on 18 September 2023 addressing the concerns raised in REP04. This contained an offer for a meeting to discuss their representation in more detail.</p> <p>With regards to Peckfield Level Crossing, Network Rail has included two options for its closure, only one of which includes a bridleway through the recreation ground. It is Network Rail's position that both options provide a safe, convenient, accessible (level), alternative route, and do not pose new risks for recreation ground users.</p> <p>Regarding the provision of a footbridge, a bridleway bridge or stepped footbridge to replace Peckfield Level Crossing was considered but these options were not taken forward due to concerns over visual impact, particularly on nearby residents, and the land take required. It was also noted that user surveys have recorded low level crossing usage at Peckfield Level Crossing by persons of reduced mobility and zero usage by equestrians. As such, it is Network Rail's view that the existing level crossing usage does not justify provision of a bridleway or stepped footbridge at this location. With regards to the suggestion for an underpass as an alternative option, an underpass was not considered for this area as the existing topography does not lend itself to that as an option.</p> <p>With regards to Ridge Road Bridge, it needs to be demolished and reconstructed to enable the installation of overhead wires. Road closures</p> |

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| Representation number | Representation | Themes of representation | Network Rail's position |
|-----------------------|-------------------|---|--|
| | | | of the A656 will be required to enable this, and Network Rail will work with LCC to minimise inconvenience to the local community and businesses when planning this road closure and the length of closure would be kept to a minimum. |
| REP05 | Royal Mail | Potential impacts on Royal Mail's operations and assets | Network Rail has consulted Royal Mail prior to receiving this representation letter, including technical consultation in October 2022. Network Rail intends to work with Royal Mail to agree a letter of commitment that reflects to points raised in the representation. |
| REP06 | National Highways | Request for protective provisions to be included in the Order | Following the receipt of the representation, Network Rail has written a response letter to National Highways and started the process of agreeing a Side Agreement with them (instead of the inclusion of protective provisions). |
| REP07 | Coal Authority | Need to consider, investigate and remediate risk posed by coal mining features in close proximity to construction works | As part of the early stages of the TRU and following the receipt of Network Rail Stage 2 Mining Reports for the Leeds to Micklefield line, Network Rail has undertaken several Mining Risk Assessments (MRA) and Mining Risk Mitigation Reports (MRMR). The outcome of these reports was that further investigation was required for much of the line. Under the guidance and permission from several active Coal Authority Permits, Network Rail has been undertaking mining ground investigations. All outcomes of these investigations will be recorded in Ground Investigation Reports. Mitigation measures will be recorded in updated MRMR reports. All documents produced in compliance with the relevant coal authority permit can be provided to the Coal Authority and Lead Local Planning Authority for reference, where requested. |

Table 11 - Key themes from representations

Statements of support

15.4.8 Network Rail has received three letters of support. The key themes that are set out in letters of support are as follows.

- Supportive of the Transpennine Route Upgrade and the benefits it will bring.
- Supportive of the closure of level crossings.

16. NOTICE AS REGARDS CORE DOCUMENTS

- 16.1.1 The list of documents appended to this Statement is a list of those documents which Network Rail currently intends to refer to or put in evidence at the Inquiry. Those documents are available for public inspection free of charge until the commencement of the Inquiry at the Leeds Central Library, Municipal Buildings, Calverley Street, Leeds, LS1 3AB and Garforth Library and One Stop Centre, Lidgett Lane, Garforth, Leeds, LS25 1EH.
- 16.1.2 As opening times are subject to change, please click on the following link <https://www.leeds.gov.uk/libraries/central-library> or <https://www.leeds.gov.uk/community-hubs/garforth> to check opening times of the library.
- 16.1.3 Copies of the documents can also be viewed and downloaded from Network Rail's website <https://www.networkrail.co.uk/running-the-railway/railway-upgrade-plan/key-projects/transpennine-route-upgrade/leeds-to-micklefield-upgrades/>.
- 16.1.4 Copies of the Statement of Case, or other documents which may be served on or by Network Rail in accordance with Rule 7 of the TWA Inquiries Rules, may be inspected free of charge and, where practicable and subject to payment of a reasonable charge be obtained from Penny Carter, Network Rail Infrastructure Limited, 3rd Floor, Square One, 4 Travis Street, Manchester M1 2NY (telephone: 0161 880 3141).

Appendices

Appendix 1 – Core Documents List

| Reference | Description of document |
|-----------|---|
| NR01 | Copy of the Application letter |
| NR02 | Draft Order |
| NR03 | Explanatory Memorandum |
| NR04 | Statement of Aims |
| NR05 | Funding Statement |
| NR06 | Estimate of Costs |
| NR07 | Consultation Report |
| NR08 | Book of Reference |
| NR09 | Land Plans |
| NR10 | EIA Screening Decision |
| NR11 | Rule 18 Waiver |
| NR12 | Request for Deemed Planning Permission |
| NR13 | Planning Statement |
| NR14 | Planning Drawings |
| NR15 | Design and Access Statement |
| NR16 | Environmental Report |
| NR17 | Code of Construction Practice |
| NR18 | List of Consents, Permission or licences under other enactments |
| NR19 | Statement of Case |
| NR20 | Kirkgate Prior Approval Decision Letter (23/03890/DPD) |

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| Reference | Description of document |
|-----------|---|
| NR21 | Osmondthorpe Lane Prior Approval Decision Letter (23/00903/DPD) |
| NR22 | EIA Screening Opinion |
| NR23 | Enhancing Level Crossing Safety 2019- 2029, A long term strategy targeting improved safety on Great Britain's railway |
| NR24 | Principles for managing level crossing safety - published on 15th June 2021 |
| NR25 | Strategy for regulation of health and safety risks – 4: level crossings |
| NR26 | Network Rail Work Instruction NR/L3/XNG/308 'Risk assessing level crossings |
| NR27 | The Integrated Rail Plan (2021) |
| NR28 | Rail Network Enhancement Pipeline – Department for Transport (2019) |
| NR29 | National Infrastructure Delivery Plan (2016) |
| NR30 | Williams-Shapps Plan for Rail – HM Government (2021) |
| NR31 | Strategic Transport Plan, Transport for the North (2019) |
| NR32 | West Yorkshire Transport Strategy 2040 |
| NR33 | Leeds City Region Strategic Economic Plan 2016-2032 |
| NR34 | North Yorkshire County Council Local Transport Plan 2016 – 2045 |
| NR35 | National Planning Policy Framework (NPPF), DCLG (2012 & updated 2023) |
| NR36 | Leeds Core Strategy (as amended by the Core Strategy Selective Review 2019) (adopted 2019) |
| NR37 | Leeds Unitary Development Plan Review 'Saved' Policies (adopted 2006) |

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| Reference | Description of document |
|-----------|---|
| NR38 | Site Allocations Plan (adopted 2019) |
| NR39 | Natural Resources and Waste Local Plan (adopted 2013) |
| NR40 | The Garforth Neighbourhood Plan |
| NR41 | The Leeds Local Plan Update Publication Draft Consultation 2022, Your Neighbourhood, Your City, Your Planet |
| NR42 | Accessible Leeds SPD |
| NR43 | National Policy Statement for National Networks 2015 |
| NR44 | Open Space Certificate Letter |
| NR45 | Decision Letter for The Network Rail Huddersfield to Westtown Dewsbury Orde |
| NR46 | Alternative Options Evaluation Studies |
| NR47 | Heritage Statements |

Appendix 2 – Table of Elements of Order Scheme

TRU LEEDS TO MICKLEFIELD– SUMMARY OF PRINCIPLE ELEMENTS


| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|---|--|---|--|--|--|
| Kirkgate Viaduct Temporary Compound and Construction Land  | <p>Temporary use for construction and access purposes.</p> <p>To facilitate the carrying out of works to Kirkgate Viaduct permitted by Permitted Development and Prior Approval pursuant to Part 18 (23/03890/DPD) submitted on 23 June 2023 and approved on 16 August 2023</p> | <p>Article 28 (<i>Temporary use of land for construction of works</i>)</p> <p>Article 30 (<i>Temporary use of land for access</i>)</p> <p>Article 12 (<i>Temporary stopping up of streets</i>)</p> | <p>Sheet 1</p> <p>1-001 to 1-004⁶</p> <p>1-005 to 1-021, 1-024 to 1-039⁷,</p> | <p>No</p> | <p>Yes</p> <p>Part 4 (Class A)</p> | <p>No</p> |

⁴ Note – only references relevant primary provisions


⁵ The Town and Country Planning (General Permitted Development) (England) Order 2015



⁶ Temporary access for construction

⁷ Temporary possession and access for construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|---|--|---|---|---|--|--|
| | | | | | | |
| <p>Kirkgate to Marsh Lane (otherwise known as Penny Pocket Park works)</p>  | Permanent acquisition of land for installation of electrification and signalling infrastructure, on side of railway. | <p>Article 7 (<i>Power to construct and maintain works</i>)</p> <p>Article 22 (<i>Power to acquire land</i>)</p> <p>Article 38 (<i>Open Space Land</i>)</p> <p>Article 39 (<i>Power to operate and use railway</i>)</p> | <p>Sheet 1</p> <p>2-001 2-002 2-003 2-004⁸</p> | <p>Yes</p> <p>Document NR14, plan references: 151666-TRA-00-HUL4-DRG-R-SG-260001, 151666-TRA-00-HUL4-DRG-R-SG-260002, 151666-TRA-00-HUL4-DRG-R-SG-260003, 151666-TRA-00-HUL4-DRG-R-SG-260004, 151666-TRA-00-HUL4-</p> | No | N/A |

⁸ Permanent Acquisition


| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|---|--|--|--|--|--|
| | | | | DRG-R-SG-260005, 151666-TRA-00-HUL4-DRG-R-SG-260006 | | |
| Marsh Lane Compound and Marsh Lane Construction Land  | <p>Temporary use of land for compound and construction purposes.</p> <p>Land required to facilitate the replacement deck of Marsh Lane Viaduct.</p> | <p>Article 28 (<i>Temporary use of land for construction of works</i>)</p> <p>Article 30 (<i>Temporary use of land for access</i>)</p> <p>Article 12 (<i>Temporary stopping up of streets</i>)</p> | <p>Sheet 1</p> <p>3-001 to 3-016</p> | <p>No</p> | <p>Yes</p> <p>Part 4 (Class A)</p> | <p>N/A</p> |
| Neville Hill Access Land | <p>Permanent use of land off Newmarket Approach to provide access to the Neville Hill railway sidings, with associated temporary</p> | <p>Article 12 (<i>Temporary Stopping up of streets</i>)</p> | <p>Sheet 2</p> <p>4-002</p> | <p>No</p> | <p>Yes</p> <p>Part 4 (Class A)</p> | <p>N/A</p> |

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|--|--|--|--|--|--|
|  | <p>land use for construction purposes.</p> <p>Separate planning permission being sought for Nevill Hill access works 23/03522/FU for construction of a new access road and shared-use footpath/cycleway.</p> | <p>Article 22 (<i>Power to Acquire Land</i>)</p> <p>Article 28 (<i>Temporary use of land for construction of works</i>)</p> | <p>4-005⁹</p> <p>4-001</p> <p>4-003</p> <p>4-004¹⁰</p> | | | |
| <p>Osmandthorpe Lane Compound</p>  | <p>Temporary use for construction and access purposes.</p> <p>To facilitate the construction of a replacement underbridge at Osmondthorpe Lane pursuant to Permitted Development and Prior</p> | <p>Article 12 (<i>Temporary stopping up of streets</i>)</p> <p>Article 28 (<i>Temporary use of land for construction of works</i>)</p> | <p>Sheet 3</p> <p>5-001 to 5-009¹¹</p> | <p>No</p> | <p>Yes</p> <p>Part 4 (Class A)</p> | <p>N/A</p> |


⁹ Temporary Possession and Temporary Stopping Up

¹⁰ Permanent Acquisition

¹¹ Temporary possession and access for construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|---|--|--|--|--|--|
| | Approval pursuant to Part 18 application ref. 23/00903/DPD issued 2 May 2023 | | | | | |
| Wykebeck Avenue Temporary Compound  | Temporary use of land for a compound (including storage) to enable track renewal and overhead line clearance works. | Article 28 (<i>Temporary use of land for construction of works</i>) Article 13 (<i>Access to works</i>) | Sheet 3 6-001 to 6-004 ¹² | No | Yes Part 4 (Class A) | N/A |
| Realignment of Austhorpe Lane Bridge | Scheduled Work No. 1 Land within the limits of deviation for Work No. 1 can be acquired permanently, used | Article 9 (<i>Power to execute street works</i>) Article 10 (<i>Stopping up of streets</i>) | Sheet 4 7-004A, 7-007, 7-007A, 7-008, 7-010, 7-012, 7-013A, 7-0013B, 17-015, 7- | Yes Document NR14, drawings: 15166-TRA-00-HUL4-DRG-R-SG-210001, 15166- | No | Yes 23/0487/LI Listed building consent for the demolition of the existing |


¹² Temporary possession and access for construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|---|---|--|---|---|--|---|
|  | <p>temporarily or new rights acquired.</p> <p>Temporary use of land as a compound outside of the limits of deviation for Work No. 1</p> | <p>Article 12 (<i>Temporary stopping up of streets</i>)</p> <p>Article 13 (<i>Access to works</i>)</p> <p>Article 14 (<i>Construction and maintenance of new or altered streets</i>)</p> <p>Article 15 (<i>Construction of Bridges</i>)</p> <p>Article 22 (<i>Power to acquire land</i>)</p> <p>Article 28 (<i>Temporary use of land for</i></p> | <p>0015A, 7-016A, 17-016B¹³</p> <p>7-003A¹⁴</p> <p>7-006A, 7-014 and 7-017¹⁵</p> | <p>TRA-00-HUL4-DRG-R-SG-210002, 15166-TRA-00-HUL4-DRG-R-SG-210003, 15166-TRA-00-HUL4-DRG-R-SG-210004, 15166-TRA-00-HUL4-DRG-R-SG-210005, 15166-TRA-00-HUL4-DRG-R-SG-210006, 15166-TRA-00-HUL4-DRG-R-SG-210007</p> | | <p>Austhorpe Lane bridge (including the adjacent footbridge and gas pipeline) and a new bridge erected in its place</p> |

¹³ Works No. 1 Limit of Deviation

¹⁴ Temporary Possession and Temporary Stopping Up



¹⁵ Temporary possession and access for construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|--|---|---|--|--|--|
| | | <i>construction of works)</i> | | | | |
| Austhorpe Lane Gas Main Diversion  | <p>Scheduled Work No. 2</p> <p>Land within the limits of deviation for Work No. 2 can be acquired permanently, used temporarily or new rights acquired.</p> <p>Temporary use of land as a compound outside of the limits of deviation for Work No. 2</p> | Same as above for Works No. 1 | <p>7-001, 7-002, 7-003, 7-004, 7-004A, 7-005, 7—05A, 17-007, 17-007A, 7-008, 17-008A, 7-010, 7-011, 7-012, 7-012A, 7-013A, 7-015, 7-015A, 7-016, 7-016A, 7-016C, 17-017C 7-017, 7-017A¹⁶</p> <p>7-003A¹⁷</p> <p>7-006A, 7-014 and 7-017¹⁸</p> <p>Sheet 5</p> | <p>Yes</p> <p>Same drawings as referenced above for Works No. 1</p> | No | No |
| Manston Lane Compound | Temporary use of land for a compound to facilitate renewal of tracks, store | Article 28 (<i>Temporary use of land for</i> | 8-001 | No | Yes Part 4 (Class A) | No |

¹⁶ Work No. 2 Limit of Deviation


¹⁷ Temporary possession and temporary stopping up

¹⁸ Temporary possession and access for construction


| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|--|--|--|---|--|--|
|  | materials and provide welfare facilities for works | <i>construction of works)</i> | 8-003 8-003A 8-005 ¹⁹ | | | |
| Crawshaw Woods Bridge Raising  | <p>Scheduled Work No. 3</p> <p>Land within the limits of deviation for Work No. 3 can be acquired permanently, used temporarily or new rights acquired.</p> <p>Temporary use of land as a compound outside of the limits of deviation for Work No. 3</p> | <p>Article 12 (<i>Temporary stopping up of streets</i>)</p> <p>Article 15 (<i>Construction of bridges</i>)</p> <p>Article 22 (<i>Power to acquire land</i>)</p> <p>Article 28 (<i>Temporary use of land for construction of works</i>)</p> | <p>Sheet 5</p> <p>8-004, 8-007, 8-008, 8-009, 8-010²⁰</p> | <p>Yes</p> <p>Document NR14, drawings: 15155-TRA-00-HUL-DRG-R-SG-220001, 15155-TRA-00-HUL-DRG-R-SG-220002, 15155-TRA-00-HUL-DRG-R-SG-220003, 15155-TRA-00-HUL-DRG-R-SG-220004, 15155-TRA-00-HUL-DRG-R-SG-</p> | No | Yes |

¹⁹ Temporary Possession and Access for Construction

²⁰ Limits of deviation for Works No. 3

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|--|---|--|---|--|--|
| | | | | 220005, 15155-TRA-00-HUL-DRG-R-SG-220006 | | |
| Crawshaw Woods Bridge North and South Compound | Temporary use of land to facilitate raising works to Crawshaw Woods Bridge | Article 28 (<i>Temporary use of land for construction of works</i>) | Sheet 5 8-002, 8-004A, 6-006, 8-007A, 8-009A, 8-0010A | No | Yes Part 4 (Class A) | N/A |
| New Barrowby Lane Bridge Closure of Barrowby Lane LC & Barrowby Foot LC  | Scheduled Work No. 4 Land within the limits of deviation for Work No. 4 can be acquired permanently, used temporarily or new rights acquired. Temporary use of land for access and construction area outside of the limits of deviation for Work No. 4 | Article 7 (<i>Power to construct and maintain works</i>) Article 15 (<i>Construction of Bridges</i>) Article 17 (<i>Replacement and closure of level crossings</i>) Article 10 (<i>Stopping up of streets</i>) | Sheet 6 and Sheet 7 9-001 to 9-006 9-007A 9-009A ²¹ 9-001A 9-005A to 9-005E 9-007 9-008 9-008A 9-009 9-009B | Yes (new bridge only) Document NR14, drawings: 15155-TRA-00-HUL-DRG-R-SG-230001, 15155-TRA-00-HUL-DRG-R-SG-230002, 15155-TRA-00-HUL-DRG-R-SG-230003, 15155-TRA-00-HUL- | No | N/A |

²¹ Works Limit of Deviation


| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|---|---|---|--|--|--|
| | Extinguishment of rights over Barrowby Lane level crossing and Barrowby Lane Foot level crossing | Article 28 (<i>Temporary use of land for construction works</i>) | 9-009C 9-010 9-013 9-013A to 9-013G ²² 9-011 9- 012 ²³ | DRG-R-SG-230004, 15155-TRA-00-HUL-DRG-R-SG-230005, 15155-TRA-00-HUL-DRG-R-SG-230006 | | |
| Garforth Moor Level Crossing  | <p>Extinguishment of existing rights over Garforth Moor Level Crossing</p> <p>New right of access (as substitute to closure of Garforth Moor Level Crossing) together with ability to upgrade and</p> | <p>Article 10 (<i>Stopping up of Streets</i>)</p> <p>Article 18 (<i>Accommodation crossings</i>)</p> <p>Article 25 (<i>Power to acquire new rights</i>)</p> | <p>Sheet 7</p> <p>10-001 to 10-004²⁴</p> <p>10-005²⁵</p> | No | Yes | No |

²² Temporary Possession and Access for Construction

²³ Extinguishment of Rights


²⁴ Permanent Right of Access

²⁵ Extinguishments of Rights

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|---|---|---|---|--|--|---|
| | maintain new access route and ability to park. In relation to new access route an application for retrospective planning permission (ref:22/03144/FU) has been submitted for access track and turning head to service adjacent allotments. | | | | | |
| Brady Farm Bridge Temporary Compound  | Temporary use of land as a compound to facilitate the demolition works. Extinguishment of private rights over Brady Farm Bridge | Article 18 (<i>Accommodation crossings</i>) Article 12 (<i>Temporary stopping up of streets</i>) Article 28 (<i>Temporary use of land for construction works</i>) | Sheet 8 11-012A to 11-012C 11-014 11-015 ²⁶ 11-013 ²⁷ | No | Part 11 | Yes 23/04389/LI Listed Building Consent for the demolition of Brady Farm overbridge (HUL4/15) |

²⁶ Temporary Possession and Access for Construction


²⁷ Permanent Acquisition/Extinguishment of Rights

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|--|--|---|---|--|--|
| Replacement Ridge Road Bridge  | <p>Scheduled Work No. 5</p> <p>Land within the limits of deviation for Work No. 5 can be acquired permanently, used temporarily or new rights acquired.</p> <p>Temporary use of land for access and construction area outside of the limits of deviation for Work No. 5.</p> | <p>Article 7 (<i>Power to construct and maintain works</i>)</p> <p>Article 9 (<i>Power to execute street works</i>)</p> <p>Article 10 (<i>Stopping up of streets</i>)</p> <p>Article 11(<i>Power to alter layout etc of streets</i>)</p> <p>Article 14 (<i>Construction and maintenance of new or altered streets</i>)</p> | <p>Sheet 9</p> <p>11-001 to 11-004 11-005A 11-012A 11-017²⁸</p> <p>11-005 to 11-012²⁹</p> <p>Sheet 10 11-012A 11-016³⁰</p> | <p>Yes</p> <p>Drawing Number: 151666-TRA-00-HUL4-DRG-R-SG-240001, 151666-TRA-00-HUL4-DRG-R-SG-240002, 151666-TRA-00-HUL4-DRG-R-SG-240003, 151666-TRA-00-HUL4-DRG-R-SG-240004, 151666-TRA-00-HUL4-DRG-R-SG-240005, 151666-TRA-00-HUL4-DRG-R-SG-240006.</p> | <p>No</p> | <p>Yes</p> <p>23/04390/LI for the demolition of the existing Ridge Road bridge a new bridge erected in its place</p> |

²⁸ Temporary Possession and Access for Construction


²⁹ Works Limit of Deviation

³⁰ Temporary Possession and Access for Construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|---|--|--|---|---|--|--|
| | | Article 15 (<i>Construction of bridges</i>) Article 22 (<i>Power to acquire land</i>) Article 28 (<i>Temporary use of land for construction of works</i>) | | | | |
| Ridge Road Gas Diversion  | <p>Scheduled Work No. 6</p> <p>Land within the limits of deviation for Work No. 6 can be acquired permanently, used temporarily or new rights acquired.</p> <p>Temporary use of land for access and construction</p> | <p>Article 7 (<i>Power to construct and maintain works</i>)</p> <p>Article 9 (<i>Power to execute street works</i>)</p> <p>Article 10 (<i>Stopping up of streets</i>)</p> | <p>Sheet 9</p> <p>11-001 to 11-004 11-005A 11-012A 11-017 ³¹</p> <p>11-005 to 11-012 ³²</p> <p>Sheet 10</p> | <p>Yes</p> <p>Drawing Number: 151666-TRA-00-HUL4-DRG-R-SG-240001, 151666-TRA-00-HUL4-DRG-R-SG-240002, 151666-TRA-00-HUL4-DRG-R-SG-240003, 151666-</p> | No | No |



³¹ Temporary possession and access for construction


³² Work limits of deviation

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|---|--|---|--|--|--|
| | area outside of the limits of deviation for Work No. 6 | Article 11 (<i>Power to alter layout etc of streets</i>) Article 14 (<i>Construction and maintenance of new or altered streets</i>) Article 22 (<i>Power to acquire land</i>) Article 28 (<i>Temporary use of land for construction of works</i>) | 11-012A 11-016 ³³ | TRA-00-HUL4-DRG-R-SG-240004, 151666-TRA-00-HUL4-DRG-R-SG-240005, 151666-TRA-00-HUL4-DRG-R-SG-240006. | | |
| Phoenix Avenue Temporary Compound  | Temporary possession and access for construction to support a range of TRU works. | Article 28 (<i>Temporary use of land for construction of works</i>) Article 30 (<i>Temporary use of land for</i> | Sheet 11 12-013 12-014A ³⁴ | No | Yes Part 4 (Class A) | No |

³³ Temporary possession and access for construction

³⁴ Temporary Possession and Access for Construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|---|--|--|--|---|--|--|
| | | <i>construction for access)</i> | | | | |
| Micklefield Track Sectioning Cabin  | New TSC needed to enable electrification of the line, to safely regulate the power supply to the overhead electric wires in the local area. It will be located 50 metres to the west of the existing Peckfield Level Crossing. | Article 7 (<i>Power to construct and maintain works</i>) Article 22 (<i>Power to acquire land.</i>) | Sheet 11 12-014 | Yes Drawing Number: 151666-TRA-00-HUL4-DRG-R-SG-250001, 151666-TRA-00-HUL4-DRG-R-SG-250002, 151666-TRA-00-HUL4-DRG-R-SG-250006, 151666-TRA-00-HUL4-DRG-R-SG-250007 | No | No |
| Peckfield Level Crossing  | Closure of the level crossing and diversion of the PROW via one of two options: Creating a new right of way Southern Boundary in recreation ground | Article 7 (<i>Power to construct and maintain works</i>) Article 17 (<i>Replacement and closure of level crossings</i>) | Sheet 12 12-001 to 12-006 12-008 | Yes Drawing Number: 151666-TRA-00-HUL4-DRG-R-SG-250001, 151666-TRA-00-HUL4-DRG-R-SG- | No | No |

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|--|---|---|---|---|--|--|
| | 1. Footpath 2. Bridleway Works and land use for the closure of Peckfield LC and construction of PROW diversion with associated highways improvement and parking works to Lower Peckfield Lane | Article 28 (<i>Temporary use of land for construction of works</i>) Article 30 (<i>Temporary use of land for access</i>) | 12-014 ³⁵ 12-007 12-010 12-011 12-013 12-014A ³⁶ 12-012 ³⁷ | 250002, 151666-TRA-00-HUL4-DRG-R-SG-250003, 151666-TRA-00-HUL4-DRG-R-SG-250004, 151666-TRA-00-HUL4-DRG-R-SG-250005, 151666-TRA-00-HUL4-DRG-R-SG-250009. | | |
| Highroyds Wood Level Crossing  | Diversion of PROW - Closure of the level crossing and diversion of Micklefield 7 permanently and extinguish the PROW. | Article 10 (<i>stopping up of streets</i>) Article 17 (<i>Replacement and closure of level crossings</i>) | Sheet 13 13-001 13-002 13-003 ³⁸ 13-004 to 13-008 | No | Part 8 (Class A) to the extent any works needed to permanently close the level crossing. | No |

³⁵ Permanent Acquisition

³⁶ Temporary Possession and Access for Construction

³⁷ Extinguishment of Rights

³⁸ Temporary Access for Construction

| Description of works and/or Affected Land | What is acquired and why (Temporary use, Permanent Acquisition, or new rights) | Order Power/Provision ⁴ | Land Parcels/Sheet no. on Land and Works Plan | Is it subject to the Application for Deemed Planning Permission? [include plan refs.] | Is it Permitted Development – Part 18/ Part 8/ Part 4 ⁵ [details of prior approvals granted] | Are the works subject to a Listed Building Application? [details of relevant LBC application] |
|---|--|---|--|--|--|--|
| | | Article 28 (<i>Temporary use of land for construction of works</i>) Article 30 (<i>Temporary use of land for access</i>) | 13-010 13-011 ³⁹ 13-009 ⁴⁰ Sheet 14 13-011 ⁴¹ 13-012 13-013 ⁴² | | | |

³⁹ Temporary Possession and Access for Construction

⁴⁰ Extinguishments of Rights

⁴¹ Temporary Possession and Access for Construction

⁴² Temporary Access for Construction

Appendix 3 – Schedule of Land and Rights

| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-001 | Unknown | 2.62 square metres of land forming part of access to car park (Crown Street Car Park), situated to the north of public adopted highway (Assembly Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre Unregistered | | Yes | | | | Yes | | Temporary Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-002 | Unknown City Fusion Limited (Co. Reg. No: 1400794) (in respect of Caution) | 50.87 square metres of land forming part of access to car park (Crown Street Car Park) and non-definitive footpath, situated to the north of public adopted highway (Assembly Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre Unregistered YY87108 | | Yes | | | | Yes | | Temporary Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-003 | Unknown City Fusion Limited | 15.00 square metres of land forming part of access to car park (Crown Street Car | | Yes | | | | Yes | | Temporary Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 1400794) (in respect of Cautions) | Park) and non- definitive footpath, situated to the north of public adopted highway (Assembly Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre Unregistered YY87108 YY105780 | | | | | | | | Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-004 | Unknown City Fusion Limited (Co. Reg. No: 1400794) (in respect of Caution) | 155.50 square metres of land forming part of car park (Crown Street Car Park) and non-definitive footpath, situated to the north of public adopted highway (Assembly Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre Unregistered YY87108 | | Yes | | | | Yes | | Temporary Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-005 | Unknown | 13.34 square metres of scrubland adjoining car park (Crown Street Car Park and non-definitive footpath, situated to the north of railway (Leeds to Hull Lines | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | (ELR:HUL4)) and west of public adopted highway (Wharf Street), in the ward of Little London and Woodhouse, Leeds City Centre Unregistered | | | | | | | | HUL4/47 Kirkgate Viaduct |
| 1-006 | City Fusion Limited (Co. Reg. No: 1400794) N. M. Rothschild & Sons Limited (Co. Reg. No: 00925279) (in respect of a legal charge dated 26 April 2004 as registered under title WYK552918) | 6.47 square metres of land forming part of car park (Crown Street Car Park), situated to the north of public adopted highway (Assembly Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre WYK552918 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-007 | City Fusion Limited (Co. Reg. No: 1400794) N. M. Rothschild & Sons Limited (Co. Reg. No: 00925279) (in respect of a legal charge dated 26 April 2004 as registered under title WYK513509) | 57.72 square metres of land forming part of car park (Crown Street Car Park) and non-definitive footpath, situated to the north of public adopted highway (Assembly Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre WYK513509 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-008 | City Fusion Limited | 143.38 square metres of land forming part of | | Yes | | | | Yes | | Temporary Possession and |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 1400794) N. M. Rothschild & Sons Limited (Co. Reg. No: 00925279) (in respect of a legal charge dated 26 April 2004 as registered under title WYK513508) | car park (Crown Street Car Park), situated to the west of public adopted highway (Wharf Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre WYK513508 | | | | | | | | Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-009 | Unknown | 3.81 square metres of land forming part of car park (Crown Street Car Park), situated to the west of public adopted highway (Wharf Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-010 | Unknown | 1.29 square metres of land forming part of car park (Crown Street Car Park), situated to the west of public adopted highway (Wharf Street) and south of public adopted highway (Kirkgate), in the ward of Little London and | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Woodhouse, Leeds City Centre Unregistered | | | | | | | | |
| 1-011 | City Fusion Limited (Co. Reg. No: 1400794) N. M. Rothschild & Sons Limited (Co. Reg. No: 00925279) (in respect of a legal charge dated 26 April 2004 as registered under title WYK513507) Unknown (in respect of mines and minerals) | 90.06 square metres of land forming part of car park (Crown Street Car Park) and non-definitive footpath, situated to the west of public adopted highway (Wharf Street) and south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Centre WYK513507 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-012 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 20.21 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)) and non-definitive footpath, situated to the south of public adopted highway (Kirkgate) and the northwest of public adopted highway (Wharf Street), in the wards of Hunslet and Riverside, and Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-013 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 29.98 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated to the south of public adopted highway (Harper Street) and to the northwest of public adopted highway (Wharf Street), in the wards of Hunslet and Riverside, and Little London and Woodhouse, Leeds City Council YY124697 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-014 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Mohammed Khalifa (in respect of caution) | 158.87 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)) and restaurant unit (81 Kirkgate), situated to the south of public adopted highway (Kirkgate) and to the northwest of public adopted highway (Wharf Street), in the wards of Hunslet and Riverside, and Little London and Woodhouse, Leeds City Council YY124697 WYK757049 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-015 | City Fusion Limited (Co. Reg. No: 1400794) Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Unknown (in respect of mines and minerals) | 2.23 square metres of commercial building situated to the south of public adopted highway (Kirkgate) and to the north of public adopted highway (Wharf Street), in the ward of Little London and Woodhouse, Leeds City Council WYK475441 YY124697 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-015a | City Fusion Limited (Co. Reg. No: 1400794) Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Mohammed Khalifa (in respect of caution) Unknown (in respect of mines and minerals) | 0.12 square metres of commercial building situated to the south of public adopted highway (Kirkgate) and to the north of public adopted highway (Wharf Street), in the ward of Little London and Woodhouse, Leeds City Council WYK475441 YY124697 WYK757049 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-016 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 4.71 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated to the northwest of public adopted highway (Wharf | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Street) and to the south of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Council YY124697 Unregistered | | | | | | | | HUL4/47 Kirkgate Viaduct |
| 1-017 | Unknown | 0.45 square metres of scrubland adjoining 81 and 83 Kirkgate, situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and to the southwest of public adopted highway (Cross York Street), in the ward of Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-018 | Yorkshire Design Services Limited (Co. Reg. No: 2120197) Unknown (in respect of mines and minerals) | 383.27 square metres of car park (7-15 Wharf Street) and situated to the northwest of public adopted highway (Wharf Street) and to the south of public adopted highway (Kirkgate), in the ward of Hunslet and Riverside, Leeds City Council WYK465067 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-018a | Yorkshire Design Services Limited (Co. Reg. No: 2120197) Unknown (in respect of mines and minerals) Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 9.36 square metres of car park (7-15 Wharf Street) and part of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated to the northwest of public adopted highway (Wharf Street) and to the south of public adopted highway (Kirkgate), in the ward of Hunslet and Riverside, Leeds City Council WYK465067 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-019 | Mohammed Khalifa (in respect of caution) Yorkshire Design Services Limited (Co. Reg. No: 2120197) Unknown (in respect of mines and minerals) | 6.45 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated to the northwest of public adopted highway (Wharf Street) and to the south of public adopted highway (Kirkgate), in the ward of Hunslet and Riverside, Leeds City Council WYK465067 WYK757049 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-020 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 1.24 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated to the southwest of public adopted highway (Cross York Street) and to the south of public adopted highway (Harper Street), in the ward of Hunslet and Riverside, Leeds City Council YY124697 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-021 | Mohammed Khalifa (in respect of caution) Yorkshire Design Services Limited (Co. Reg. No: 2120197) Unknown (in respect of mines and minerals) | 6.73 square metres of part of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated to the southeast of public adopted highway (Harper Street) and to the south of public adopted highway (Kirkgate), in the ward of Hunslet and Riverside, Leeds City Council WYK465067 WYK757049 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-022 | Unknown Leeds City Council (in respect of public | 272.90 square metres of public adopted highway (Kirkgate), situated to the north of | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <p><i>adopted highway (Kirkgate) and in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>City Fusion Limited (Co. Reg. No: 1400794) <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>Network Rail Infrastructure Limited (Co. Reg. No: 02904587) <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> | <p>railway (Leeds to Hull Lines (ELR:HUL4)) and to the northwest of public adopted highway (Wharf Street), in the ward of Little London and Woodhouse, Leeds City Council</p> <p>Unregistered</p> | | | | | | | | Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-023 | Leeds City Council | 91.66 square metres of public adopted highway and footway (Kirkgate), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and to the northwest of public adopted highway (Wharf Street), in the ward of Little London and Woodhouse, Leeds City Council WYK912031 | | Yes | | | | Yes | | <p>Temporary Possession and Temporary Stopping Up</p> <p>Required for reconstruction of HUL4/47 Kirkgate Viaduct</p> |
| 1-024 | Network Rail Infrastructure Limited | 62.06 square metres of Marsh Lane Viaduct carrying railway | | Yes | | | | Yes | | Temporary Possession and |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 02904587) | (Leeds to Hull Lines (ELR:HUL4)) and advertising hoarding, situated to the south of public adopted highway (Kirkgate) and to the northwest of public adopted highway (Wharf Street) in the wards of Hunslet and Riverside and Little London and Woodhouse, Leeds City Council YY12469 Unregistered | | | | | | | | Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-025 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Leeds City Council (in respect of public adopted highway (Kirkgate)) | 151.85 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)) over public adopted highway (Kirkgate), situated to the southwest of public adopted highway (Cross York Street) and to the southeast of public adopted highway (Harper Street), in the wards of Hunslet and Riverside and Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-025a | Network Rail Infrastructure Limited | 55.49 square metres of Marsh Lane Viaduct carrying railway | | Yes | | | | Yes | | Temporary Possession and |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 02904587) | (Leeds to Hull Lines (ELR:HUL4)) situated to the southwest of public adopted highway (Cross York Street) and to the southeast of public adopted highway (Harper Street), in the wards of Hunslet and Riverside and Little London and Woodhouse, Leeds City Council Unregistered | | | | | | | | Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-025b | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Leeds City Council (in respect of public adopted highway (Cross York Street)) | 4.35 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)) over public adopted highway (Cross York Street), situated to the southwest of public adopted highway (Cross York Street) and to the southeast of public adopted highway (Harper Street), in the wards of Hunslet and Riverside and Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-026 | Network Rail Infrastructure Limited (Co. Reg. No: | 22.05 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), situated | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | 02904587) | to the north of public adopted highway (Kirkgate) and to the southwest of public adopted highway (Cross York Street), in the wards of Hunslet and Riverside and Little London and Woodhouse, Leeds City Council YY124698 Unregistered | | | | | | | | Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-027 | Leeds City Council Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Unknown (in respect of mines and minerals) | 32.13 square metres of private land and railway power cable pylon, situated to the north of public adopted highway (Kirkgate) and west of Church Walk, in the ward of Little London and Woodhouse, Leeds City Council WYK912031 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-028 | Urban Edge New York Street Limited (Co. Reg. No: 04215741) Santander UK Plc (Co. Reg. No: 02294747) (in respect of a Legal Charge dated 21 October 2015 as registered under title WYK764172) | 1.90 square metres of private land, east of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council WYK764172 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-029 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 0.89 square metres of railway works, and land, situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Cross York Street), in the ward of Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-029a | Yorkshire Housing Limited (Co. Reg. No: 30443R) Affordable Housing Finance plc (Co. Reg. No: 08434613) (in respect of a Legal Charge dated 27 March 2018 as registered under title WYK832807) | 0.98 square metres of land, situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Cross York Street), in the ward of Little London and Woodhouse, Leeds City Council WYK832807 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-030 | Unknown | 2.34 square metres of scrubland, situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Cross York Street), in the ward of Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-031 | Leeds City Council Yorkshire Housing Limited (Co. Reg. No: 30443R) Affordable Housing Finance Plc (Co. Reg. No: 8434613) (in respect of a legal charge dated 27 March 2018 as registered under title WYK832807) | 0.05 square metres of scrubland, situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Cross York Street), in the ward of Little London and Woodhouse, Leeds City Council WYK912031 WYK832807 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-032 | Urban Edge New York Street Limited (Co. Reg. No: 04215741) Santander UK Plc (Co. Reg. No: 02294747) (in respect of a Legal Charge dated 21 October 2015 as registered under title WYK764172) | 0.36 square metres of private land, west of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council WYK764172 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-033 | Yorkshire Housing Limited (Co. Reg. No: 30443R) Affordable Housing Finance Plc (Co. Reg. No: 8434613) (in respect of a legal charge dated 27 March 2018 as registered under title WYK832807) | 0.2 square metres of private land, west of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | March 2018 as registered under title WYK832807) | WYK832807 | | | | | | | | |
| 1-034 | Urban Edge New York Street Limited (Co. Reg. No: 04215741) Santander UK Plc (Co. Reg. No: 02294747) (in respect of a Legal Charge dated 21 October 2015 as registered under title WYK764172) Yorkshire Housing Limited (Co. Reg. No: 30443R) Affordable Housing Finance Plc (Co. Reg. No: 8434613) (in respect of a legal charge dated 27 March 2018 as registered under title WYK832807) | 1.87 square metres of private land, west of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council WYK832807 WYK764172 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-035 | Urban Edge New York Street Limited (Co. Reg. No: 04215741) Santander UK Plc (Co. Reg. No: 02294747) (in respect of a Legal Charge dated | 0.17 square metres of private land, west of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | 21 October 2015 as registered under title WYK764172) | Woodhouse, Leeds City Council WYK764172 | | | | | | | | |
| 1-036 | Yorkshire Housing Limited (Co. Reg. No: 30443R) Affordable Housing Finance Plc (Co. Reg. No: 8434613) (in respect of a legal charge dated 27 March 2018 as registered under title WYK832807) | 12.51 square metres of private land, west of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council WYK832807 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-036a | Unknown | 2.73 square metres of private land, west of public adopted highway (Cross York Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-037 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Leeds City Council | 2.50 square metres of Marsh Lane Viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)) over public adopted highway verge (Kirkgate) situated to the southwest of public adopted | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | highway (Cross York Street) and northwest of public adopted highway (Wharf Street), in the ward of Hunslet and Riverside, Leeds City Council WYK911734 Unregistered | | | | | | | | |
| 1-038 | Leeds City Council | 41.44 square metres of public adopted highway verge (Wharf Street) situated to the southwest of public adopted highway (Cross York Street) and to the south of public adopted highway (Kirkgate), in the ward of Hunslet and Riverside, Leeds City Council WYK911734 | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 1-039 | Unknown Leeds City Council <i>(in respect of public adopted highways (Wharf Street, Kirkgate and Cross York Street) and in respect of subsoil rights up to the centreline of the public highway)</i> | 396.03 square metres of public adopted highways and verge (Wharf Street, Kirkgate and Cross York Street) situated to the southeast of public adopted highway (Harper Street) and to the northwest of public adopted highway (High Court), in the ward of Hunslet and Riverside, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 1-040 | Leeds City Council | 115.72 square metres of public adopted highway (Wharf Street) situated to the southwest of public adopted highway (Cross York Street) and to the south of public adopted highway (Kirkgate), in the ward of Hunslet and Riverside, Leeds City WYK911734 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/47 Kirkgate Viaduct |
| 2-001 | Unknown Wade's Charity (Charity. Reg. No: 224939) | 125.84 square metres of embankment and Penny Pocket Park situated to the south of public adopted highway (York Street) and to the west of public adopted highway (Duke Street), in the ward of Little London and Woodhouse, Leeds City Council WYK924323 Unregistered | Yes | | | | Yes | Yes | Permanent Acquisition Required in connection with new rail infrastructure | |
| 2-002 | Unknown Wade's Charity (Charity. Reg. No: 224939) | 23.62 square metres of embankment and Penny Pocket Park, situated to the west of public adopted highway (Duke Street) and to the north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and | Yes | | | | Yes | Yes | Permanent Acquisition Required in connection with new rail infrastructure | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Woodhouse, Leeds City Council WYK924323 Unregistered | | | | | | | | |
| 2-003 | Unknown Wade's Charity (Charity. Reg. No: 224939) | 19.82 square metres of embankment and graveyard, situated to the west of public adopted highway (Duke Street) and to the east of public adopted highway (Church Lane), in the ward of Little London and Woodhouse, Leeds City Council WYK924323 Unregistered | Yes | | | | Yes | Yes | Permanent Acquisition Required in connection with new rail infrastructure | |
| 2-004 | Unknown Wade's Charity (Charity. Reg. No: 224939) | 21.27 square metres of grass, embankment and graveyard, situated to the west of public adopted highway (Duke Street) and to the north of public adopted highway (Kirkgate), in the ward of Little London and Woodhouse, Leeds City Council WYK924323 Unregistered | Yes | | | | Yes | Yes | Permanent Acquisition Required in connection with new rail infrastructure | |
| 3-001 | Network Rail Infrastructure Limited | 360.74 square metres of viaduct carrying railway (Leeds to Hull Lines (ELR:HUL4)), commercial unit (All | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Brass & Woodwind), advertising hoarding walled compound and commercial unit beneath viaduct arches situated to the southwest of St Peters Place and to the northeast of Brussels Street, in the wards of Hunslet and Riverside, and Little London and Woodhouse, Leeds City Council YY124699 Unregistered | | | | | | | | Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-002 | Leeds City Council | 40.07 square metres of public footway situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and to the northwest of public adopted highway (Railway Street), in the ward of Little London and Woodhouse, Leeds City Council WYK912140 | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-003 | Unknown Leeds City Council <i>(in respect of public adopted highway (Marsh Lane) and in respect of subsoil rights up to the centre line of the public highway)</i> | 0.46 square metres of public adopted highway (Marsh Lane), situated to the northwest of public adopted highway (Railway Street) and north of railway (Leeds to Hull Lines (ELR:HUL4)) in the | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | ward of Little London and Woodhouse, Leeds City Council Unregistered | | | | | | | | |
| 3-004 | Unknown Leeds City Council <i>(in respect public adopted highway, cycle lane and footway (Marsh Lane) and in respect of subsoil rights up to the centre line of the public highway)</i> Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> <i>(in respect of subsoil rights up to the centre line of the public highway)</i> | 125.07 square metres of public adopted highway, cycle lane and footway (Marsh Lane), situated to the north of public adopted highway (Saxton Lane) and to the north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-005 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 79.13 square metres of land forming part of railway, works and land (Leeds to Hull Lines (ELR:HUL4)) and advertising hoarding, situated to the east of public adopted highway (Marsh Lane) and to the north of public adopted highway (Railway Street), in the ward of Burmantofts and | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |

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|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Richmond Hill, Leeds City Council YY124685 Unregistered | | | | | | | | |
| 3-005a | Unknown Leeds City Council <i>(in respect of public adopted highway, cycle lane and footway (Marsh Lane))</i> Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> <i>(in respect of subsoil right up to the centreline of the public highway)</i> | 1.50 square metres public adopted highway (Marsh Lane) situated to the north of (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Railway Street), in the ward of Burmantofts and Richmond Hill, Leeds City Council YY124685 Unregistered | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-006 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 13.38 square metres of pavement situated to the north of public adopted highway (Railway Street) and to the east of public adopted highway (Marsh Lane), in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-007 | Network Rail Infrastructure Limited <i>(Co. Reg. No:</i> | 7.54 square metres of walled compound situated to the north of public highway (Saxton Lane) and to the north of railway | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | 02904587) | (Leeds to Hull Lines (ELR:HUL4)), in the ward of Little London and Woodhouse, Leeds City Council Unregistered | | | | | | | | Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-008 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 4.26 square metres of walled compound situated to the northeast of public adopted highway (Brussels Street) and to the northeast of public adopted highway (Railway Street), in the ward of Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-009 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 101.35 square metres of viaduct (HUL4/40) carrying railway (Leeds to Hull Lines (ELR:HUL4)) and commercial unit beneath the viaduct arches, situated to the northeast of public adopted highway (Brussels Street) and to the west of public adopted highway (Railway Street), in the wards of Hunslet and Riverside, and Little London and Woodhouse, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/40 Marsh Lane Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 3-010 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 152.66 square metres of viaduct (HUL4/40) carrying railway (Leeds to Hull Lines (ELR:HUL4)), and advertising billboards, situated to the north of public adopted highway (Saxton Lane) and to the south of public adopted highway (York Street), in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-011 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 48.77 square metres of viaduct (HUL4/40) carrying railway (Leeds to Hull Lines (ELR:HUL4)) and underlying commercial unit (Paradise Grill), situated to the north of public adopted highway (Railway Street) and to the north of public adopted highway (Saxton Lane), in the ward of Burmantofts and Richmond Hill, Leeds City Council YY124685 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-012 | Unknown Leeds City Council (in respect of public adopted highway (Marsh Lane) and in | 48.99 square metres of public adopted highway (Marsh Lane), footway and metal fence situated | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>respect of subsoil rights up to the centreline of the public highway)</i> CDR Nomineeco 1 Limited <i>(Co. Reg. No: 11519165)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> CDR Nomineeco 2 Limited <i>(Co. Reg. No: 11519140)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | to the south of railway (Leeds to Hull Lines (ELR:HUL4)), and to the north of public adopted highway (Saxton Lane), in the wards of Hunslet and Riverside, and Little London and Woodhouse, Leeds City Council Unregistered | | | | | | | | Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-013 | Unknown Leeds City Council <i>(in respect of public adopted highway (Railway Street) and in respect of subsoil rights up to the centreline of the public highway)</i> | 184.06 square metres of public adopted highway (Railway Street) and pedestrian area, south of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Marsh Lane), in the wards of | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | CDR Nomineeco 1 Limited <i>(Co. Reg. No: 11519165)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> CDR Nomineeco 2 Limited <i>(Co. Reg. No: 11519140)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | Hunslet and Riverside and Burmantofts and Richmond Hill, Leeds City Council Unregistered | | | | | | | | |
| 3-014 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 64.27 square metres of public adopted highway (Railway Street) and pedestrian area, south of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (Marsh Lane), in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK912140 | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 3-015 | Leeds City Council | 3.15 square metres of public adopted highway (Marsh Lane) and footway situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)), and to the east of public adopted highway (Brussels Street), in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK549769 | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-016 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 9.12 square metres of footway situated to the west of public adopted highway (Marsh Lane) and to the north of public adopted highway (Saxton Lane), in the ward of Hunslet and Riverside, Leeds City Council WYK912140 | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |
| 3-017 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> Leeds City Council <i>(in respect of public adopted highway (Marsh Lane))</i> | 235.07 square metres of public adopted highway (Marsh Lane), Marsh Lane Bridge (HUL4/40) carrying railway (Leeds to Hull Lines (ELR:HUL4)) and advertising hording to the east of public adopted highway (Brussels Street) and south of public | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for reconstruction of HUL4/40 Marsh Lane Viaduct |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | adopted highway (York Street),), in the ward of Hunslet and Riverside, Leeds City Council Unregistered | | | | | | | | |
| 4-001 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 979.89 square metres of public adopted highway (New Market Approach), footway and grassed area, south of Train Repairing Centre (Neville Hill TMD) and north of public adopted highway (Pontefract Lane) in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK865414 Unregistered | Yes | | | | Yes | Yes | Permanent Acquisition Construction of new access road | |
| 4-002 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 225.20 square metres of non-definitive bridleway and scrubland, south of Train Repairing Centre (Neville Hill TMD) and north of public adopted highway (New Market Approach) in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK865414 Unregistered | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Construction of new access road |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 4-003 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 154.88 square metres of non-definitive bridleway, footway and scrubland, south of Train Repairing Centre (Neville Hill TMD) and north of public adopted highway (New Market Approach) in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK865414 Unregistered | Yes | | | | Yes | Yes | Permanent Acquisition Construction of new access road | |
| 4-004 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 383.86 square metres of scrubland to the north of public adopted highway (New Market Approach) and south of Train Repairing Centre (Neville Hill TMD) in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK865414 Unregistered | Yes | | | | Yes | Yes | Permanent Acquisition Construction of new access road | |
| 4-005 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 175.14 square metres of non-definitive bridleway and scrubland, south of Train Repairing Centre (Neville Hill TMD) and north of public adopted highway (New Market Approach) in the ward | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Construction of new access road |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | of Burmantofts and Richmond Hill, Leeds City Council WYK865414 Unregistered | | | | | | | | |
| 5-001 | Unknown | 64.39 square metres of scrubland, east of Osmondthorpe Lane and west of Diesel Repair Shed, in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-001a | Unknown Leeds City Council <i>(in respect of public adopted highway and footway (Osmondthorpe Lane))</i> | 22.90 square metres of footway forming part of public adopted highway (Osmondthorpe Lane), east of Osmondthorpe Lane and west of Diesel Repair Shed, in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-002 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 76.41 square metres of metal fencing and scrubland forming part of Diesel Repair Shed, West of Osmondthorpe Lane, north of 210 Osmondthorpe Lane, in the ward of Burmantofts and | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Richmond Hill, Leeds City Council Unregistered YY144492 | | | | | | | | |
| 5-003 | Leeds City Council | 33.18 square metres of grassland, hedgerow, railway embankment and bridge structure forming part of Osmondthorpe Lane Bridge (HUL4/32) carrying railway (Leeds to Hull Lines (ELR:HUL4)), southeast of Diesel Repair Shed, west of 235 Osmondthorpe Lane, in the ward of Temple Newsam, Leeds City Council WYK850940 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-004 | Unknown Leeds City Council <i>(in respect of public adopted highway footway (Osmondthorpe Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> | 1.40 square metres of footway forming part of public adopted highway footway (Osmondthorpe Lane), northeast of 210 Osmondthorpe Lane, west of 235 Osmondthorpe Lane in the ward of Temple Newsam, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-005 | Unknown | 0.09 square metres of grassed area to the west of public adopted highway (Osmondthorpe | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Lane), south of Osmondthorpe Lane Bridge (HUL4/32) carrying railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | | | | | | | Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-006 | Unknown Leeds City Council <i>(in respect of public adopted highway footway (Osmondthorpe Lane))</i> Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 4.84 square metres of footway forming part of public adopted highway (Osmondthorpe Lane), bridge structure forming part of Osmondthorpe Lane Bridge (HUL4/32) and private accessway to railway (Leeds to Hull Lines (ELR:HUL4)), south of Diesel Repair Shed and west of 235 Osmondthorpe Lane, in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-007 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 63.07 square metres of land scrubland forming part of railway embankment, bridge structure forming part of Osmondthorpe Lane Bridge (HUL4/32) and private accessway to railway (Leeds to Hull Lines | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | (ELR:HUL4)), south of Diesel Repair Shed and west of 235 Osmondthorpe Lane, in the ward of Burmantofts and Richmond Hill, Leeds City Council Unregistered WYK595039 | | | | | | | | Osmandthorpe Lane Underbridge |
| 5-008 | Leeds City Council Unknown <i>(is respect of mines and minerals)</i> | 2961.87 square metres of grassland, lying to the north of public adopted highway (Neville Garth) and west of public adopted highway (Osmondthorpe Lane), in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK840693 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |
| 5-009 | Leeds City Council Unknown <i>(is respect of mines and minerals)</i> | 1534.86 square metres of scrubland, lying to the northwest of public adopted highway (Neville Garth) and west of public adopted highway (Osmondthorpe Lane), south of Diesel Repair Shed, in the ward of Burmantofts and Richmond Hill, Leeds City Council WYK865414 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for reconstruction of HUL4/32 Osmandthorpe Lane Underbridge |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Unregistered | | | | | | | | |
| 6-001 | Leeds City Council | 72.54 square metres of Cul-de-Sac forming part of public adopted highway (Wykebeck Avenue), footway and hedgerow, south of 86 Wykebeck Avenue and east of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Temple Newsam, Leeds City Council WYK879524 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for creation of temporary Road Rail Access Point. |
| 6-002 | Gleeson Regeneration Limited (Co. Reg. No: 03920096) Unknown (in respect of mines and minerals only) | 1868.47 square metres of scrubland, hedgerows and trees, south of public adopted highway (Wykebeck Avenue) and east of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Temple Newsam, Leeds City Council YY99047 (Pending Transfer) | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for creation of temporary Road Rail Access Point. |
| 6-003 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Unknown (in respect of mines and minerals only) | 123.51 square metres of scrubland, hedgerow, fencing access and railway bridge embankment, south of 86 Wykebeck Avenue and east of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Temple | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for creation of temporary Road Rail Access Point. |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Newsam, Leeds City Council YY99047 (Pending Transfer) | | | | | | | | |
| 6-004 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 232.78 square metres of land adjoining railway works and land (Leeds to Hull Lines (ELR:HUL4)), southwest of public adopted highway (Wykebeck Avenue) and east of The Doreen Hamilton Centre, in the ward of Temple Newsam, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for creation of temporary Road Rail Access Point. |
| 7-001 | Unknown Leeds City Council (in respect of public adopted highway (Austhorpe Road)) Alice Elizabeth Swinburn (in respect of subsoil rights up to the centreline of the public highway) Geoffrey Brian Wilson (in respect of subsoil rights up to the centreline of the public highway) Helen Cowley (in respect of subsoil rights up to the | 796.84 square metres of public adopted highway (Austhorpe Road), situated to the west of public adopted highway (Croftdale Grove) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|-------------|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|----------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <p><i>centreline of the public highway)</i></p> <p>HR Trust Limited (Co. Reg. No: 00734984) (in respect of subsoil rights up to the centreline of the public highway)</p> <p>James Brian Parkinson (in respect of subsoil rights up to the centreline of the public highway)</p> <p>John Frewen Swinburn (in respect of subsoil rights up to the centreline of the public highway)</p> <p>L&P 183 Limited (Co. Reg. No: 06177983) (in respect of subsoil rights up to the centreline of the public highway)</p> <p>Marjorie Wilson (in respect of subsoil rights up to the centreline of the public highway)</p> <p>Sylvia Parkinson (in respect of subsoil rights up to the</p> | | | | | | | | | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <p><i>centreline of the public highway)</i></p> <p>The Owner <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>Timothy Mark Cowley <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> | | | | | | | | | |
| 7-002 | <p>Unknown</p> <p>Leeds City Council <i>(in respect of public adopted highway (Croftdale Grove))</i></p> <p>James Brian Parkinson <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>Sylvia Parkinson <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>The Owner <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> | <p>122.71 square metres of public adopted highway (Croftdale Grove), situated to the east of public adopted highway (Austhorpe Road) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council</p> <p>Unregistered</p> | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 7-003 | Unknown Leeds City Council <i>(in respect of public adopted highway (Railway Road) and in respect of subsoil rights up to the centreline of the public highway)</i> L&P 183 Limited <i>(Co. Reg. No: 06177983)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 165.66 square metres of public adopted highway (Railway Road), situated to the west of public adopted highway (Austhorpe Road) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |
| 7-003a | Unknown Leeds City Council <i>(in respect of public adopted highway (Railway Road) and in respect of subsoil rights up to the centreline of the public highway)</i> L&P 183 Limited <i>(Co. Reg. No: 06177983)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Elaine Mary Gardham Van Sante <i>(in respect of subsoil rights up to the</i> | 288.01 square metres of public adopted highway (Railway Road), situated to the west of public adopted highway (Austhorpe Road) and north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Temporary Stopping Up Required for Work No. 2 (Gas Main Diversion) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>centreline of the public highway)</i> Rob Van Sante <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | | | | | | | | | |
| 7-004 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 115.13 square metres of woodland and hedgerows, situated to the south of public adopted highway (Croftdale Grove) and to the west of public adopted highway (Austhorpe Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council YWE45138 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |
| 7-004a | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 45.52 square metres of woodland and hedgerows, situated to the south of public adopted highway (Croftdale Grove) and to the west of public adopted highway (Austhorpe Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council YWE45138 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-005 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 143.22 square metres of bridge carrying public adopted highway (Austhorpe Lane) over disused railway situated to the | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | northeast of public adopted highway (Kennerleigh Avenue) and to the west of public adopted highway (Amelia Stewart Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK889420 | | | | | | | | |
| 7-005a | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 61.47 square metres of bridge carrying public adopted highway (Austhorpe Lane) over disused railway situated to the northeast of public adopted highway (Kennerleigh Avenue) and to the west of public adopted highway (Amelia Stewart Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK889420 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-006 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 1174.37 square metres of fence, trees and outbuildings (stables), situated to the south of public adopted highway (Railway Road) and to the west of public adopted highway (Austhorpe Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 7-006a | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | YWE45138 705.47 square metres of fence, trees and outbuildings (stables), situated to the south of public adopted highway (Railway Road) and to the west of public adopted highway (Austhorpe Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council YWE45138 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 2 (Gas Main Diversion) |
| 7-007 | Bellway Homes Limited <i>(Co. Reg. No: 00670176)</i> Unknown <i>(in respect of mines and minerals)</i> | 140.81 square metres of public adopted highway (Austhorpe Lane) and verge situated to the west of public adopted highway (Ada Glassby Court) and to the east of public adopted highway (Railway Road), in the ward of Cross Gates and Whinmoor, Leeds City Council YY789 Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-007a | Bellway Homes Limited <i>(Co. Reg. No: 00670176)</i> Unknown <i>(in respect of mines and minerals)</i> | 45.60 square metres of footway and grassed area situated to the west of public adopted highway (Ada Glassby Court) and to the east of public adopted highway | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | (Railway Road), in the ward of Cross Gates and Whinmoor, Leeds City Council YY789 Unregistered | | | | | | | | |
| 7-008 | Unknown Leeds City Council <i>(in respect of public adopted highway (Austhorpe Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> Bellway Homes Limited <i>(Co. Reg. No: 00670176)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> John Dring <i>(in respect of subsoil rights up to the centreline of the public highway)</i> The Executor of Kevin Martin Griffin <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 289.31 square metres of public adopted highway (Austhorpe Lane), north of railway (Leeds to Hull Lines (ELR:HUL4)) and southeast of public adopted highway (Railway Road), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-008a | Unknown | 53.76 square metres of hardstanding and scrubland north of | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | railway (Leeds to Hull Lines (ELR:HUL4)) and southeast of public adopted highway (Railway Road), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | | | | | | | | |
| 7-009 | John Dring The Executor of Kevin Martin Griffin Unknown <i>(in respect of mines and minerals)</i> | 910.05 square metres of hardstanding, outbuildings and scrubland situated to west of public adopted highway (Austhorpe Lane) and to the southeast of public adopted highway (Marshall Avenue), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK409108 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |
| 7-010 | Thorpe Park Developments Limited <i>(Co. Reg. No: 04141502)</i> | 95.28 square metres of woodland situated to the northeast of public adopted highway (Kennerleigh Avenue) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK624551 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-011 | Network Rail Infrastructure Limited | 413.77 square metres of railway, works and land (Leeds to Hull | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 02904587) | Lines (ELR:HUL4)) and cutting situated to southeast of public adopted highway (Railway Road) and to the northeast of public adopted highway (Kennerleigh Avenue), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | | | | | | | | |
| 7-012 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Leeds City Council (in respect of bridge carrying public adopted highway (Austhorpe Lane)) | 250.91 square metres of bridge carrying public adopted highway (Austhorpe Lane) over railway line (Leeds to Hull Lines (ELR:HUL4)) situated to the northeast of public adopted highway (Kennerleigh Avenue) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-012a | Leeds City Council (in respect of public adopted highway (Austhorpe Lane)) Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 13.21 square metres of bridge carrying public adopted highway (Austhorpe Lane) situated to the northeast of public adopted highway (Kennerleigh Avenue) and to the southwest | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | | | | | | | | |
| 7-013 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 1370.46 square metres of railway, works and land (Leeds to Hull Lines (ELR:HUL4)) and cutting situated to southeast of public adopted highway (Railway Road) and to the northeast of public adopted highway (Kennerleigh Avenue), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |
| 7-013a | Leeds City Council <i>(in respect of public adopted highway (Austhorpe Lane))</i> Network Rail Infrastructure Limited (Co. Reg. No: 02904587) <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 4.69 square metres of public adopted highway (Austhorpe Lane) verge to the northeast of public adopted highway (Kennerleigh Avenue) and west of public adopted highway (Amelia Stewart Lane), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-013b | Network Rail Infrastructure | 292.38 square metres of railway, works and | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Limited (Co. Reg. No: 02904587) | land (Leeds to Hull Lines (ELR:HUL4)) and cutting situated to southeast of public adopted highway (Railway Road) and to the northeast of public adopted highway (Kennerleigh Avenue), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | | | | | | | (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-014 | David Meehan Elaine Yvonne Meehan | 117.03 square metres of garden and driveway forming part of residential property (193 Austhorpe Lane), west of public adopted highway (Austhorpe Lane) and south of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK194649 | | Yes | Yes | | | Yes | | Temporary Possession and Access for Construction Required for Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) |
| 7-015 | Leeds City Council | 450.13 square metres of public adopted highway (Austhorpe Lane) and footway situated to the northeast of public adopted highway (Kingswear Close) and to the south of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | WYK889407 | | | | | | | | |
| 7-015a | Leeds City Council | 960.07 square metres of public adopted highway (Austhorpe Lane) and footway situated to the northeast of public adopted highway (Kingswear Close) and to the south of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK889407 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-016 | Unknown | 241.81 square metres of wooded area situated to the northeast of public adopted highway (Kingswear Close) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |
| 7-016a | Unknown Leeds City Council <i>(in respect of public adopted highway (Austhorpe Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> | 899.00 square metres of public adopted highway (Austhorpe Lane) and grass verge situated to the northeast of public adopted highway (Kingswear Close) and to the southwest of public adopted | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | | | | | | | | |
| 7-016b | Unknown | 35.78 square metres of wooded area situated to the northeast of public adopted highway (Kingswear Close) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) | |
| 7-016c | Unknown Leeds City Council <i>(in respect of public adopted highway (Austhorpe Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> Thorpe Park Developments Limited <i>(Co. Reg. No: 04141502)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 119.52 square metres of public adopted highway (Austhorpe Lane), footway and grass verge situated to the northeast of public adopted highway (Kingswear Close) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 7-017 | Thorpe Park Developments Limited (Co. Reg. No: 04141502) | 3522.60 square metres of woodland and scrubland, situated to the northeast of public adopted highway (Kennerleigh Avenue) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK624551 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.1 (Reconstruction of Road and Foot Bridge) & Work No. 2 (Gas Main Diversion) |
| 7-017a | Thorpe Park Developments Limited (Co. Reg. No: 04141502) | 21717.54 square metres of woodland situated to the northeast of public adopted highway (Kennerleigh Avenue) and to the southwest of public adopted highway (Ada Glassby Court), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK624551 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 2 (Gas Main Diversion) | |
| 8-001 | Leeds City Council Leeds City Council (in respect of mines and minerals) | 168.78 square metres of agricultural land, situated to the east of Gate House and to the southwest of Lazencroft Farm, in the ward of Cross Gates and Whinmoor, Leeds City Council WYK867535 YY107923 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | | | | | | | | | Bridge) and Track Renewal Works |
| 8-002 | Scarborough Group Limited (Co. Reg. No: 05308255) Thorpe Park Developments Limited (Co. Reg. No: 04141502) (in respect of mines and minerals) | 2176.53 square metres of scrubland and site access, situated to the east of public adopted highway (William Parkin Way (A63)) and to the southwest of Lazencroft Farm, in the ward of Temple Newsam, Leeds City Council YY107433 YY54263 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) |
| 8-003 | Leeds City Council Leeds City Council (in respect of mines and minerals) | 20245.99 square metres of agricultural land, situated to the south of public adopted highway (Manston Lane) and to the southwest of Barnbow Common, in the ward of Cross Gates and Whinmoor, Leeds City Council WYK867535 YY107925 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) and Track Renewal Works |
| 8-003a | Leeds City Council Leeds City Council (in respect of mines and minerals) | 131.96 square metres of agricultural land, situated to the south of public adopted highway (Manston Lane) and to the southwest of Barnbow Common, in the ward of Cross Gates and Whinmoor, Leeds City Council | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | WYK867535 YY107925 | | | | | | | | |
| 8-004 | Scarborough Group Limited (Co. Reg. No: 05308255) | 100.12 square metres of agricultural land situated to the south of public adopted highway (Manston Lane) and to the southwest of Shippen House Farm, in the ward of Temple Newsam, Leeds City Council YY107433 YY50875 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) | |
| 8-004a | Scarborough Group Limited (Co. Reg. No: 05308255) | 9993.46 square metres of agricultural land and public footpath and bridleway link (Leeds 124) situated to the south of public adopted highway (Manston Lane) and to the southwest of Shippen House Farm, in the ward of Temple Newsam, Leeds City Council YY107433 YY50875 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) |
| 8-005 | Leeds City Council Unknown (in respect of mines and minerals) | 6797.40 square metres of agricultural land situated to the west of Shippen House Farm and to the east of public adopted highway (Manston Lane Roundabout), in the | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | ward of Cross Gates and Whinmoor, Leeds City Council WYK867535 | | | | | | | | Crawshaw Woods Bridge) and Track Renewal Works |
| 8-006 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 1410.78 square metres of agricultural land situated to the south of Barnbow Common and to the north of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Cross Gates and Whinmoor, Leeds City Council WYK867535 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) |
| 8-007 | Leeds City Council | 830.09 square metres of agricultural land and public footpath and link bridleway (Leeds 124) situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and to the southwest of Shippen Cottages, in the ward of Cross Gates and Whinmoor, Leeds City Council WYK868966 YY107925 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) | |
| 8-007a | Leeds City Council | 1198.38 square metres of agricultural land and public footpath and link bridleway (Leeds 124) situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and to the southwest | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | of Shippen Cottages, in the ward of Cross Gates and Whinmoor, Leeds City Council WYK868966 YY107925 | | | | | | | | Crawshaw Woods Bridge) |
| 8-008 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 900.74 square metres of railway (Leeds to Hull Lines (ELR:HUL4)), and overbridge for farm access and carrying public footpath and bridleway link (Leeds 124), situated to the south of Manston Lane and to the east of Gate House, in the wards of Cross Gates and Whinmoor and Temple Newsam, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) | |
| 8-009 | Unknown Scarborough Group Limited (Co. Reg. No: 05308255) (in respect of mines and minerals) | 145.09 square metres of hardstanding and public footpath and bridleway link (Leeds 124) situated to the south of Railway (Leeds to Hull Lines (ELR:HUL4)) and to the northwest of Crawshaw Wood in the ward of Temple Newsam, Leeds City Council YY50875 Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) | |
| 8-009a | Unknown | 50.20 square metres of hardstanding and | | Yes | | | | Yes | | Temporary Possession and |

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|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Scarborough Group Limited (Co. Reg. No: 05308255) (in respect of mines and minerals) | public footpath and bridleway link (Leeds 124) situated to the south of Railway (Leeds to Hull Lines (ELR:HUL4)) and to the northwest of Crawshaw Wood in the ward of Temple Newsam, Leeds City Council YY50875 Unregistered | | | | | | | | Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) |
| 8-010 | Leeds City Council H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | 362.71 square metres of agricultural land situated to the south of Shippen House Farm and to the northwest of Crawshaw Wood, in the ward of Cross Gates and Whinmoor Leeds City Council WYK867479 WYK868966 | Yes | | | | Yes | Yes | Limits of Deviation of Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) | |
| 8-010a | Leeds City Council H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | 5864.18 square metres of agricultural land situated to the south of Shippen House Farm and to the northwest of Crawshaw Wood, in the ward of Cross Gates and Whinmoor Leeds City Council WYK867479 WYK868966 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No.3 (Reconstruction of HUL4/20 Crawshaw Woods Bridge) |
| 9-001 | Leeds City Council H.J. Banks and Company Limited | 18524.70 square metres of agricultural land situated to the | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>(in respect of mines and minerals)</i> | north of Barrowby Lodge and to the east of Crawshaw Wood, in the ward of Temple Newsam, Leeds City Council WYK868966 WYK880843 | | | | | | | Bridge at Barrowby Lane) | |
| 9-001a | Leeds City Council H.J. Banks and Company Limited <i>(in respect of mines and minerals)</i> | 3189.02 square metres of agricultural land situated to the north of Barrowby Lodge and to the east of Crawshaw Wood, in the ward of Temple Newsam, Leeds City Council WYK868966 WYK880843 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-002 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 3860.02 square metres of railway, works and land (Leeds to Hull Lines (ELR:HUL4)), railway embankment and hedgerows, south of private road (Nanny Goat Lane), north of public adopted highway (Barrowby Lane), in the wards of Harewood and Temple Newsam, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |
| 9-003 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 1.87 square metres of woodland, situated to the west of private road (Nanny Goat Lane) and to the north | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |

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|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | of Crawshaw Wood, in the ward of Temple Newsam, Leeds City Council WYK868966 Unregistered | | | | | | | | |
| 9-004 | Graham Gardner Maria Helen Klima H.J. Banks and Company Limited <i>(in respect of mines and minerals)</i> | 2803.30 square metres of agricultural land and hedgerows, situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and to the west of private road (Nanny Goat Lane), in the wards of Harewood and Temple Newsam, Leeds City Council WYK867479 WYK544420 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |
| 9-005 | Graham Gardner Maria Helen Klima H.J. Banks and Company Limited <i>(Co. Reg. No: 01247725)</i> <i>(in respect of mines and minerals)</i> | 89.56 square metres of public bridleway (Barwick 10) and private road (Nanny Goat Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK867479 WYK544420 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |
| 9-005a | Graham Gardner Maria Helen Klima H.J. Banks and Company Limited | 225.05 square metres of public bridleway (Barwick 10), non-definitive bridleway and private road (Nanny Goat Lane), | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 01247725) (in respect of mines and minerals) | situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK867479 WYK544420 | | | | | | | | Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-005b | Graham Gardner Maria Helen Klima Network Rail Infrastructure Limited (Co. Reg. No: 02904587) H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | 35.03 square metres of non-definitive bridleway and private road (Nanny Goat Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK867479 WYK544420 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-005c | Graham Gardner Maria Helen Klima H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | 8.99 square metres of non-definitive bridleway and private road (Nanny Goat Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK867479 WYK544420 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-005d | Graham Gardner | 49.34 square metres of non-definitive | | Yes | | | | Yes | | Temporary Possession and |

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|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Maria Helen Klima Network Rail Infrastructure Limited (Co. Reg. No: 02904587) H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | bridleway and private road (Nanny Goat Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK867479 WYK544420 | | | | | | | | Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-005e | Graham Gardner Maria Helen Klima H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | 20.18 square metres of non-definitive bridleway and private road (Nanny Goat Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK867479 WYK544420 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-006 | Andrew Jeremy Barrowman Unknown (in respect of mines and minerals) | 527.41 square metres of woodland, situated to the north of Barrowby Lodge, and to the southwest of Barrowby Gates in the ward of Garforth and Swillington, Leeds City Council WYK409604 Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 9-007 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 56.19 square metres of wooded area and public adopted highway (Barrowby Lane) situated to the north of Barrowby Lodge and to the south of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Temple Newsam, Leeds City Council WYK868966 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-007a | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 43.41 square metres of wooded area and public adopted highway (Barrowby Lane) situated to the north of Barrowby Lodge and to the south of railway (Leeds to Hull Lines (ELR:HUL4)), in the ward of Temple Newsam, Leeds City Council WYK868966 Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |
| 9-008 | Unknown H.J. Banks and Company Limited <i>(in respect of mines and minerals)</i> Leeds City Council <i>(in respect of public adopted highway (Barrowby Lane))</i> | 9.51 square metres of public adopted highway (Barrowby Lane), public bridleway link (Leeds 125), track and hedgerows, situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)) and to | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>and in respect of subsoil rights up to the centreline of the public highway)</i> Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | the north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council Unregistered WYK880843 | | | | | | | | |
| 9-008a | Unknown H.J. Banks and Company Limited <i>(in respect of mines and minerals)</i> Leeds City Council <i>(in respect of public adopted highway (Barrowby Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 1.22 square metres of public adopted highway (Barrowby Lane), public bridleway link (Leeds 125), track and hedgerows, situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)) and to the north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council WYK880843 YY181596 (Pending First Registration) | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-009 | Unknown Leeds City Council <i>(in respect of public adopted highway (Barrowby Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> | 444.92 square metres of public adopted highway (Barrowby Lane), public bridleway link (Leeds 125) and hedgerows, situated to the south of railway (Leeds to Hull Lines | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>the centreline of the public highway)</i> Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Clifford Marshall <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | (ELR:HUL4)) and to north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council Unregistered | | | | | | | | of Bridge at Barrowby Lane) |
| 9-009a | Unknown Leeds City Council <i>(in respect of public adopted highway (Barrowby Lane) and in respect of subsoil rights up to the centreline of the public highway)</i> Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Clifford Marshall <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 205.61 square metres of public adopted highway (Barrowby Lane) public bridleway link (Leeds 125) and hedgerows, situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)) and to the north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 4 (Construction of Bridge at Barrowby Lane) | |
| 9-009b | Unknown Leeds City Council <i>(in respect of public</i> | 935.34 square metres of public adopted highway (Barrowby Lane), public bridleway link (Leeds | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <p><i>adopted highway (Barrowby Lane)</i></p> <p>Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>Clifford Marshall <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>Michael Jubb <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> | <p>125), track and hedgerows, situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)) and to north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council</p> <p>Unregistered</p> | | | | | | | | Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-009c | <p>Unknown</p> <p>Leeds City Council <i>(in respect of public adopted highway (Barrowby Lane) and subsoil rights up to the centreline of the public highway)</i></p> <p>Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centreline of the public highway)</i></p> <p>Clifford Marshall <i>(in respect of subsoil rights up to the</i></p> | <p>22.62 square metres of public adopted highway (Barrowby Lane), public bridleway link (Leeds 125) and hedgerows, situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)) and to north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council</p> <p>YY181596 (Pending First Registration)</p> | | Yes | | | | Yes | | <p>Temporary Possession and Access for Construction</p> <p>Required for Work No. 4 (Construction of Bridge at Barrowby Lane)</p> |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-------------------------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>centreline of the public highway)</i> | | | | | | | | | |
| 9-010 | Unknown H.J. Banks and Company Limited <i>(in respect of mines and minerals)</i> Leeds City Council <i>(in respect of public adopted highway (Barrowby Lane))</i> Andrew Jeremy Barrowman <i>(in respect of subsoil rights up to the centre line of the public highway)</i> | 15.76 square metres of public adopted highway (Barrowby Lane), public bridleway link (Leeds 125), track and hedgerows, situated to the south of railway (Leeds to Hull Lines (ELR:HUL4)) and to the north of Barrowby Lodge, in the wards of Garforth and Swillington and Temple Newsam, Leeds City Council Unregistered WYK880843 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-011 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 32.87 square metres of public bridleway (Barwick 10, Nanny Goat Lane), Barrowby Foot Crossing and railway (Leeds to Hull Lines (ELR:HUL4)), situated to the southeast of Barrowby Gates, and to the northeast of Barrowby Lodge, in the ward of Harewood, Leeds City Council Unregistered | Yes – extinguishment of rights only | | | | | | Closure of Level Crossing | |
| 9-012 | Network Rail Infrastructure Limited | 123.02 square metres of railway, works and land (Leeds to Hull | Yes – extinguishment of rights only | | | | | | Closure of Level Crossing | |

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|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 02904587) | Lines (ELR:HUL4)) and public footpath (Garforth 6), situated to the north of public adopted highway (Barrowby Lane) and to the west of public adopted highway (Barwick Road), in the wards of Garforth and Swillington and Harewood, Leeds City Council Unregistered | | | | | | | | |
| 9-013 | Unknown H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 181.28 square metres of private road (Nanny Goat Lane) and non-definitive bridleway to the west of public adopted highway (Barwick Road) and north of public adopted highway (Barrowby Lane) in the wards of Garforth and Swillington and Harewood, Leeds City Council WYK867479 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-013a | Unknown | 7.07 square metres of public bridleway (Barwick 10) private road (Nanny Goat Lane) to the west of public adopted highway (Barwick Road) and north of public adopted highway (Barrowby | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Lane) in the wards of Garforth and Swillington and Harewood, Leeds City Council Unregistered | | | | | | | | |
| 9-013b | Unknown H.J. Banks and Company Limited (Co. Reg. No: 01247725) (in respect of mines and minerals) | 3.90 square metres of public bridleway (Barwick 10) private road (Nanny Goat Lane) to the west of public adopted highway (Barwick Road) and north of public adopted highway (Barrowby Lane) in the wards of Garforth and Swillington and Harewood, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-013c | Graham Gardner Maria Helen Klima Unknown (in respect of mines and minerals) | 31.24 square metres of non-definitive bridleway and private road (Nanny Goat Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK544420 Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-013d | Graham Gardner Maria Helen Klima | 31.24 square metres of non-definitive bridleway and private road (Nanny Goat | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Unknown (in respect of mines and minerals) | Lane), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)), and to the northeast of Barrowby Gates in the ward of Harewood, Leeds City Council WYK544420 Unregistered | | | | | | | | Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-013e | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 53.23 square metres of private road (Nanny Goat Lane) and non-definitive bridleway to the west of public adopted highway (Barwick Road) and north of public adopted highway (Barrowby Lane) in the wards of Garforth and Swillington and Harewood, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 9-013f | Unknown | 604.83 square metres of private road (Nanny Goat Lane) and non-definitive bridleway to the west of public adopted highway (Barwick Road) and north of public adopted highway (Barrowby Lane) in the wards of Garforth and Swillington and Harewood, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|----------------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 9-013g | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 1712.61 square metres of private road (Nanny Goat Lane) and non-definitive bridleway to the west of public adopted highway (Barwick Road) and north of public adopted highway (Barrowby Lane) in the wards of Garforth and Swillington and Harewood, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 4 (Construction of Bridge at Barrowby Lane) |
| 10-001 | Brannan James Tempest Barclays Bank UK plc (Co. Reg. No: 9740322) (in respect of a legal charge dated 13 May 2008 registered under title WYK488886) H.J. Banks and Company Limited (Co. Reg. No: 1247725) (in respect of mines and minerals) | 496.38 square metres of private access road, hedgerow and public footpath (Garforth 7A) and public bridleway (Parlington), situated to the east of public adopted highway (Barwick Road) and to the north of Allotment Gardens, in the ward of Garforth and Swillington, Leeds City Council WYK488886 WYK880944 | Yes – right of access only | | | | | | Acquisition of new right of access for benefit of third parties | |
| 10-002 | Unknown | 167.71 square metres of private access road, hedgerow public | Yes – right of access only | | | | | | Acquisition of new right of access for | |

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|---------|--|---|----------------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | footpath (Garforth 7A) and public bridleway (Parlington), situated to the east of public adopted highway (Barwick Road) and to the north of Allotment Gardens, in the ward of Garforth and Swillington, Leeds City Council Unregistered | | | | | | | benefit of third parties | |
| 10-003 | Andrew Christopher Chapman David Alan Leeming John Michael Chapman H.J. Banks and Company Limited (Co. Reg. No: 1247725) (in respect of mines and minerals) | 2385.56 square metres of private road verge, hedgeows agricultural land and public footpath (Garforth 7), situated to the east of public adopted highway (Barwick Road) and north of Allotment Gardens, in the ward of Garforth and Swillington, Leeds City Council WYK297984 WYK880944 | Yes – right of access only | Yes | | | | | Acquisition of new right of access for benefit of third parties. | Temporary Possession and Access for Construction Required for surface works to access track to allotments |
| 10-004 | Andrew Christopher Chapman David Alan Leeming John Michael Chapman | 39.27 square metres of private road verge, field access and hedgerow, situated to the east of public adopted highway (Barwick Road) and south of Rose Cottage, in the ward of Garforth and Swillington, Leeds City Council | Yes – right of access only | | | | | | Acquisition of new right of access for benefit of third parties | |

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|---------|---|---|----------------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | WYK297984 | | | | | | | | |
| 10-005 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 98.60 square metres of railway, works and land (The Leeds to Hull Lines (ELR:HUL4)), level crossing and public footpath (Garforth 7), situated to the southeast of public adopted highway (Barwick Road) and south of Allotment Gardens, in the ward of Garforth and Swillington, Leeds City Council Unregistered | Yes – right of access only | | | | | | Acquisition of new right of access for benefit of third parties | |
| 11-001 | Unknown Leeds City Council (in respect of public adopted highway (Church Lane)) The Executor of John Francis Hills (in respect of subsoil rights up to the centreline of the public highway) | 319.96 square metres of public adopted highway (Church Lane) situated to the north of railway (Leeds to Micklefield Line) and to the west of Three Acre Plantation, in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) |
| 11-002 | The Executor of John Francis Hills | 4183.06 square metres of fence, hedgerow and agricultural land, situated to the south of public adopted | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | highway (Church Lane), and to the north of railway (Leeds to Micklefield Line), in the ward of Kippax and Methley, Leeds City Council YY46839 | | | | | | | | Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) |
| 11-003 | The Executor of John Francis Hills | 14528.478 square metres of agricultural land and public footpath (Micklefield 3), situated to the northeast of Roman Ridge Bridge and to the south of public adopted highway (Church Lane), in the ward of Kippax and Methley, Leeds City Council WYK913414 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) |
| 11-003a | Unknown Leeds City Council <i>(in respect of public adopted highway (Ridge Road) and in respect of public footpath (Micklefield 3))</i> The Executor of John Francis Hills <i>(in respect of subsoil rights up to the centre line of the public highway)</i> | 11.24 square metres of public adopted highway (Ridge Road) and public footpath (Micklefield 3), situated to the northeast of Roman Ridge Bridge and to the south of public adopted highway (Church Lane), in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) |

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|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 11-004 | Unknown Leeds City Council <i>(in respect of public adopted highway (Ridge Road))</i> The Executor of John Francis Hills <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 1685.56 square metres of public adopted highway verge (Ridge Road) situated to the east of public adopted highway (Church Lane) and to the northeast of Roman Ridge Bridge, in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) |
| 11-005 | Makins Enterprises Ltd <i>(Co. Reg. No: 13205715)</i> HSBC UK Bank PLC <i>(Co. Reg. No: 09928412)</i> <i>(in respect of a legal charge dated 25 June 2021 as registered under title WYK563840)</i> Christopher William Makin <i>(in respect of mines and minerals)</i> | 8887.99 square metres of trees, agricultural land, public footpath (Sturton Grange 4) and private road (Berry Lane) situated to the north of railway (Leeds to Micklefield Line) and to the east of Sturton Grange Far, in the ward of Garforth and Swillington, Leeds City Council WYK563840 YY1303 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 6 (Diversion of Gas Main) | |
| 11-005a | Makins Enterprises Ltd <i>(Co. Reg. No: 13205715)</i> | 5041.93 square metres of trees, agricultural land, public footpath (Sturton Grange 3) and private road | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | HSBC UK Bank PLC (Co. Reg. No: 09928412) (in respect of a legal charge dated 25 June 2021 as registered under title WYK563840) Christopher William Makin (in respect of mines and minerals) | (Berry Lane) situated to the north of railway (Leeds to Micklefield Line) and to the east of Sturton Grange Farm, in the ward of Garforth and Swillington, Leeds City Council WYK563840 YY1303 | | | | | | | | Required for Work No. 6 (Diversion of Gas Main) |
| 11-006 | Makins Enterprises Ltd (Co. Reg. No: 13205715) HSBC UK Bank PLC (Co. Reg. No: 09928412) (in respect of a legal charge dated 25 June 2021 as registered under title WYK563840) Unknown (in respect of mines and minerals) | 714.73 square metres of railway embankment and hedgerows, situated to the west of public adopted highway (Phoenix Avenue) and to the northwest of Ridge Bridge Cottage, in the ward of Garforth and Swillington, Leeds City Council WYK563840 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 6 (Diversion of Gas Main) | |
| 11-006a | Makins Enterprises Ltd (Co. Reg. No: 13205715) Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 45.46 square metres of railway embankment and hedgerows, situated to the west of public adopted highway (Phoenix Avenue) and to the northwest of Ridge Bridge Cottage, in the ward of Garforth | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 6 (Diversion of Gas Main) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | HSBC UK Bank PLC (Co. Reg. No: 09928412) (in respect of a legal charge dated 25 June 2021 as registered under title WYK563840) Unknown (in respect of mines and minerals) | and Swillington, Leeds City Council WYK563840 | | | | | | | | |
| 11-007 | Unknown Leeds City Council (in respect of public adopted highway (Ridge Road)) Christopher William Makin (in respect of subsoil rights up to the centreline of the public highway) Makins Enterprises Ltd (Co. Reg. No: 13205715) (in respect of subsoil rights up to the centreline of the public highway) | 278.80 square metres of public adopted highway (Ridge Road) and hedgerows, situated to the north of railway (Leeds to Micklefield Line) and to the south of public adopted highway (Church Lane), in the ward of Kippax and Methley, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) | |
| 11-007a | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 132.58 square metres of bridge (Roman Ridge Bridge) carrying public adopted highway (Ridge Road) | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|---|----------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Leeds City Council <i>(in respect of public adopted highway (Ridge Road))</i> | over railway (Leeds to Micklefield Line) situated to the northwest of Peckfield Business Park and to the southwest of Three Acre Plantation, in the ward of Kippax and Methley, Leeds City Council Unregistered | | | | | | | | |
| 11-008 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> Leeds City Council <i>(in respect of public adopted highway (Ridge Road))</i> | 87.44 square metres of bridge (Roman Ridge Bridge) carrying public adopted highway (Ridge Road) over railway (Leeds to Micklefield Line) situated to the northwest of Peckfield Business Park and to the southwest of Three Acre Plantation, in the ward of Kippax and Methley, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) | |
| 11-009 | Unknown Leeds City Council <i>(in respect of public adopted highway (Ridge Road))</i> Christopher William Makin <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 444.28 square metres of bridge (Roman Ridge Bridge) carrying public adopted highway (Ridge Road) situated to the west of Phoenix Avenue and to the south of railway (Leeds to Micklefield Line), in the ward of Kippax and Methley, Leeds City Council | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|--|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|---|----------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Jean Lilian Makin <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Kevin David Adams <i>(in respect of subsoil rights up to the centreline of the public highway)</i> Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | Unregistered | | | | | | | | |
| 11-010 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 1690.79 square metres of railway, works and land (Leeds to Micklefield Line), embankment and hedgerows, situated to the west of Roman Ridge Bridge and to the northwest of public adopted highway (Phoenix Avenue), in the wards of Garforth and Swillington, and Kippax and Methley, Leeds City Council Unregistered | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 6 (Diversion of Gas Main) | |

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|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| 11-011 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Christopher William Makin (in respect of mines and minerals) | 542.55 square metres of railway embankment and hedgerow, situated to the south of railway (Leeds to Micklefield Line) and to the west of Roman Ridge Bridge, in the wards of Garforth and Swillington, and Kippax and Methley, Leeds City Council YY1303 WYK936047 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 6 (Diversion of Gas Main) | |
| 11-012 | Jean Lilian Makin Christopher William Makin (in respect of mines and minerals) | 5318.50 square metres of agricultural land, situated to the west of public adopted highway (Ridge Road) and to the south of railway (Leeds to Micklefield Line), in the wards of Garforth and Swillington, and Kippax and Methley, Leeds City Council WYK586916 YY1303 | Yes | | | | Yes | Yes | Limits of Deviation of Work No. 6 (Diversion of Gas Main) | |
| 11-012a | Jean Lilian Makin Christopher William Makin (in respect of mines and minerals) Leeds City Council (in respect of public | 31245.53 square metres of agricultural land and public footpath (Sturton Grange 5), situated to the west of public adopted highway (Ridge Road) and to the south of railway | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>footpath (Sturton Grange 5))</i> | (Leeds to Micklefield Line), in the wards of Garforth and Swillington, and Kippax and Methley, Leeds City Council WYK586916 YY1303 | | | | | | | | Bridge), Work No. 6 (Diversion of Gas Main) & demolition of Brady Farm Bridge |
| 11-012b | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Christopher William Makin (in respect of mines and minerals) | 25.21 square metres of scrubland and railway embankment situated to the west of public adopted highway (Ridge Road) and to the south of railway (Leeds to Micklefield Line), in the wards of Garforth and Swillington, and Kippax and Methley, Leeds City Council WYK936047 YY1303 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge) & Work No. 6 (Diversion of Gas Main) & demolition of Brady Farm Bridge |
| 11-012c | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) Christopher William Makin (in respect of mines and minerals) | 5.13 square metres of bridge carrying footpath over railway (Leeds to Micklefield Line), situated to the south of Sturton Grange Farm and to the west of public adopted highway (Roman Ridge), in the ward of Garforth and Swillington, Leeds City Council Unregistered | Yes | | | | | Yes | | Permanent Possession and Access for Construction Demolition of Brady Farm bridge and extinguishment of rights |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|---|--------------------------|-----------------------------|-----------------------------|-----------------------|--|--|----------------------------|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | YY1303 | | | | | | | | |
| 11-013 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 439.81 square metres of bridge carrying footpath over railway (Leeds to Micklefield Line), situated to the south of Sturton Grange Farm and to the west of public adopted highway (Roman Ridge), in the ward of Garforth and Swillington, Leeds City Council Unregistered | Yes | | | | | Yes | | Permanent Possession and Access for Construction Demolition of Brady Farm bridge and extinguishment of rights |
| 11-014 | Christopher William Makin | 21.00 square metres of public footpath (Garforth 17) and grassland, situated to the north of railway (Leeds to Micklefield Line) and to the southwest of Sturton Grange Farm, in the ward of Garforth and Swillington, Leeds City Council WYK586891 YY1303 | | Yes | | | | Yes | | Temporary Possession and Access for Construction Demolition of Brady Farm bridge and extinguishment of rights |
| 11-015 | Makins Enterprises Ltd (Co. Reg. No: 13205715) HSBC UK Bank PLC | 254.62 square metres of public footpath (Garforth 17) and grassland, situated to the north of railway (Leeds to Micklefield Line) and to the | | Yes | | | | Yes | | Temporary Possession and Access for Construction |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | (Co. Reg. No: 09928412) (in respect of a legal charge dated 25 June 2021 as registered under title WYK563840) Christopher William Makin (in respect of mines and minerals) | southeast of Sturton Grange Farm, in the ward of Garforth and Swillington, Leeds City Council WYK563840 YY1303 | | | | | | | | Demolition of Brady Farm bridge and extinguishment of rights |
| 11-016 | Unknown Leeds City Council (in respect of public adopted highway (Ridge Road) and in respect of public footpath (Sturton Grange 5)) Jean Lilian Makin (in respect of subsoil rights up to the centreline of the public highway) | 337.92 square metres of public adopted highway (Ridge Road), public footpath (Sturton Grange 5) and verge to the south of railway (Leeds to Micklefield Line) and west of public footpath (Micklefield 9), in the ward of Garforth and Swillington, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 5 (Reconstruction of HUL4/14 Ridge Road Bridge), Work No. 6 (Diversion of Gas Main) & demolition of Brady Farm Bridge |
| 11-017 | Unknown Leeds City Council (in respect of public adopted highway (Ridge Road)) Makins Enterprises Ltd (Co. Reg. No: 13205715) | 126.04 square metres of public adopted highway (Ridge Road) and verge to the north of railway (Leeds to Micklefield Line) and south west of public adopted highway (Church Lane), in the ward of Garforth and | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required for Work No. 6 (Diversion of Gas Main) |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|---|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | Swillington, Leeds City Council Unregistered | | | | | | | | |
| 12-001 | Unknown Leeds City Council <i>(in respect of public adopted highway (Great North Road) and in respect of public bridleway (Definitive Footpath Micklefield 16))</i> Ashdale Land and Property Company Limited <i>(Co. Reg. No: 00470852)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 51.81 square metres of public adopted highway (Great North Road), public bridleway (Definitive Footpath Micklefield 16) and grass verge, situated to the west of public adopted highway (Daisy Bank Drive) and to the south of Bland's Arms in the ward of Kippax and Methley, Leeds City Council Unregistered | Yes | | | | | Yes | Improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |
| 12-002 | Ashdale Land and Property Company Limited <i>(Co. Reg. No: 00470852)</i> Unknown <i>(in respect of mines and minerals)</i> | 2860.99 square metres of private road (Pit Lane), public bridleway (Micklefield 8), verge, hedgerows and trees, situated to the south of Micklefield Church of England Primary School and to the west of public adopted highway (Great North Road), in the ward of Kippax and Methley, Leeds City Council | Yes | | | | | Yes | Improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |

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| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|-------------------------|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | WYK788662 Unregistered | | | | | | | | |
| 12-003 | Unknown Leeds City Council <i>(in respect of public bridleway (Micklefield 8))</i> | 1462.47 square metres of private road (Pit Lane), public bridleway (Micklefield 8), verge and trees, situated to the north of Enterprise Court and to the west of public adopted highway (Great North Road), in the ward of Kippax and Methley, Leeds City Council Unregistered | Yes | | | | | Yes | Improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |
| 12-004 | Network Space Land Limited <i>(Co. Reg. No: 01604509)</i> Unknown <i>(in respect of mines and minerals)</i> | 48.65 square metres of agricultural land and trees, situated to the southwest of Enterprise Court and to the south of public adopted highway (Phoenix Avenue), in the ward of Kippax and Methley, Leeds City Council WYK621176 Unregistered | Yes | | | | | Yes | Improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |
| 12-005 | Hall Farm Micklefield LLP <i>(LLP Reg. No: OC402258)</i> Unknown <i>(in respect of mines and minerals)</i> | 110.17 square metres of scrubland and trees, situated to the west of private road (Pit Lane) and to the south of Micklefield Church of England | Yes | | | | | Yes | Improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |

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|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Primary School, in the ward of Kippax and Methley, Leeds City Council YY11420 Unregistered | | | | | | | | |
| 12-006 | Great North Developments Limited <i>(Co. Reg. No: 02743524)</i> Unknown <i>(in respect of mines and minerals)</i> | 3824.41 square metres of agricultural land situated to the northwest of The Old Fire Station and to the north of Railway Cottages, in the ward of Kippax and Methley, Leeds City Council YY151552 YY170001 Unregistered | Yes | | | | | Yes | Improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |
| 12-007 | Micklefield Parish Council <i>(as sole Trustee of Micklefield Recreation Ground Charity)</i> | 40.74 square metres of part of Micklefield Recreational Park, situated to the north of railway (Leeds to Micklefield Line) and to the northeast of Enterprise Court, in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required in connection with improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing |
| 12-008 | Unknown | 19.96 square metres of scrubland and fence, situated to the | Yes | | | | | Yes | Improvements to Lower | |

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|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | west of private road (Pit Lane) and to the north of railway (Leeds to Micklefield Line), in the ward of Kippax and Methley, Leeds City Council Unregistered | | | | | | | Peckfield Lane and to facilitate closure of Peckfield Level Crossing | |
| 12-009 | NUMBER NOT USED | NUMBER NOT USED | | | | | | | | |
| 12-010 | Micklefield Parish Council <i>(as sole Trustee of Micklefield Recreation Ground Charity)</i> | 3083.16 square metres of Micklefield recreational park, footpath, car park, trees and hedgerows, situated to the north of railway (Leeds to Micklefield Line) and to the northeast of Enterprise Court, in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required in connection with improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing |
| 12-011 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 58.92 square metres of Peckfield Level Crossing across railway (Leeds to Micklefield Line) and public bridleway (Micklefield 8), situated to the north of Enterprise Court and to the south of Railway Cottages, in the ward of Kippax and Methley, Leeds City Council | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required in connection with improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level |

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|---------|--|---|--------------------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | Unregistered | | | | | | | | Crossing |
| 12-012 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 0.04 square metres of private road (Pit Lane) and public bridleway (Micklefield 8), situated to the south of railway (Leeds to Micklefield Line) and to the north of Enterprise Court, in the ward of Kippax and Methley, Leeds City Council WYK482655 Unregistered | Yes – extinguishment of rights | | | | | | Extinguishment of rights to facilitate closure of Peckfield Level Crossing | |
| 12-013 | Unknown | 262.39 square metres of trees, hedgerows and fence, situated to the south of railway (Leeds to Micklefield Line) and to the northwest of Enterprise Court, in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes | | | | Yes | | Temporary Possession and Access for Construction Required in connection with improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing |
| 12-014 | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 1431.50 square metres of private road (Phoenix Avenue), situated to the south of railway (Leeds to Micklefield Line) and to the north of Enterprise Court, in | Yes | | | | Yes | Yes | Track Sectioning Cabin and ancillary infrastructure and works | |

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|---------|--|---|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|---|--|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | the ward of Kippax and Methley, Leeds City Council WYK482655 Unregistered | | | | | | | | |
| 12-014a | Leeds City Council Unknown <i>(in respect of mines and minerals)</i> | 19915.05 are metres of private road (Phoenix Avenue), footway and scrubland and situated to the south of railway (Leeds to Micklefield Line) and to the north of Enterprise Court, in the ward of Kippax and Methley, Leeds City Council WYK482655 Unregistered | Yes – rights only | Yes | | | | Yes | Acquisition of right of way along private road (Phoenix Avenue) | Temporary Possession and Access for Construction Required in connection with improvements to Lower Peckfield Lane and to facilitate closure of Peckfield Level Crossing |
| 13-001 | Unknown Leeds City Council <i>(in respect of public adopted highway (Great North Road))</i> National Highway Limited <i>(Co. Reg. No: 09346363)</i> <i>(in respect of subsoil rights up to the centreline of the public highway)</i> | 41.76 square metres of public adopted highway (Great North Road), situated to the west of Castle Hills and southeast of Newton Farm, in the ward of Kippax and Methley, Leeds City Council Unregistered | | Yes – Access Only | | | | Yes | | Temporary Access for Construction |
| 13-002 | National Highway Limited <i>(Co. Reg. No: 09346363)</i> | 1021.86 square metres of private road leading to Motor Sports Circuit | | Yes – Access Only | | | | Yes | | Temporary Access for Construction |

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|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | (Micklefield MX), public footpath (Micklefield 7), grassland, verge, woodland and A road embankment (A1), situated to the east of Great North Road and west of Castle Hills, in the ward of Kippax and Methley, Leeds City Council WYK889915 | | | | | | | | |
| 13-003 | National Highway Limited (Co. Reg. No: 09346363) | 532.45 square metres of bridge carrying public adopted highway (A1) and underpass, private road, public footpath (Micklefield 7 and road embankment, situated to the west of Motor Sports Circuit and east of public adopted highway (Great North Road), in the ward of Kippax and Methley, Leeds City Council WYK874415 | | Yes – Access Only | | | | Yes | | Temporary Access for Construction |
| 13-004 | Macauley Homes Limited (Co. Reg. No: 05014517) | 2736.22 square metres of land forming part of Motor Sports Circuit (Micklefield MX, private accessway, woodland and public footpath (Micklefield 7), situated to the east of | | Yes | | | | Yes | | Temporary possession and access for construction Required in connection with diversion of public right of way. |

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|---------|--|--|-----------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|-------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | | the public adopted highway (A1) and north of Castle Hills, in the ward of Kippax and Methley, Leeds City Council WYK387950 | | | | | | | | |
| 13-005 | National Highway Limited (Co. Reg. No: 09346363) | 85.71 square metres of land forming part of private accessway to public adopted highway (A1), woodland and public footpath (Micklefield 7), situated to the south of Motor Sports Circuit and north of Castle Hills, in the ward of Kippax and Methley, Leeds City Council WYK874415 | | Yes | | | | Yes | | Temporary possession and access for construction Required in connection with diversion of public right of way. |
| 13-006 | Leeds City Council | 5105.38 square metres of woodland (Highroyds Wood) and public footpath (Micklefield 7), situated to the south of Motor Sports Circuit and east public adopted highway (A1), in the ward of Kippax and Methley, Leeds City Council WYK505637 | | Yes | | | | Yes | | Temporary possession and access for construction Required in connection with diversion of public right of way. |
| 13-007 | Philip John Chapman | 3797.60 square metres of agricultural land and hedgerows, | | Yes | | | | Yes | | Temporary possession and |

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Statement of Case NR19

17 November 2023

| Plot No | Freehold Owners | Description | Acquisition | | Purpose | | | | Explanation (permanent) | Explanation (temporary) |
|---------|--|---|--------------------------------|--------------------------|-----------------------|--------------------|----------------------------------|--|--------------------------|---|
| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | Richard Thomas Chapman National Westminster Bank plc <i>(Co. Reg. No: 00929027)</i> <i>(in respect of a legal charge dated 29 January 2003 as registered under title NYK276741)</i> | situated to the southwest of Brookfield House and west of Newthorpe Quarry, in the wards of Kippax and Methley, and Selby, Leeds City and North Yorkshire Council NYK276741 | | | | | | | | access for construction Required in connection with diversion of public right of way. |
| 13-008 | Leeds City Council | 962.59 square metres of woodland (Highroyds Wood) and public footpath (Micklefield 7), situated to the south of railway (Leeds to Hull Lines (ELR:HUL3)) and southwest of Brookfield House, in the wards of Kippax and Methley, and Selby, Leeds City and North Yorkshire Council WYK505637 | | Yes | | | | Yes | | Temporary possession and access for construction Required in connection with diversion of public right of way. |
| 13-009 | Network Rail Infrastructure Limited <i>(Co. Reg. No: 02904587)</i> | 78.16 square metres of railway, works and land (Selby to York Line), and public footpath (Micklefield 7), situated to the northeast of Highroyds Wood and east of public adopted | Yes – extinguishment of rights | | | | | Yes | Extinguishment of rights | |

The Network Rail (Leeds to Micklefield Enhancements) Order
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| | | highway (A1), in the ward of Kippax and Methley, Leeds City Council Unregistered | | | | | | | | |
| 13-010 | Network Rail Infrastructure Limited (Co. Reg. No: 02904587) | 247.19 square metres of railway, works and land (Leeds to Hull Lines (ELR:HUL3)), situated to the southwest of Brookfield House and to the north of Highfield, in the ward of Selby, North Yorkshire Council Unregistered | | Yes | | | | Yes | | Temporary possession and access for construction Required in connection with diversion of public right of way. |
| 13-011 | Catherine Hare Richard Hare | 8516.96 square metres of track, grassland, trees, hedgerows and public footpath (NYC 35.39/2/1)), situated to the north of railway (Leeds to Hull Lines (ELR:HUL4)) and east of public adopted highway (A1), in the ward of Selby, North Yorkshire Council NYK198109 | | Yes | | | | Yes | | Temporary possession and access for construction Required in connection with diversion of public right of way. |
| 13-012 | Philip John Chapman Richard Thomas Chapman | 1191.25 square metres of hedgerows, situated to the northeast of Newthorpe Quarry and southwest of Hill House Farm, in the | | Yes – Access Only | | | | Yes | | Temporary Access for Construction |

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| | | | Permanent Acquisition | Temporary Possession/Use | Protective Works Only | Maintenance Access | Placing permanent infrastructure | Construction Activities & Mitigation Works | | |
| | National Westminster Bank plc <i>(Co. Reg. No: 00929027)</i> <i>(in respect of a legal charge dated 29 January 2003 as registered under title NYK276741)</i> | ward of Selby, North Yorkshire Council NYK276741 | | | | | | | | |
| 13-013 | Unknown | 1974.54 square metres of private road, hedgerows and public footpath (NYC 35.39/2/1), situated to the northeast of Newthorpe Quarry and southwest of Hill House Farm, in the ward of Selby, North Yorkshire Council Unregistered | | Yes – Access Only | | | | Yes | | Temporary Access for Construction |